



COMMUNITY DEVELOPMENT DEPARTMENT

Council Meeting Date: July 31, 2012
Staff Report #: 12-114

Agenda Item #:E-1

PUBLIC HEARING: Consider a Request for a Use Permit, Architectural Control, Tentative Subdivision Map, Below Market Rate Housing Agreement, Application of State Density Bonus Law, and Environmental Impact Report to Construct 26 Residential Units on a 1.23-acre Site Located at 612 Partridge Avenue, 603 - 607 College Avenue, and 321 - 389 El Camino Real (Collectively Known as 389 El Camino Real)

RECOMMENDATION

Staff recommends that the City Council concur with the recommendation of the Planning Commission and approve the following actions related to the 389 El Camino Real Project, subject to the specific actions contained in Attachment A:

1. **Environmental Review:** Adopt a resolution certifying the Final Environmental Impact Report (EIR), Adopting the Statement of Overriding Considerations and Adopting the Mitigation Monitoring and Reporting Program;
2. **State Density Bonus:** Apply the State Density Bonus Law to allow one incentive and six development standard waivers;
3. **Use Permit:** Make findings and approve a Use Permit for construction of three or more units in the R-3 zoning district and new construction of residential units in the C-4(ECR) zoning district;
4. **Architectural Control:** Adopt findings and approve the Architectural Control for design review of the new buildings and site improvements;
5. **Tentative Map:** Make findings and approve the Tentative Map to merge seven lots into two lots, abandon the public street easement for Alto Lane, and create 26 residential condominium units; and
6. **Below Market Rate (BMR) Housing Agreement:** Approve the BMR Housing Agreement to provide three on-site BMR units in accordance with the City's Below Market Rate Housing Program and State Density Bonus Law.

BACKGROUND

On September 2, 2009, the applicant submitted initial plans for a 26-unit residential project under the State Density Bonus Law (Government Code Section 65915 and relevant amendments). The applicant had previously presented conceptual plans for a larger mixed-use project that would have required General Plan and Zoning Ordinance Amendments, but this option was ultimately not pursued. A Planning Commission study session was held on the 26-unit residential project on June 28, 2010, in which both the Planning Commission and members of the public commented on the proposal. Commissioners generally shared similar sentiments about the proposal, and highlighted potential concerns about the lack of open space, the impacts to the local school district, and too many inconsistencies with the Zoning Ordinance's development regulations, which created elements that were out of character with the Allied Arts neighborhood. The primary concerns raised by the neighbors were the density and scale of the development compared to its surroundings. The topics of parking and traffic were also issues. In addition, the Commission and several members of the public were interested in learning more about the State Density Bonus Law, which would allow the project to have a density bonus and apply development standard waivers.

On May 2, 2011, the Planning Commission conducted a study session regarding the State Density Bonus Law (Government Code section 65915) with the City Attorney's Office. The study session was not specific to the 389 El Camino Real project, but the City Attorney's Office reviewed components of the Law that would be applicable to the project. The applicability of the State Density Bonus Law with respect to the proposed project is further discussed below in the State Density Bonus Law section.

Following the June 28, 2010 Planning Commission study session, the applicant worked with a Neighborhood Task Force and staff to discuss and address concerns. Based upon the comments raised by the Commission, the Neighborhood Task Force, and staff, the applicant made the following revisions to the project:

- Reduced the overall height of the buildings to conform with the height limitations of the C-4 (ECR) and R-3 zoning districts;
- Reduced the height of the residences adjacent to the Allied Arts neighborhood from three stories to two stories;
- Reoriented units to face El Camino Real;
- Increased the front setback for the unit fronting on College Avenue, to provide a better transition between the units closer to El Camino Real and the adjacent single-family neighborhood;
- Increased the setbacks along the right side property line of the R-3 zoned property, creating more private open space for these units as well as providing a greater buffer between these units and the adjacent single-family residential property;
- Increased the size and amenities of the College Avenue pocket park;

- Created a new common open space area along El Camino Real through the elimination of a driveway;
- Redesigned the architecture to avoid uniformity and blend in more appropriately with the Allied Arts neighborhood;
- Incorporated higher quality building materials and finishes; and
- Provided the option for an elevator to be installed in five of the residences, which would provide flexibility for disabled persons to purchase a residence in the development.

A Planning Commission study session was held on the project and public hearing held for the Draft EIR on March 19, 2012, in which both the Planning Commission and members of the public had the opportunity to comment on the Draft EIR and discuss the revisions made to the project since the previous study session. Comments and responses on the Draft EIR are discussed in the Environmental Review section of this report.

On June 25, 2012, the Planning Commission held a public hearing to consider recommendations on the actions required for project approval. The approved excerpt minutes for this meeting are included as Attachment H. After considering public comments and project materials, including the EIR, the Planning Commission recommended that the City Council take all required actions and approve the project. With the exception of the State Density Bonus component (5-2, with Commissioners Bressler and Kadvany opposed), all recommendations for approval were unanimous.

The staff reports and minutes from all of the meetings are available online and at the Community Development Department for review.

On June 5 and 12, 2012, the City Council approved and adopted the El Camino Real/Downtown Specific Plan and conducted related approval actions. The associated ordinances became effective on July 12, 2012, and the Specific Plan includes the subject properties within its Plan area. However, because the project was submitted prior to the Specific Plan becoming effective, it may be reviewed and approved under the preexisting Zoning Ordinance regulations. This report contains a section comparing the proposal to the equivalent Specific Plan regulations, for reference.

ANALYSIS

Proposed Project

The proposal involves the demolition of one single-family residence and a triplex and the construction of 26 residential units, designed as 17 attached townhouses and nine single-family residences (five of which would be fully detached and four of which would be structurally attached via roof connections) on a 1.23-acre site. A location map and the project plans are included as Attachments E and F, respectively.

Site Layout/Access

The townhomes would be arranged in a series of four rows perpendicular to El Camino Real, consisting of four to five units per row. To provide a more active street presence along El Camino Real, the entrances to the units adjacent to El Camino Real, with the exception of the end unit in Building C adjacent to Planet Auto, are oriented to face El Camino Real. Each of the El Camino Real entrances would provide a porch, which would help frame the entry and represent an inviting architectural feature.

The single-family semi-attached and detached units would be located parallel to El Camino Real along the rear of the property (as viewed from El Camino Real). The frontage of the two units along College Avenue and Partridge Avenue are oriented to face the street, while the seven interior units are oriented to face the internal driveway.

Access to the site would consist of two driveways off of El Camino Real, with each driveway providing two-way access to and from the site. These driveways connect to form a loop, providing access to all but two units. Two single-family units would be independent and take access from College and Partridge Avenues. The Menlo Park Fire Protection District has been consulted on the proposed site layout to ensure that emergency vehicles would be able to make a loop through the site if access is needed.

Mix of Units/Size of Units

The proposed 17 townhouse units feature a mix of two- and three-bedroom units, while the nine single-family units (includes semi-attached and detached units) are all four-bedroom units. The table below shows the mix of units as well as an approximate square footage for each of the units. The overall gross floor area for the project is approximately 46,600 square feet.

Product Mix

	Square Footage (range)	Number of Units Proposed
2 bedroom/2.5 baths	1,342 – 1,410	2
3 bedroom/3 bath	1,471 – 1,582	7
3 bedroom/3.5 bath	1,653 – 2,038	8
4 bedroom/2.5 bath	1,925 – 2,059	9

Architectural Style and Materials

The architectural style would be traditional in nature to blend with the varied architectural styles of the Allied Arts neighborhood, with articulation through the use of pop out windows, balconies and porches.

The townhouse units would feature gable roofs, shingle siding, divided light windows (with interior and exterior grids and a between-the-glass spacer bar), copper gutters and downspouts, decorative corbels, “spider” and decorative metal railings, tapered columns, and enhanced use of stone veneer at the bases and columnar features. The building height of the proposed townhouses is 30 feet to the top of the roof; however, an additional 3.8 feet of roof height would be provided to screen for rooftop mechanical equipment. The design of the roofline, both for the actual roof and roof screening, have been designed to blend in with the overall architectural style of the buildings.

The semi-detached and detached single-family residences along the rear would feature complimentary, but different materials. The seven interior homes would feature hip roofs, a combination of stucco and horizontal siding, or stucco and board and batten on the exterior façades. Wood trim, trellises and simulated divided light windows, similar to the townhouse units, would also be used on these single-family homes.

The residences facing Partridge and College Avenues would have an independent design to not appear as part of the larger project, and to create a transition into the adjacent neighborhood. The residence on College Avenue has been designed to reflect Craftsman-style architecture, with a mix of hip and gable roofs, shingle siding, divided light windows, tapered wood porch column, dormers, decorative wood corbels, and stone veneer base. The proposed residential unit on Partridge Avenue would be reminiscent of Spanish style architecture, and would feature a hip concrete tile roof, stucco siding, decorative ceramic tile and metal railing, divided light windows, arched entryway with decorative stucco and ceramic tile trim, and decorative metalwork throughout the façade.

Open Space

The proposed project includes two common open space areas: a large open space area along El Camino Real located between Buildings A1 and A2, and a smaller “pocket park” along College Avenue. The open space areas would not only be amenities to the residents on the site, they would also aesthetically enhance the neighborhood, as the two areas would be visible from College Avenue and El Camino Real. The open space near El Camino Real would include a fountain that will serve as a focal point, and include other passive elements such as a lawn and a barbeque. The pocket park near College Avenue would feature the existing heritage redwood tree that would be preserved in place. Both of these areas would include functional and decorative features such as seating areas and wood trellises. Although they are not public parks, both common open spaces would remain publicly accessible. Permanent barriers are not proposed, and limitations to public access (i.e. permanent barrier fencing, gates)

would not be permitted, as these areas would aesthetically enhance the streetscape along El Camino Real and College Avenue.

Landscaping

The site contains one heritage size redwood tree located at 603 College Avenue, which is proposed to remain and be a feature of the pocket park. As part of the off-site improvements, the applicant proposes to remove five of the existing, non-heritage street trees along El Camino Real to accommodate the new driveways and provide views to the fountain and open space. One non-heritage cedar street tree along College Avenue is recommended for removal by the City Arborist, as this tree is in poor condition and competes with the nearby heritage redwood tree.

The applicant is proposing to plant 58 new trees throughout the site, including decorative accent trees (such as crepe myrtle and Eastern redbud) along El Camino Real and College Avenue, and a row of trees (*arbutus marina*) along the fence line in the rear yards of the single-family homes to provide privacy screening for both the new homeowners and the adjacent neighbors.

The front yard landscaping along College Avenue, as well as the other common open spaces, would be maintained by the future homeowners' association to maintain a quality and manicured presence.

Tentative Map

The seven legal parcels that comprise the project site are proposed to be merged to form two new parcels that would substantially follow the existing zoning boundary line between the C-4 (ECR) and the R-3 districts. The larger of the two parcels is approximately 0.98-acre and would be in the C-4 (ECR) zoning district, while the smaller parcel is approximately .25-acre and would be in the R-3 zoning district. As noted previously, while the properties have recently been rezoned to the SP-ECR/D (El Camino Real/Downtown Specific Plan) zoning district, the project was submitted prior to that action and is being considered for approval consistent with the preexisting zoning districts. The technical front property line for each of the two new lots would be along College Avenue.

The 26 residential units would be condominiums on these two shared common lots. With the exception of exclusive use easements for private open space, all shared facilities and landscaping would be maintained by the future homeowner's association.

Abandonment of Alto Lane

The proposed development includes the abandonment of the public street easement for Alto Lane. As part of the proposed street abandonment, the existing storm drain easement that runs through Alto Lane and extends the length of the project site would also be abandoned, and a new realigned storm drain easement would be created.

The portion of Alto Lane on this block is only accessible from College Avenue, as it ends mid-block and does not provide a connection to Partridge Avenue. Currently, this portion of Alto Lane appears only to serve the triplex at 603-607 College Avenue. With the demolition of the triplex as part of the proposed project, this portion of Alto Lane would no longer serve a useful purpose as a public street easement, and its abandonment will not affect any users. The area occupied by Alto Lane would be incorporated into the overall project site, divided between the two new parcels, and assume the respective zoning designations of these two parcels.

The abandonment of Alto Lane and the existing storm drain easement are necessary for the development of the proposed project, as the function of the proposed site layout and circulation are conditional upon the abandonment of these easements.

Pedestrian Access Easement along El Camino Real

The proposed development would require the dedication of a 3.7-foot-wide pedestrian access easement (PAE) along the site's El Camino Real frontage. The PAE would accommodate the proposed six foot wide sidewalk by providing public access over this portion of the project site because there is insufficient width in the existing right-of-way. Additional discussion of the sidewalks is provided in the El Camino Real/Downtown Specific Plan comparison section of this report.

Below Market Rate (BMR) Housing Agreement

The applicant is proposing to provide three on-site BMR units to low-income households, in compliance with the City's Below Market Rate Housing Program and State Density Bonus Law. The provision of low-income units would provide a level of affordability that exceeds that of typical BMR units in the City, which are generally at moderate income levels. All three units are townhouses consisting of one 2 bedroom/2.5 bath unit, one 3 bedroom/3 bath unit, and one 3 bedroom/3.5 bath unit. These units are spread out across the site in three different buildings, and are generally representative of the selection of townhouse floor plans. The draft Below Market Rate For-Sale Agreement is included as Attachment D. The Housing Commission has reviewed the BMR Housing Agreement and approved the selection of these three units at their regular meeting on May 2, 2012.

Application of the State Density Bonus Law to the Project

The applicant is proposing to apply the provisions of Government Code Section 65915 (GC 65915), the State Density Bonus Law, to the project. A copy of GC 65915 is included for reference as Attachment G. The purpose of GC 65915 is to encourage and provide incentives to developers to include lower income housing units in their developments. In this case, the applicant is proposing to include three units for low-income households. Where the proposal exceeds the requirements of the City's Below Market Rate (BMR) Ordinance, the applicant is entitled to the benefits provided by GC

65915. The language of GC 65915 is mandatory; therefore, the City must grant the applicant a density bonus, which would allow the applicant to increase the density above the maximum allowable limit under the Zoning Ordinance, and grant one or more incentives or concessions for the production of housing units.

Density Bonus

The percentage density bonus for low income, very-low income and moderate income units is detailed in the tables found in sub-section (f) of GC 65915. The more low-income units provided, the greater the density bonus up to a maximum of 35 percent. Since 14 percent (three of 21 units) of the project units are designated for low income households, the applicant is entitled to a 26 percent density bonus or six additional units. While this would allow for a maximum of 27 residential units on the site, the applicant is requesting approval of 26 units. Per GC 65915, the applicant must agree to restrict the low-income units for at least 30 years.

Incentives

An applicant that has applied for a State density bonus may submit a proposal for specific incentives. An incentive means any of the following:

1. A reduction in site development standards or a modification of zoning code requirements or architectural design requirements that result in identifiable, financially sufficient, and actual cost reductions.
2. Approval of mixed use zoning in conjunction with a housing project.
3. Other regulatory incentives proposed by the developer that result in identifiable, financially sufficient, and actual cost reductions.

The number of incentives a project is entitled to depends on the percentage of low, very-low or moderate income units provided (no incentive is provided for the provision of non-income restricted senior housing units). In this case, the applicant is entitled to one incentive because the project includes at least 10 percent of total units for low income households. Per GC 65915, the City shall grant the incentive requested by the developer, unless the City makes a written finding, based upon substantial evidence, of any of the following:

1. The incentive is not required to provide for affordable housing costs as defined in Health and Safety Code Section 50052.5.
2. The incentive would have a specific adverse impact, as defined in Government Code Section 65589.5(d)(2), upon public health and safety or the physical environment or any real property listed in the California Register of Historic Places.
3. The incentive would be contrary to federal or state law. (GC 65915(d)(1))

The applicant has identified the requested incentive to have the maximum allowable Floor Area Ratio (FAR) for the C-4 (ECR) zoning district be 75 percent. Per the existing

C-4 (ECR) zoning district regulations, the maximum allowed FAR (for non-office uses only) is 55 percent, except that an FAR not exceeding 75 percent may be authorized by a use permit. In this case, a use permit to obtain the 75 percent FAR would not be required, if granted as an incentive per GC 65915. The incentive shall be granted unless a finding based on one of the three criteria noted above is made.

Development Standard Waivers

In addition to an incentive, the applicant is entitled to development standard waivers if the application of a development standard would physically preclude construction of a project that includes lower income housing. There is no specific limit on the number of development standard waivers that an applicant may request. Furthermore, the City is obligated to grant the requested development standard waiver(s), unless it can find that the waiver would have a specific adverse impact, as defined in Government Code Section 65589.5(d)(2), upon public health and safety or the physical environment or any property listed on the California Register of Historical Places or would be contrary to federal or state law.

The applicant is proposing a total of six development standard waivers, including five waivers in the R-3 zoning district, including modifications to the rear setback, separation between buildings (on adjacent sites), building coverage, FAR, and landscaping, and one waiver in the C-4 (ECR) zoning district regarding building FAR. While the requested incentive would allow the floor area ratio in the C-4 (ECR) zoning district to increase to 75 percent without the need for a use permit, a 90 percent floor area ratio is what would be required to physically enable the construction of the proposed number of units; therefore, it is necessary to apply a development standard waiver for floor area ratio in the C-4 (ECR) zoning district in addition to the requested incentive discussed above.

The following tables summarize the Zoning Ordinance development standards of the R-3 and C-4(ECR) zoning districts, and compare them with the proposed development. The development standards for which waivers are requested are highlighted.

R-3 Zoning District Comparison

	PROPOSED PROJECT		R-3 ZONING ORDINANCE	
Lot area	11,146	sq. ft.	7,000	sq. ft.
Lot width	59.7	ft.	80	ft. min.
Lot depth	158.3	ft.	100	ft. min.
Front setback (College)	20	ft.	20	ft. min.
Rear setback	3.3	ft.	15	ft. min.
Right side setback	15	ft.	10	ft. min.
Left side setback (ECR)	10	ft.	10	ft. min.
Between building setbacks (on-site)	Attached		20	ft. min.
Between building setbacks (adjacent sites)	6.7	ft.	20	ft. min.
Building coverage	4,983	sf	3,343	sf max.
	44.7	%	30	% max.
FAR (Floor Area Ratio)	8,231	sf	5,015	sf max.
	73.8	%	45	%
Building height	27.9	ft.	35	ft. max.
Landscaping	42.9	%	50	% min.
Paving	12.4	%	20	% max.
Balcony	No balconies		20	ft. from the side property line when abutting single-family residences

C-4(ECR) Zoning District Comparison

	PROPOSED PROJECT		C-4(ECR) ZONING ORDINANCE	
Lot area	42,516	sq. ft.	10,000	sq. ft.
Lot width	189.5	ft.	75	ft. min.
Lot depth	128.6	ft.	125	ft. min.
Front setback (ECR)	3.9 to 16	ft.	0	ft. min.
Rear setback	5.3	ft.	0	ft. min.
Right side setback	2	ft.	0	ft. min.
Left side setback	2.4	ft.	0	ft. min.
Building coverage	19,571	sf	42,516	sf max.
	46.0	%	100	% max.
FAR (Floor Area Ratio)	38,350	sf	31,887	sf max.
	90.2	%	75	%
Building height	30	ft.	30	ft. max.
Landscaping	28.3	%	10	% min.

Parking

GC 65915(p) provides that no city shall require a vehicular parking ratio, inclusive of handicapped and guest parking, for a development with at least 10 percent low-income units that exceeds the following ratios:

1. Zero (0) to one (1) bedroom, one (1) parking space.
2. Two (2) or three (3) bedroom, two (2) parking spaces.
3. Four (4) or more bedrooms, two and one-half (2.5) parking spaces.

The on-site parking requirement can be met through tandem or uncovered parking spaces. The application of GC 65915 differs from the City’s Zoning Ordinance, which requires two parking spaces (one covered and the second either covered or uncovered) per dwelling unit, and each space must be independently accessible and not located within the front or side setback. However, parking standards per GC 65915 preempt local parking requirements.

The applicant is proposing 62 parking spaces, consisting of a mix of 34 covered spaces, 18 covered tandem spaces, and 10 uncovered guest parking spaces. Under GC 65915, the required number of parking spaces is 57 spaces as shown in the table below.

Proposed Parking

	Number of Units in Proposed Project	Number of Parking Spaces Required Per GC 65915
0-1 bedrooms (1 space)	0	0
2-3 bedrooms (2 spaces)	17	34
4 or more bedrooms (2.5 spaces)	9	22.5
TOTAL	26	57*

*Per GC 65915, the total number of parking spaces shall be rounded up to the next whole number.

EI Camino Real/Downtown Specific Plan Comparison

The proposed project is located within the project area for the EI Camino Real/Downtown Specific Plan. On June 12, 2012, the City Council completed all actions necessary to approve the Specific Plan, which went into effect on July 12, 2012. However, the project application was deemed complete prior to final action on the Specific Plan, and therefore, the project would not be subject to its rules and regulations.

Although the applicant intends to pursue the proposal under the General Plan and Zoning Ordinance land use designations prior to the adoption of the EI Camino Real/Downtown Specific Plan, and in accordance with the State Density Bonus law, this

section of the report provides an overview of how the proposed project would relate to the El Camino Real/Downtown Specific Plan. This section is for reference purposes only.

Under the Specific Plan, the project site is located in the El Camino Real Mixed Use land use designation and the El Camino Real South-West (ECR SW) zoning district. The El Camino Real Mixed Use land use designation allows for a mix of retail, service, residential, and public and semi-public uses. Residential dwelling units are a permitted use within the designation.

The El Camino Real South-West (ECR SW) zoning district establishes a base maximum intensity (FAR) of 110 percent and base maximum density of 25 dwelling units per acre. The base intensity and density may be exceeded up to a maximum intensity of 150 percent and maximum density of 40 dwelling units per acre with the provision of public benefit. With a proposed overall FAR of 87 percent and density of 21.1 dwelling units per acre for the entire project site, the proposed project would be consistent with the base intensity and density.

The ECR SW zoning district restricts height to 38 feet, although façade heights on all but the interior side of a lot are limited to 30 feet. Above the 30 foot maximum façade height, a 45-degree profile is required. All of the buildings of the proposed project are 30 feet or less and therefore, would meet the façade height limit.

The ECR SW zoning district requires setbacks along front and street sides of corner lots of between seven and 12 feet. Rear setbacks are required to be a minimum of 20 feet and interior side setbacks may range from a minimum of five to a maximum of 25 feet. The setback range is intended to provide flexibility to allow each development to optimize building placement according to a specific situation. Additionally, sidewalk widths along El Camino Real are required to be a minimum of 12 feet, which includes a minimum eight-foot wide walking zone and a minimum four-foot wide area for street furnishings.

The proposed project would provide sidewalks that are six feet in width along El Camino Real and College Avenue, which is wider than existing, and would maintain the existing four-foot wide sidewalk along Partridge Avenue. With the proposed building setbacks of 3.9 feet to 16 feet along El Camino Real, it would not be possible to achieve an eight-foot wide sidewalk area along the entire length of the project's El Camino Real frontage. Areas with larger front setbacks would be landscaped. The interior side setback would be met, but the rear setback of 20 feet would not be met.

With regard to parking, the ECR SW zoning district requires a ratio of 1.85 spaces per dwelling unit for a total of 49 spaces where 62 spaces is being proposed. Finally, the ECR SW district requires a minimum of 30 percent open space with additional provisions for private open space. The proposed common open space would be approximately 20.6 percent of the lot area and private open space would be

approximately an additional 13.5 percent, for a total combined open space of approximately 34.1 percent.

FISCAL ANALYSIS

The City's independent economic consultant, Bay Area Economics (BAE) prepared a Fiscal Impact Analysis (FIA), projecting the potential changes in fiscal revenues and service costs directly associated with development of the proposed project. The FIA evaluated the potential fiscal impacts of the proposed project for the City of Menlo Park and other governmental entities that serve the project site including the Menlo Park Fire Protection District, Bear Gulch Water District, West Bay Sanitary District, Elementary and High School Districts, San Mateo County Community College District, San Mateo County Office of Education Special District, the Midpeninsula Regional Open Space District, and the Sequoia Health Care District.

The FIA found that the proposed project would have a slight positive fiscal impact (surplus) for the City's General Fund. All special districts would experience a positive net fiscal impact from the proposed project, except for the Menlo Park City Elementary School District and the Sequoia Union High School District. The payment of one-time impact fees and capital facilities charges to the City and special districts would total approximately \$1.1 million.

The Draft FIA was released on May 21, 2012 for a public comment period that ended on June 19, 2012. No comments were received during the public comment period; therefore, the Draft FIA has been finalized with no changes.

The FIA does not require action by the City Council. The City Council should consider the FIA in reviewing the proposed project.

ENVIRONMENTAL REVIEW

The California Environmental Quality Act (CEQA) requires the evaluation of the potential impacts of projects that will result in a physical change in the environment. In accordance with CEQA, the preparation of an EIR is required when a project has the potential to result in a significant environmental impact that cannot be mitigated to a less than significant level. The purpose of an EIR is to inform City decision-makers, responsible agencies, and the general public of the potential environmental impacts associated with a project, and will be used by the City and the public in their review of the proposed project and associated approvals.

The EIR for the 389 El Camino Real project evaluates 16 topic areas as required by CEQA for potential project impacts. A Notice of Preparation (NOP) was circulated on February 16, 2011 to notify responsible agencies and interested parties that an EIR would be prepared for the proposed project. Based on the verbal comments presented at the EIR scoping session at the Planning Commission meeting on February 28, 2011 and written communication received during the EIR scoping period, six out of the 16

environmental topic areas were identified as potential areas of controversy surrounding the project. Particular focus on the analysis of these six topic areas are addressed in separate sections of the EIR and include the following:

- Land Use and Planning Policy
- Public Services and Utilities
- Air Quality
- Noise
- Aesthetics
- Transportation, Circulation and Parking

The following topics are not evaluated in detail in the EIR: agriculture and forestry resources; biological resources; cultural resources; geology and soils; greenhouse gas emissions; hazards and hazardous materials; hydrology and water quality; mineral resources; population and housing; and recreation. These topics are discussed together in the Effects Found Not to be Significant section of Chapter VI – Other CEQA Considerations in the Draft EIR.

The EIR identifies that the proposed project will result in potentially significant impacts in the Transportation, Air Quality, Noise, and Aesthetics categories. Impacts in all categories, with the exception of Transportation impacts, will be mitigated to a less than significant level. Impacts in the Transportation category are significant and unavoidable. A complete list of impacts and mitigation measures is included in Chapter II – Summary of the Draft EIR. A comprehensive table of all potential environmental impacts and associated mitigation measures can be found in Table II-2, which begins on page 9. Additionally, the significant and unavoidable transportation impacts were explained in detail in the March 19, 2012 Planning Commission staff report and are summarized below.

The road segment analysis found that the proposed project would result in three significant unavoidable transportation impacts:

- 1) In the Near Term Plus Project Condition, the proposed project would add 68 vehicles to the roadway segment of University Drive between Middle Avenue and Cambridge Avenue, which exceeds the City's 25-trip threshold for local roadways with Average Daily Trips (ADT) greater than 1,350 vehicles;
- 2) In the Long Term Plus Project Condition, the proposed project would add 68 vehicles to the roadway segment of University Drive between Middle Avenue and Cambridge Avenue, which exceeds the City's 25-trip threshold for local roadways with ADT greater than 1,350 vehicles; and,
- 3) In the Long Term Plus Project Condition, the proposed project would add 52 vehicles to the roadway segment of Middle Avenue between University Drive and El Camino Real, which exceeds the City's 50-trip threshold for collector roadways with ADT greater than 9,000 vehicles.

Comments received on the Draft EIR, both at the March 19, 2012 Planning Commission study session on the project and public hearing on the Draft EIR and in writing during the public review period, are addressed in the Response to Comments document that was circulated on June 14, 2012.

The key differences between the Draft EIR and the Final EIR are related to transportation and public services, and are summarized below:

- Transportation: As a result of comments by David Roise and Planning Commissioners at the March 19, 2012 study session and public hearing, traffic analysis of the project's potential impacts to the intersection of El Camino Real and Sand Hill Road, and potential traffic through Creek Drive were evaluated. In both cases, the traffic analysis determined that the project would not result in any new impacts upon the roadway system beyond those impacts described in the Draft EIR.
- Public Services: Several Planning Commissioners at the March 19, 2012 study session and public hearing expressed interest in the potential student generation rate from the proposed project, and had asked staff to look into the actual student generation rates from the 110 and 175 Linfield Drive residential project, which features units of a size comparable to the units in the proposed project. Based on current student enrollment data from the Menlo Park City School District and the Sequoia Union High School District, the actual current student enrollment rate at the 56-unit Linfield Drive project is 15 students (13 elementary/middle school students, and two high school students). As a comparison, the Draft EIR estimated that a total of 15 students would be generated from the proposed project based on the student generation rates provided by the school districts. The project's potential impacts to schools remain less than significant.

The responses and revision in the Final EIR substantiate and confirm the analysis contained in the Draft EIR. No new significant environmental impacts, no new significant information, and no increase in the significance of an already-identified impact have resulted from responding to comments.

Additionally, the Response to Comments includes text revisions that are intended to provide clarification and include previously omitted appendices, but do not identify a new impact or increase in the significance of an already-identified impact. The Draft EIR and the Response to Comments document together constitute the Final EIR. Given the significant and unavoidable impacts associated with the project, the City Council would be required to adopt a Statement of Overriding Considerations, if it determines that the project's benefits outweigh its environmental impacts. A draft Resolution Certifying the Final EIR, Adopting the Statement of Overriding Considerations and Adopting the Mitigation Monitoring and Reporting Program is included as Attachment B. The Mitigation Monitoring and Reporting Program is included as Exhibit A in Attachment B.

CORRESPONDENCE

Correspondence received on the Draft EIR during the public review period has been incorporated into the Final EIR's Response to Comments document. Since the June 25, 2012 Planning Commission public hearing, the City has not received any additional correspondence.

IMPACT ON CITY RESOURCES

The project sponsor is required to pay planning permit fees, based on the City's Master Fee Schedule, to fully cover the cost of staff time spent on the review of the project. The project sponsor is also required to bear the cost of the associated environmental review and fiscal analysis. For the environmental review and fiscal analysis, the project sponsor deposits money with the City and the City pays the consultants.

As noted previously, the project FIA found that the proposed project is projected to have a slight positive fiscal impact (surplus) for the City's General Fund.

POLICY ISSUES

The Project does not require an amendment to the City's General Plan. The primary policy issues for the City Council to consider while reviewing the project are whether the required use permit, architectural control, and related findings can be made. In addition, the City Council should consider the benefits of the project in relation to the significant and unavoidable environmental impacts.

CONCLUSION

The proposed project would occupy an existing underutilized site and provide higher density housing near Downtown, including providing three low-income housing units for the City. The applicant has redesigned the project to accommodate the requests of the Neighborhood Task Force by shifting the height and mass of buildings away from the neighbors, changing the architectural style to blend more appropriately with the Allied Arts neighborhood, and improving the quality of the building materials and finishes.

As part of the review of the project, an Environmental Impact Report was prepared, which determined that the project would result in significant and unavoidable impacts related to transportation. Staff believes that the project includes substantial benefits that outweigh its significant, and adverse environmental impacts. As such, staff recommends that the City Council concur with the recommendation of the Planning Commission and adopt a resolution certifying the Environmental Impact Report, adopting the Statement of Overriding Considerations, and adopting the Mitigation Monitoring and Reporting Program. Staff further recommends that the Council approve the Use Permit, Architectural Control, major subdivision, BMR Housing Agreement, and application of the State Density Bonus Law. Conditions of approval are included in Attachment C.

Signature on file
Thomas Rogers
Senior Planner

Signature on file
Arlinda Heineck
Community Development Director

PUBLIC NOTICE

Public notification consisted of publishing a notice in the local newspaper and notification by mail of owners and occupants in the area bounded by El Camino Real, Harvard Avenue, University Drive, and Middle Avenue, and residents on Morey Drive and Kenwood Drive. In addition, the 389 El Camino Real project page is available at the following web address: http://www.menlopark.org/projects/comdev_389ecr.htm.

This page provides up-to-date information about the project, allowing interested parties to stay informed of its progress. The page allows users to sign up for automatic email bulletins, notifying them when content is updated. Previous staff reports and other related documents are available for review on the project page.

ATTACHMENTS

- A. Draft Findings and Actions for Approval
- B. Draft Resolution Certifying the Final Environmental Impact Report, Adopting the Statement of Overriding Consideration and Adopting the Mitigation Monitoring and Reporting Program
- C. Draft Conditions of Approval
- D. Draft Below Market Rate (BMR) For-Sale Agreement for 389 El Camino Real
- E. Location Map
- F. Project Plans
- G. State Density Bonus Law
- H. Planning Commission – Approved Excerpt Minutes from the Meeting of June 25, 2012

Note: Attached are reduced versions of maps and diagrams submitted by the applicants. The accuracy of the information in these drawings is the responsibility of the applicants, and verification of the accuracy by City Staff is not always possible. The original full-scale maps, drawings and exhibits are available for public viewing at the Community Development Department.

EXHIBITS TO BE PROVIDED AT MEETING

Color and Materials Boards

DOCUMENTS AVAILABLE FOR REVIEW AT CITY OFFICES AND WEBSITE

- Planning Commission Staff Report for the meeting of June 25, 2012
- Final Environmental Impact Report (EIR)
- Fiscal Impact Analysis (FIA)

ATTACHMENT A - DRAFT

FINDINGS AND RECOMMENDED ACTIONS FOR APPROVAL

389 El Camino Real Project

The Planning Commission recommends that the City Council take the following actions:

Environmental Review

1. Adopt a Resolution of the City Council of the City of Menlo Park, State of California, Certifying the Final Environmental Impact Report (EIR), Adopting the Statement of Overriding Considerations and Adopting the Mitigation Monitoring and Reporting Program for the 389 El Camino Real Project for the 389 El Camino Real Project (Attachment B).

State Density Bonus Law

2. Apply the State Density Bonus Law to allow one incentive and six development standard waivers.

Use Permit

3. Make findings, as per Section 16.82.030 of the Zoning Ordinance pertaining to the granting of use permits, that the proposed use will not be detrimental to the health, safety, morals, comfort and general welfare of the persons residing or working in the neighborhood of such proposed use, and will not be detrimental to property and improvements in the neighborhood or the general welfare of the City.
4. Approve the use permit for construction of three or more units in the R-3 zoning district and for new construction of residential units in the C-4(ECR) zoning district subject to the Conditions of Approval (Attachment C).

Architectural Control

5. Adopt the following findings, as per Section 16.68.020 of the Zoning Ordinance, pertaining to architectural control approval:
 - a. The general appearance of the structures is in keeping with the character of the neighborhood;
 - b. The development will not be detrimental to the harmonious and orderly growth of the City;
 - c. The development will not impair the desirability of investment or occupation in the neighborhood; and,

- d. The development provides adequate parking as required in all applicable City Ordinances and has made adequate provisions for access to such parking.
6. Approve the architectural control for the proposed design of the new buildings and site improvements subject to the Conditions of Approval (Attachment C).

Major Subdivision

7. Make findings that the proposed major subdivision is technically correct and in compliance with all applicable State regulations, City General Plan, Zoning and Subdivision Ordinances, and the State Subdivision Map Act.
8. Approve the request for a Tentative Map to merge seven lots into two lots, abandon the public street easement for Alto Lane, and create 26 residential condominium units.

Below Market Rate Housing Agreement

9. Approve the Below Market Rate Housing Agreement to provide three on-site BMR units in accordance with the City's Below Market Rate Housing Program and State Density Bonus Law (Attachment D).

RESOLUTION NO.

RESOLUTION OF THE CITY COUNCIL OF THE CITY OF MENLO PARK CERTIFYING THE FINAL ENVIRONMENTAL IMPACT REPORT AND ADOPTING FINDINGS, A STATEMENT OF OVERRIDING CONSIDERATIONS AND A MITIGATION MONITORING AND REPORTING PROGRAM FOR THE PROPERTY LOCATED AT 389 EL CAMINO REAL

WHEREAS, 389 El Camino Real, LLC (“Project Sponsor”) proposes to redevelop an approximately 1.23-acre site (consisting of seven legal parcels) located at 389 El Camino Real, Menlo Park (“Project Site”); and

WHEREAS, the Project Sponsor would demolish an existing one-story single family residence and a triplex and develop 26 residential units and associated parking, facilities and landscaping, including approximately 18,315 square feet of open space that comprises approximately 34 percent of the Project Site (“Project”); and

WHEREAS, all of the residential units would be for sale, and three of the 26 units would be priced at affordable levels for low-income households, in accordance with the City’s Below Market Rate (“BMR”) Housing Program and the provisions of Government Code Section 65915, State Density Bonus Law; and

WHEREAS, for purposes of compliance with the provisions of the California Environmental Quality Act (“CEQA”) and the CEQA Guidelines, on February 16, 2011, a Notice of Preparation (“NOP”) was circulated notifying responsible agencies and interested parties that an Environmental Impact Report (“EIR”) would be prepared for the Project; and

WHEREAS, the City of Menlo Park (“City”) held a scoping meeting before the Planning Commission during the NOP comment period, on February 28, 2011, to receive comments from the public and interested public agencies on issues that should be addressed in the EIR; and

WHEREAS, the City published a Draft EIR (SCH #201102207) on February 17, 2012, and provided a 46-day public comment period lasting until April 2, 2012; and

WHEREAS, the City filed the Draft EIR with the California Office of Planning and Research and made copies of the Draft EIR available at the Community Development Department, on the City’s website, and at the Menlo Park Library; and

WHEREAS, the City’s Planning Commission held a public hearing on the Draft EIR for the Project on March 19, 2012; and

WHEREAS, all comments on the Draft EIR concerning environmental issues received during the public comment period were evaluated and responded to in writing by the

City as the Lead Agency in accordance with Section 15088 of the CEQA Guidelines;
and

WHEREAS, the comments on the Draft EIR and the written responses were packaged into a Response to Comments Document that was published on June 14, 2012, and the City made copies of the Response to Comments Document available at the Community Development Department, on the City's website, and at the Arrillaga Family Recreation Center; and

WHEREAS, the Draft EIR and the Response to Comments Document comprise the Final EIR; and

WHEREAS, CEQA and the CEQA Guidelines require a written analysis and conclusions regarding the Project's environmental impacts, mitigation measures and Project alternatives that, in the City's view, justify approval of the Project; and

WHEREAS, after notice having been lawfully given, a public hearing was scheduled before the City's Planning Commission on June 25, 2012, whereat all persons interested therein might appear and be heard; and

WHEREAS, on June 25, 2012, the City's Planning Commission, after having fully reviewed, considered and evaluated all the testimony and evidence submitted in this matter, voted affirmatively to recommend that the City Council find that the Final EIR for the Project was prepared in compliance with CEQA, certify the Final EIR for the Project pursuant to CEQA, make the findings required by CEQA, adopt the Statement of Overriding considerations, and adopt the Mitigation Monitoring and Reporting Program ("MMRP"); and

WHEREAS, after notice having been lawfully given, a public hearing was scheduled before the City Council on July 31, 2012, whereat all persons interested therein might appear and be heard; and

WHEREAS, on July 31, 2012, after closing the public hearing, the City Council of the City of Menlo Park having fully reviewed, considered and evaluated all the testimony and evidence submitted in this matter voted affirmatively to find that the Final EIR for the Project was prepared in compliance with CEQA, certify the Final EIR for the Project pursuant to CEQA, make the findings required by CEQA, adopt the Statement of Overriding considerations, and adopt the MMRP.

NOW, THEREFORE, the City of Menlo Park, acting by and through its City Council, hereby resolves as follows:

1. The City Council of the City of Menlo Park hereby certifies the Final EIR for the Project pursuant to CEQA.

2. The City Council of the City of Menlo Park hereby makes the following findings with respect to the significant effects on the environment as identified in the Final EIR for the Project and adopts the MMRP for the Project:

I. RECORD OF PROCEEDINGS

For purposes of CEQA and the findings set forth herein, the record of proceedings for the City's findings and determinations consists of the following documents and testimony, at a minimum:

- a. The Final EIR for the Project and all related reports, documents, studies, memoranda, and maps.
- b. The NOP and other public notices issued by the City in conjunction with the EIR for the Project.
- c. All written and oral comments submitted by agencies or members of the public during the public review period for the EIR and any public hearings or meetings held on Project approvals.
- d. All other public reports, documents, studies, memoranda, maps, or other planning documents related to the Project prepared by the City, consultants to the City, or responsible or trustee agencies with respect to the City's compliance with the requirements of CEQA and the Project entitlements.
- e. All matters of common knowledge to this Planning Commission and City Council, including, but not limited to:
 - i. The City's General Plan and other applicable policies;
 - ii. The City's Zoning Ordinance and other applicable ordinances;
 - c. Information regarding the City's fiscal status; and
 - d. Applicable City policies and regulations.

The documents described above comprising the record of proceedings are located in the Community Development Department, City of Menlo Park, 701 Laurel Street, Menlo Park, CA 94025. The custodian of these documents is the Community Development Director or her designee.

II. FINDINGS FOR SIGNIFICANT IMPACTS AVOIDED OR MITIGATED TO A LESS-THAN-SIGNIFICANT LEVEL

The Final EIR for the Project concluded that there would be significant environmental impacts. The City finds that by incorporating into the Project all the mitigation measures outlined in the MMRP, the impacts discussed below are reduced to a less-than-significant level.

A. Air Quality

Impact AIR-1: Construction of the Project would generate air pollutant emissions that could expose sensitive receptors to substantial pollutant concentrations.

Mitigation Measure AIR 1: Consistent with guidance from the Bay Area Air Quality Management District (“BAAQMD”), the following actions shall be required of construction contracts and specifications for the Project:

- All exposed surfaces (e.g., parking areas, staging areas, soil piles, graded areas, and unpaved access roads) shall be watered two times per day.
- All haul trucks transporting soil, sand, or other loose material off-site shall be covered.
- All visible mud or dirt tracked-out onto adjacent public roads shall be removed using wet power vacuum street sweepers at least once per day. The use of dry power sweeping is prohibited.
- All vehicle speeds on unpaved roads shall be limited to 15 miles per hour.
- All roadways, driveways, and sidewalks to be paved shall be completed as soon as possible.
- Building pads shall be laid as soon as possible after grading unless seeding or soil binders are used.
- Idling times shall be minimized either by shutting equipment off when not in use or reducing the maximum idling time to five minutes (as required by the California airborne toxics control measure, Title 13, Section 2485 of the California Code of Regulations). Clear signage shall be provided for construction workers at all access points.
- All construction equipment shall be maintained and properly tuned in accordance with the manufacturer’s specifications. All equipment shall be checked by a certified mechanic and determined to be running in proper condition prior to operation.
- A publicly visible sign shall be posted with the telephone number and person to contact at the City regarding dust complaints. This person shall respond and take corrective action within 48 hours. The BAAQMD’s phone number shall also be visible to ensure compliance with applicable regulations.

FINDINGS: Based upon the Final EIR for the Project and the entire record before the Planning Commission and City Council, this City Council finds that:

Effects of Mitigation: Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect related to construction period emissions, as identified in the Final EIR for the Project. The City finds that the implementation of air quality control measures during the construction period, in conformance with guidance from BAAQMD, is feasible and will reduce the temporary construction-period impacts related to air pollution to a less-than-significant level.

Remaining Impacts: Any remaining impacts related to construction period emissions would not be significant.

Impact AIR 2: Construction of the Project would generate air pollutant emissions that could expose sensitive receptors to substantial toxic air contaminants.

Mitigation Measure AIR 2: Consistent with guidance from the BAAQMD, the following actions shall be required of construction contracts and specifications for the Project:

- The construction contractor shall ensure the idling time of diesel-powered construction equipment is two minutes or less.
- The construction contractor shall utilize off-road equipment (more than 50 horse-power) used in the construction of the Project (i.e., owned, leased, and subcontractor vehicles) that achieves a project wide fleet-average 20 percent nitrogen oxide reduction and 45 percent particulate matter reduction compared to the most recent ARB fleet average. Acceptable options for reducing emissions include the use of late model engines, low-emission diesel products, alternative fuels, engine retrofit technology, after-treatment products, add-on devices such as particulate filters, and/or other options that are available.
- All construction equipment, diesel trucks, and generators shall be equipped with Best Available Control Technology for emission reductions of nitrogen oxides and particulate matter.
- The Project construction contractor shall use equipment that meets the ARB's most recent certification standard for off-road heavy duty diesel engines.

FINDINGS: Based upon the Final EIR for the Project and the entire record before the Planning Commission and City Council, this City Council finds that:

Effects of Mitigation: Changes or alterations have been required in, or incorporated into the Project that avoid or substantially lessen the significant environmental effect related to toxic air contaminants as identified in the Final EIR for the Project. The efficient machinery required to be used as part of Mitigation Measure AIR-2 would result in measurable reductions in toxic air contaminant emissions compared to standard equipment. The City finds that the implementation of air quality control measures during the construction period designed to reduce diesel exhaust and other toxic air contaminants, in conformance with guidance from BAAQMD, is feasible and will reduce related impacts to a less-than-significant level.

Remaining Impacts: Any remaining impacts related to the reduction of diesel exhaust and other toxic air contaminants would not be significant.

B. Noise

Impact NOISE-1: Noise levels from Project construction activities could result in a substantial temporary or periodic increase in ambient noise levels in the Project Site vicinity above levels existing without the Project.

Mitigation Measure NOISE-1: The following measures shall be implemented during construction of the Project:

- (a) To minimize construction noise impacts on nearby residents and businesses, and to be consistent with Chapter 8.06 of the City's Municipal Code,

standard construction activities that exceed stated noise limits shall be permitted only between the hours of 8:00 a.m. and 6:00 p.m. from Monday to Friday.

(b) To reduce daytime construction-related noise impacts to the maximum feasible extent, the Project Sponsor shall develop a site-specific noise reduction program subject to City review and approval, which includes the following measures:

- Signs shall be posted at the construction site that include permitted construction days and hours, a day and evening contact number for the job site, and a day and evening contact number for the City. The signs shall be posted at all entrances to the construction site upon the commencement of construction for the purpose of informing contractors and subcontractors and all other persons at the construction site of the basic requirements of the Noise Ordinance of the Municipal Code. The signs shall be at least five feet above ground level and shall consist of a white background with black letters.
- A pre-construction meeting shall be held with the job inspectors and the general contractor/on-site project manager to confirm that noise mitigation protocols are in place prior to the issuance of a building permit (including the establishment of construction hours, neighborhood notification, posted signs, etc.).
- Equipment and trucks used for Project construction shall utilize the best available noise control techniques (e.g., improved mufflers, equipment redesign, use of intake silencers, ducts, engine enclosures, and acoustically attenuating shields or shrouds).
- Impact tools (e.g., jack hammers, pavement breakers, and rock drills) used for Project demolition or construction activities shall be hydraulically or electrically powered wherever possible to avoid noise associated with compressed-air exhaust from pneumatically powered tools. Where use of pneumatic tools is unavoidable, an exhaust muffler on equipment with compressed-air exhaust systems shall be used; this muffler can lower noise levels, which could achieve a reduction of 5 A-weighted decibels (“dBA”). Quieter procedures shall be used, such as drills rather than impact equipment, whenever feasible.
- Stationary noise sources shall be located as far from sensitive receptors as possible and they shall be muffled and enclosed within temporary sheds; or insulation barriers or other measures shall be incorporated to the extent feasible.
- No piece of powered equipment shall generate noise in excess of 85 dBA at 50 feet. Powered equipment is defined by the City to be a motorized device powered by electricity or fuel used for construction, demolition, and property or landscape maintenance or repairs. Powered equipment includes but is not limited to: parking lot sweepers, saws, sanders, motors, pumps, generators, blowers, wood chippers, vacuums, drills and nail guns (but specifically excluding internal fuel combustion engine leaf blowers).
- Prior to construction, a temporary sound barrier shall be constructed along the Project’s western property line adjacent to the existing residential properties that border the Project Site. The temporary sound barrier shall extend from the Project property line at College Avenue to the Project

property line at Partridge Avenue. This temporary sound barrier shall be constructed at the minimum height of six feet above the proposed finished pad elevation with a minimum surface weight of four pounds per square foot (or with any commercially available sound barrier material that has an equivalent noise reduction coefficient as a material with a minimum surface weight of four pounds per square foot) and shall be constructed so that vertical or horizontal gaps are eliminated. This temporary barrier shall remain in place through the construction phase in which heavy construction equipment, such as excavators, bulldozers, scrapers, loaders, rollers, pavers, and dump trucks are operating within 100 feet of the western Project Site boundary.

FINDINGS: Based upon the Final EIR for the Project and the entire record before the Planning Commission and City Council, this City Council finds that:

Effects of Mitigation: Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect related to construction period noise as identified in the Final EIR for the Project. Similar measures are routinely applied to development projects throughout the City and region. The City finds that the provisions for implementation of noise control/containment measures during the construction period for the Project are feasible and will reduce the temporary construction-period impact to noise levels to a less-than-significant level.

Remaining Impacts: Any remaining impacts related to construction noise would not be significant.

Impact NOISE-2: Implementation of the Project would expose future residents of the Project to noise levels that exceed the “normally acceptable” standard for new residential development established in the City’s Land Use Compatibility Standards for Community Noise Environments.

Mitigation Measure NOISE-2a: In order to ensure that windows can remain closed for prolonged periods of time to meet the interior noise standard of 45 dBA community noise equivalent level (“CNEL”) established by the City, an alternative form of ventilation, such as air conditioning or noise-attenuated passive ventilation systems, shall be included in all proposed dwelling units.

Mitigation Measure NOISE-2b: In order to meet the interior noise standard of 45 dBA CNEL established by the City, all proposed dwelling units that would be located within 45 feet of the centerline of the outermost travel lane of El Camino Real shall be constructed to have an overall minimum STC rating of STC-35, and all exterior doors and windows shall have a minimum rating of STC-33. Quality control shall be exercised in construction to ensure all air-gaps and penetrations of the building shell are controlled and sealed.

FINDINGS: Based upon the Final EIR for the Project and the entire record before the Planning Commission and City Council, this City Council finds that:

Effects of Mitigation: Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect on interior noise levels as identified in the Final EIR for the Project. These measures are standard construction practices that insulate interior spaces from exterior noise, including traffic noise. The City finds that the provisions for implementation of an alternative form of ventilation and minimum STC ratings, for buildings located within 45 feet of the centerline of the outermost travel lane, will reduce noise related to traffic to a less-than-significant level.

Remaining Impacts: Any remaining impacts related to traffic noise would not be significant.

Impact NOISE-2: Implementation of the Project could expose nearby existing land uses to unacceptable noise levels in violation of the City's Noise Ordinance (Chapter 8.06).

Mitigation Measure NOISE-3: The Project Sponsor shall ensure that Project plans submitted for a building permit include documentation that proposed stationary equipment shall not generate noise that exceeds 60 dBA equivalent continuous noise level ("Leq") during daytime hours and 50 dBA Leq during nighttime hours, as measured at any point on a neighboring residential property nearest where the noise source at issue generates the highest noise level.

FINDINGS: Based upon the Final EIR for the Project and the entire record before the Planning Commission and City Council, this City Council finds that:

Effects of Mitigation: Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect related to noise generated by stationary equipment as identified in the Final EIR for the Project. The careful location and shielding of mechanical equipment is a practical measure that will reduce potential adverse effects on the ambient noise environment. The City finds that the provisions for implementation of noise control/containment measures for stationary equipment for the Project are feasible and will reduce the operation period impact to noise levels to a less-than-significant level.

Remaining Impacts: Any remaining impacts related to operation period noise would not be significant.

C. Aesthetics

Impact AES-1: The Project could increase the amount of light and glare in Menlo Park.

Mitigation Measure AES-1: The Project Sponsor shall prepare a lighting plan and photometric study and submit them to the City for review and approval prior to issuance of a building permit. City staff shall review the plan to ensure that any outdoor lighting for the Project is oriented downwards and is designed to minimize lighting or glare off-site.

FINDINGS: Based upon the Final EIR for the Project and the entire record before the Planning Commission and City Council, this City Council finds that:

Effects of Mitigation: Changes or alterations have been required in, or incorporated into, the Project that avoid or substantially lessen the significant environmental effect related to light and glare as identified in the Final EIR for the Project. The careful design of lighting on the Project Site is a practical way to avoid glare and unnecessary light spillover. The City finds that the provisions for preparation of a lighting plan and photometric study are feasible and will reduce the impacts of the Project related to light and glare to a less-than-significant level.

Remaining Impacts: Any remaining impacts related to light and glare would not be significant.

III. FINDINGS AND RECOMMENDATIONS REGARDING SIGNIFICANT AND UNAVOIDABLE IMPACTS

The Final EIR for the Project concluded that there would be significant environmental impacts. The City finds that by incorporating into the Project all the mitigation measures outlined in the MMRP, impacts are reduced. However, even after mitigation, some impacts are significant and unavoidable. The City finds that there is no additional feasible mitigation that could be imposed beyond what is detailed herein. For the reasons set forth in the Statement of Overriding Considerations below, the City finds that there are economic, legal, social, technological or other benefits of the Project that override the significant and unavoidable impacts.

A. Transportation, Circulation and Parking

Impact TRANS-1: In the Near Term Plus Project Condition, the Project would contribute trips to University Drive between Middle Avenue and Cambridge Avenue that would exceed the City's 25-trip threshold for local roadways with ADT greater than 1,350 vehicles.

Mitigation Measure TRANS-1a: Additional roadway capacity may reduce this impact to a less-than-significant level. University Drive between Middle Avenue and Cambridge Avenue currently has one travel lane in each direction and obtaining additional roadway capacity could include constructing an additional travel lane in one or both travel directions. However, this measure would require right-of-way acquisition, which is infeasible. As such, the impact would remain significant and unavoidable.

Mitigation Measure TRANS-1b: The Project Sponsor shall develop and implement a Transportation Demand Management (“TDM”) Program to encourage the use of alternative modes of transportation and reduce the daily number of vehicles generated by the Project. The TDM Program shall be consistent with the City of Menlo Park TIA Guidelines. Potential TDM measures include the following:

- A commute assistance kiosk. A kiosk or bulletin board that provides information on alternative modes of transportation available in the area;
- Subsidized public transit passes. As part of homeowners or membership fees, a subsidized pass for public transit may be provided to residents;
- Carpool matching assistance. A person or database to link residents traveling to similar locations, to allow for carpooling;
- Vanpools. Vanpools are generally privately-sponsored and provide pick-up and drop-off services for commuters who work and live in the same general area;
- Shuttle service to area transit hubs. Privately-sponsored vehicles transport residents between dwelling units and area transit hubs such as SamTrans bus stops, BART Stations, and the Menlo Park Caltrain Station; and
- Bicycle facilities. For residential projects, these facilities would generally include bicycle racks and bicycle lockers (although residents would be expected to store bicycles in their residences).

FINDINGS: Based upon the Final EIR for the Project and the entire record before the Planning Commission and City Council, this City Council finds that:

Effects of Mitigation: Implementation of the mitigation measures above would not reduce the potentially significant traffic impacts to a less-than-significant level. Implementation of a TDM Program would incrementally reduce the impacts of the Project on roadway capacity, but not to a less-than-significant level. The addition of roadway capacity along University Drive between Middle Avenue and Cambridge Avenue would require the acquisition of private property and the disruption of the City’s residential neighborhoods, and was deemed infeasible.

Remaining Impacts: The impacts to the roadway capacity of University Drive between Middle Avenue and Cambridge Avenue would remain significant and unavoidable.

Impact TRANS-2: In the Long Term Plus Project Condition, the Project would contribute trips to University Drive between Middle Avenue and Cambridge Avenue that would exceed the City’s 25-trip threshold for local roadways with ADT greater than 1,350 vehicles.

Mitigation Measure TRANS-2: Implement Mitigation Measures TRANS-1a and TRANS-1b.

FINDINGS: Based upon the Final EIR for the Project and the entire record before the Planning Commission and City Council, this City Council finds that:

Effects of Mitigation: Implementation of the mitigation measures above would not reduce the potentially significant traffic impacts to a less-than-significant level. Implementation of a TDM Program would incrementally reduce the impacts of the Project on roadway capacity, but not to a less-than-significant level. The addition of roadway capacity along University Drive between Middle Avenue and Cambridge Avenue would require the acquisition of private property and the disruption of the City's residential neighborhoods, and was deemed infeasible.

Remaining Impacts: The impacts to the roadway capacity of University Drive between Middle Avenue and Cambridge Avenue would remain significant and unavoidable.

Impact TRANS-3: In the Long Term Plus Project Condition, the Project would contribute trips to Middle Avenue between University Drive and El Camino Real that would exceed the City's 50-trip threshold for collector roadways with ADT greater than 9,000 vehicles.

Mitigation Measure TRANS-3a: Additional roadway capacity would reduce this impact to a less-than-significant level. Middle Avenue between University Drive and El Camino Real currently has one travel lane in each direction and obtaining additional roadway capacity would include constructing an additional travel lane in one or both travel directions. However, this measure would require right-of-way acquisition, which is infeasible. As such, the impact would remain significant and unavoidable.

Mitigation Measure TRANS-3b: Implement Mitigation Measure TRANS-1b.

FINDINGS: Based upon the Final EIR for the Project and the entire record before the Planning Commission and City Council, this City Council finds that:

Effects of Mitigation: Implementation of the mitigation measures above would not reduce the potentially significant traffic impacts to a less-than-significant level. Implementation of a TDM Program would incrementally reduce the impacts of the Project on roadway capacity, but not to a less-than-significant level. The addition of roadway capacity along Middle Avenue between University Drive and El Camino Real would require the acquisition of private property and the disruption of the City's residential neighborhoods, and was deemed infeasible.

Remaining Impacts: The impacts to the roadway capacity of Middle Avenue between University Drive and El Camino Real would remain significant and unavoidable.

IV. PROJECT ALTERNATIVES

CEQA requires that EIRs assess feasible alternatives or mitigation measures that may substantially lessen the significant effects of projects prior to approval (Public

Resources Code Section 21002). With the exception of the “no project” alternative, the specific alternatives or types of alternatives that must be assessed are not specified. CEQA “establishes no categorical legal imperative as to the scope of alternatives to be analyzed in an EIR. Each case must be evaluated on its own facts, which in turn must be reviewed in light of the statutory purpose.” Citizens of Goleta Valley v. Board of Supervisors (1990) 52 Cal.3d. 553, 556. The legislative purpose of CEQA is to protect public health, welfare and the environment from significant impacts associated with all types of development by ensuring that agencies regulate activities so that major consideration is given to preventing environmental damage while providing a decent home and satisfying living environment for every Californian (Public Resources Code Section 21000).

CEQA Guidelines state that the “range of potential alternatives to the proposed project shall include those that could feasibly accomplish most of the basic purposes of the project and could avoid or substantially lessen one or more of the significant effects” of the project. CEQA Guidelines Section 15126(d)(2). Thus, an evaluation of the project objectives is key to determining which alternatives should be assessed in the EIR.

The main objective of the Project Sponsor is to develop a residential project that is economically feasible and contributes to the City’s housing stock. Other Project objectives are as follows:

- Redevelop an underutilized site with a mixture of attached and detached single-family units that is compatible with the surrounding neighborhood;
- Design the Project in a way that is sensitive to the character of the Allied Arts neighborhood to the west;
- Encourage in-fill development in the City and allow for a more vibrant mix and density of land uses;
- Provide housing opportunities, including affordable housing, for existing and future residents of Menlo Park;
- Create development that enhances the visual character of the El Camino Real corridor;
- Locate a project in close proximity to a regional transportation corridor with good local access from major streets and freeways; and
- Locate a project in close proximity (i.e., easy access by foot and/or bike) to transit services, and other major local and regional services and employment centers, including the Safeway grocery-shopping complex, the Stanford Shopping Center, the Stanford Hospital, and the Menlo Park Caltrain station.

The CEQA Guidelines state that the “range of potential alternatives to the proposed project shall include those that could feasibly accomplish most of the basic purposes of the project and could avoid or substantially lessen one or more of the significant effects” of the project. The City evaluated the alternatives listed below.

Alternative 1: No Project Alternative

The No Project alternative is discussed on pages 192 to 194 of the Draft EIR. The No Project alternative assumes re-occupancy of the triplex and single-family

residence on the site. The existing buildings and infrastructure would remain with minimal building upgrades.

The No Project alternative would eliminate many of the significant impacts associated with the Project, in that it would not result in ground-disturbing activities, new construction, or the development of new residential uses at the Project Site (and the associated generation of new vehicle trips). Therefore, the No Project alternative would avoid several impacts that could result from the Project, including: certain traffic impacts and congestion on local roadways; air quality impacts associated with the Project; exposure to noise from construction, traffic and stationary sources on the Project Site; and the creation of new light and glare. While the No Project alternative would be the environmentally superior alternative in the context of impact reduction, it would not meet the primary objectives of the Project. Specifically, it would not develop the Project Site with residential uses located in close proximity to a regional transportation corridor with access to transit, services, and regional job centers.

FINDINGS: The No Project alternative is rejected as an alternative because it would not achieve the primary objectives of the Project.

Alternative 2: Baseline Zoning Alternative

The Baseline Zoning alternative is discussed on pages 194 to 197 of the Draft EIR. The Baseline Zoning alternative assumes that all structures on the Project Site would be demolished and a mixture of uses would be developed that is in general conformance with the C-4(ECR) and R-3 zones within the site. Approximately 23,000 square feet of commercial space could be developed in a two-story building located adjacent to El Camino Real. Per the requirements of the Zoning Ordinance, 156 parking spaces would be required as part of the commercial component, some of which would be located in a parking structure. The remainder of the site would be developed with three single-family residential units, ranging in size from approximately 1,500 square feet to 2,000 square feet, which would contain two garage parking spaces each.

The Baseline Zoning alternative would have similar impacts to the Project resulting from construction and redevelopment on the Project Site and the location of the Project Site with respect to existing land uses. The majority of these impacts could be mitigated to a less-than-significant level, similar to the Project. However, this alternative would generate significant unavoidable transportation impacts beyond those identified for the Project. In the Near Term Condition and Long Term Condition, the alternative would be expected to result in impacts at local approaches to State-controlled intersections that would not occur with implementation of the Project. In addition, impacts would occur to roadway segments beyond the University Avenue and Middle Avenue segments that would be substantially adversely affected by the Project.

Also, because this alternative would result in a significant increase in vehicle trips compared to the Project, it would result in significant impacts to regional air quality and greenhouse gas emissions compared to those identified for the Project. Although the alternative would achieve some of the objectives of the Project, it would do so to a

lesser extent than the Project because housing development on the Project Site would be reduced.

FINDINGS: The Baseline Zoning alternative is rejected as an alternative because it would include the construction of commercial uses (which is not a project objective), would not substantially reduce the environmental impacts of the Project, and would result in significant congestion of roadway segments in the near and long term. The alternative would achieve some of the objectives of the Project, although not to the degree of the Project, because commercial uses would be substituted for some of the residential units that would be included as part of the Project. Therefore, the alternative would not expand the City's supply of residential uses to the extent of the Project.

Alternative 3: Reduced Residential Alternative

The Reduced Residential alternative is discussed on pages 197 to 200 of the Draft EIR. The Reduced Residential alternative is designed to avoid the Project's significant and unavoidable contribution to traffic volumes on University Drive between Middle Avenue and Cambridge Avenue in the Near Term Plus Project Condition and Long Term Plus Project Condition, and Middle Avenue between University Drive and El Camino Real in the Long Term Plus Project Condition. To avoid these impacts would require a reduction in the number of residential units on the site from a total of 26 residential units (including nine single-family units and 17 townhouse units) to a total of 12 residential units (including five single-family units and seven townhouse units). All existing structures on the Project Site would be demolished as part of the alternative.

In general, vehicle trip rates are closely tied to the density and intensity of a given use (along with user characteristics and other relevant factors). With less than half of the residential units of the Project, the Reduced Residential alternative would generate fewer trips and would thus avoid the significant transportation impacts of the Project. Other impacts, like the potential to expose adjacent sensitive receptors to air pollution, would be similar to the Project. This alternative would meet most of the Project objectives, although the objectives relating to the development of single-family housing on the Project Site and providing additional housing opportunities would be achieved to a lesser extent than the Project. In addition, this alternative would be inconsistent with the vision for the area as defined by the El Camino Real/Downtown Specific Plan (approved in June 2012) and other planning documents, which envision the development of more intense land uses along the El Camino Real corridor to promote increased walkability and transit use. Most importantly, the alternative would reduce the potential for higher-intensity housing along El Camino Real.

FINDINGS: The Reduced Residential alternative is rejected as an alternative. The alternative would meet most of the Project objectives, although the objectives relating to the development of single-family housing on the site and providing additional housing opportunities would be achieved to a lesser extent than the Project. The alternative would avoid the significant transportation impacts associated with the Project.

Alternative 4: Mixed Use Alternative

The Mixed Use alternative is discussed on pages 200 to 202 of the Draft EIR. This alternative assumes development of a mixed-use project similar to that envisioned in El Camino Real/Downtown Specific Plan (approved in June 2012): 22 multi-family residential units and approximately 13,400 square feet of commercial space. It is assumed the commercial space would be general retail space, occupied by one tenant or two to three smaller tenants.

The Mixed Use alternative would result in impacts similar to the Project related to construction and redevelopment of the Project Site and the location of the Project Site with respect to existing land uses. The majority of these impacts could be mitigated to a less-than-significant level, similar to the Project. However, the alternative would generate significant unavoidable transportation impacts beyond those identified for the Project. In the Near Term Condition and Long Term Condition, the alternative would be expected to result in impacts at local approaches to State-controlled intersections that would not occur with implementation of the Project. In addition, impacts would occur to roadway segments beyond the University Avenue and Middle Avenue segments that would be substantially adversely affected by the Project. Also, because the alternative would result in a significant increase in vehicle trips compared to the Project, it would result in significant impacts to regional air quality and delivery trucks operating within the Project Site could expose nearby residential uses to elevated levels of toxic air contaminants. This alternative would meet most of the Project objectives, although the objectives relating to the development of single-family housing on the site and providing additional housing opportunities would be achieved to a lesser extent than the Project.

FINDINGS: The Mixed Use alternative is rejected as an alternative to the Project. While the alternative would meet most of the Project objectives, with the exception of the objective relating to the development of single-family housing on the Project Site, it would not substantially reduce the impacts of the Project on the environment related to transportation. In addition, the objective regarding providing additional housing opportunities would not be achieved to the same extent as the Project.

Alternative 5: Senior Housing Alternative

The Senior Housing alternative is discussed on pages 202 to 204 of the Draft EIR. The Senior Housing alternative assumes the development of 26 attached units restricted to seniors. The units would be approximately 1,000 square feet in size, on average, and would contain either one or two bedrooms. Because seniors typically drive less than non-seniors, and due to the proximity of the site to transit, the alternative would include only one parking space per unit plus nine guest spaces, for a total of 35 parking spaces.

Because seniors typically drive at lower rates than non-seniors, the Senior Housing alternative would generate less traffic than the Project and avoid the significant impacts to the local roadway system. Other impacts, like the potential to expose adjacent sensitive receptors to air pollution, would be similar to the Project. This

alternative would achieve all of the Project objectives, with the exception of the objective relating to the development of single-family housing.

While the Senior Housing alternative would not be infeasible on its face (senior housing developments of between 30 and 100 units exist in Menlo Park), it would be challenging to develop on the Project Site. According to correspondence from the Project Sponsor submitted during public review of the Draft EIR, the alternative would be difficult to develop or undesirable for the following reasons:

- The Project Sponsor is not capable of building senior housing on the Project Site due to the lack of past experience building such projects.
- A senior housing project would require on-site social and health services, which could not practicably be accommodated on the Project Site (and, if developed, would generate adverse traffic and air quality impacts).
- A senior housing project of 26 units would be on the low end of the critical mass of units needed to make such a project cost-effective.
- Single-story building structures or elevators in a higher-rise structure would be required on the site, neither of which would be feasible and/or cost-effective on the site.

FINDINGS: The Senior Housing alternative is rejected as an alternative to the Project. The alternative would avoid the significant unavoidable impacts of the Project on the local roadways system and promote City policies related to the development of housing along transit corridors and the provision of housing for seniors. However, it would not develop a mixture of attached and detached single-family residential units. Even though the Senior Housing alternative is identified as the environmentally superior alternative in the Draft EIR, it is not feasible for the Project Sponsor to develop senior housing on this site.

V. STATEMENT OF OVERRIDING CONSIDERATIONS RELATED TO THE PROJECT FINDINGS

The City Council of the City of Menlo Park adopts and makes the following Statement of Overriding Considerations regarding the significant unavoidable impacts of the Project. After review of the entire administrative record the City Council finds that pursuant to CEQA Section 21081(b) and CEQA Guidelines Section 15093, specific economic, legal, social, technological and other benefits of the Project outweigh the Project's unavoidable adverse impacts and the City Council finds that the significant and unavoidable adverse impacts are acceptable in light of the Project's benefits.

A. Significant Unavoidable Impacts

With respect to the foregoing findings and in recognition of those facts that are included in the record, the City has determined that the Project would result in significant unavoidable transportation impacts as disclosed in the Final EIR for the Project. The impacts would not be reduced to a less-than-significant level by feasible changes or alterations to the Project.

The City hereby finds that, where possible, changes or alterations have been required in or incorporated into the Project that substantially lessen the significant environmental effects identified in the Final EIR for the Project. The City further finds that there are no additional feasible mitigation measures that could be imposed to reduce and/or eliminate the significant and unavoidable impacts listed above. These impacts could not be reduced to a less than significant level by feasible changes, mitigations measures, or alterations to the Project.

B. Overriding Considerations

The City Council finds that each of the overriding considerations set forth below constitutes a separate and independent ground for a finding that the benefits of the Project outweigh its significant adverse environmental impacts and is an overriding consideration warranting approval of the Project. The City Council of the City of Menlo Park specifically adopts and makes this Statement of Overriding Considerations regarding the significant unavoidable impacts of the Project and the anticipated benefits of the Project.

The City Council has considered the Final EIR, the public record of proceedings on the Project, and other written materials presented to the City as well as oral and written testimony at all public hearings related to the Project, and does hereby determine that implementation of the Project as specifically provided for in the Project documents would result in the following substantial public benefits:

1. The Project will benefit the surrounding neighborhood through the conversion of an underutilized site into higher density residential uses that will increase pedestrian activity around El Camino Real, and provide more customers for local businesses and Downtown Menlo Park.
2. The Project will enhance the visual and community character of the surrounding area and El Camino Real corridor compared to existing conditions.
3. The Project will encourage the use of public transit, and walking and bicycling due to the site's location in close proximity to transit services, and other major local and regional services and employment centers.
4. The Project would provide housing opportunities, including affordable housing, for existing and future residents of Menlo Park and assist the City in meeting its Regional Housing Needs Allocation (RHNA) obligations.
5. The Project will generate new construction-related jobs in the City of Menlo Park.
6. The Project will contribute to the planned conversion of El Camino Real from an automobile-oriented commercial-style strip to a more compact, urban land use pattern.
7. The Project will encourage residential growth around transit, which is a recognized way to reduce per capita greenhouse gas emissions and combat global climate change.

VI. SEVERABILITY

If any term, provision, or portion of these Findings and Statement of Overriding Considerations or the application of these Findings and Statement of Overriding Considerations to a particular situation is held by a court to be invalid, void or unenforceable, the remaining provisions of these Findings and Statement of Overriding Considerations, or their application to other actions related to the Project, shall continue in full force and effect unless amended or modified by the City.

VII. ADOPTION OF THE MMRP

The City Council hereby adopts the mitigation measures set forth for the Project in the Final EIR and the MMRP attached hereto as Exhibit A and incorporated herein by this reference.

I, Margaret S. Roberts, City Clerk of Menlo Park, do hereby certify that the above and foregoing Council Resolution was duly and regularly passed and adopted at a meeting by said Council on the 31st day of July, 2012, by the following votes:

AYES:

NOES:

ABSTAIN:

ABSENT:

IN WITNESS WHEREOF, I have hereunto set my hand and affixed the Official Seal of said City on this thirty-first day of July, 2012.

Margaret S. Roberts, MMC
City Clerk

Table 1: Mitigation Monitoring and Reporting Program

Mitigation Measures	Monitoring Responsibility	Monitoring and Reporting Action	Monitoring Schedule	Non-Compliance Sanction
A. Land Use and Planning Policy				
<i>There are no significant land use and planning policy impacts.</i>				
B. Transportation, Circulation and Parking				
<p>TRANS-1a: Additional roadway capacity may reduce this impact to a less-than-significant level. University Drive between Middle Avenue and Cambridge Avenue currently has one travel lane in each direction and obtaining additional roadway capacity could include constructing an additional travel lane in one or both travel directions. However, this measure would require right-of-way acquisition, which is infeasible. As such, the impact would remain significant and unavoidable.</p>	Mitigation measure found infeasible	Mitigation measure found infeasible	Mitigation measure found infeasible	Mitigation measure found infeasible
<p>TRANS-1b: The project sponsor shall develop and implement a Transportation Demand Management (TDM) Program to encourage the use of alternative modes of transportation and reduce the daily number of vehicles generated by the project. The TDM Program shall be consistent with the City of Menlo Park TIA Guidelines. Potential TDM measures include the following:</p> <ul style="list-style-type: none"> • A commute assistance kiosk. A kiosk or bulletin board that provides information on alternative modes of transportation available in the area; • Subsidized public transit passes. As part of homeowners or membership fees, a subsidized pass for public transit may be provided to residents; • Carpool matching assistance. A person or database to link residents traveling to similar locations, to allow for carpooling; • Vanpools. Vanpools are generally privately-sponsored and provide pick-up and drop-off services for commuters who work and live in the same general area; • Shuttle service to area transit hubs. Privately-sponsored vehicles transport residents between dwelling units and area transit hubs such as SamTrans bus stops, BART Stations, and the Menlo Park Caltrain Station; and • Bicycle facilities. For residential projects, these facilities would generally include bicycle racks and bicycle lockers (although residents would be expected to store bicycles in their residences). <p>The TDM Program, which could be shared with that of other residential developments or businesses in the area, shall be reviewed and approved by the City.</p>	Planning Division and Transportation Division	Review and approve the TDM program	Prior to granting certificate of occupancy	No issuance of certificate of occupancy

Table 1 Continued

Mitigation Measures	Monitoring Responsibility	Monitoring and Reporting Action	Monitoring Schedule	Non-Compliance Sanction
<p><u>TRANS-2:</u> Implement Mitigation Measures TRANS-1a and TRANS-1b.</p> <p><u>TRANS-3a:</u> Additional roadway capacity would reduce this impact to a less-than-significant level. Middle Avenue between University Drive and El Camino Real currently has one travel lane in each direction and obtaining additional roadway capacity would include constructing an additional travel lane in one or both travel directions. However, this measure would require right-of-way acquisition, which is infeasible. As such, the impact would remain significant and unavoidable.</p> <p><u>TRANS-3b:</u> Implement Mitigation Measure TRANS-1b.</p>	<p>(Refer to Mitigation Measures TRANS-1a and TRANS-1b)</p> <p>Mitigation measure found infeasible</p> <p>(Refer to Mitigation Measure TRANS-1b)</p>	<p>(Refer to Mitigation Measures TRANS-1a and TRANS-1b)</p> <p>Mitigation measure found infeasible</p> <p>(Refer to Mitigation Measure TRANS-1b)</p>	<p>(Refer to Mitigation Measures TRANS-1a and TRANS-1b)</p> <p>Mitigation measure found infeasible</p> <p>(Refer to Mitigation Measure TRANS-1b)</p>	<p>(Refer to Mitigation Measures TRANS-1a and TRANS-1b)</p> <p>Mitigation measure found infeasible</p> <p>(Refer to Mitigation Measure TRANS-1b)</p>
<p>C. Air Quality</p> <p><u>AIR-1:</u> Consistent with guidance from the BAAQMD, the following actions shall be required of construction contracts and specifications for the project:</p> <ul style="list-style-type: none"> • All exposed surfaces (e.g., parking areas, staging areas, soil piles, graded areas, and unpaved access roads) shall be watered two times per day. • All haul trucks transporting soil, sand, or other loose material off-site shall be covered. • All visible mud or dirt tracked-out onto adjacent public roads shall be removed using wet power vacuum street sweepers at least once per day. The use of dry power sweeping is prohibited. • All vehicle speeds on unpaved roads shall be limited to 15 mph. • All roadways, driveways, and sidewalks to be paved shall be completed as soon as possible. • Building pads shall be laid as soon as possible after grading unless seeding or soil binders are used. • Idling times shall be minimized either by shutting equipment off when not in use or reducing the maximum idling time to 5 minutes (as required by the California airborne toxics control measure, Title 13, Section 2485 of the California Code of Regulations [CCR]). Clear signage shall be provided for construction workers at all access points. 	<p>Building Division</p>	<p>Review and approve the dust control plan. Verify that air quality control measures in Mitigation Measure AIR-1 are included.</p>	<p>Prior to issuance of building or grading permit</p>	<p>No issuance of building or grading permit</p>

Table 1 *Continued*

Mitigation Measures	Monitoring Responsibility	Monitoring and Reporting Action	Monitoring Schedule	Non-Compliance Sanction
<p><u>AIR-1</u> <i>Continued</i></p> <ul style="list-style-type: none"> All construction equipment shall be maintained and properly tuned in accordance with the manufacturer's specifications. All equipment shall be checked by a certified mechanic and determined to be running in proper condition prior to operation. A publicly visible sign shall be posted with the telephone number and person to contact at the City regarding dust complaints. This person shall respond and take corrective action within 48 hours. The BAAQMD's phone number shall also be visible to ensure compliance with applicable regulations. 				
<p><u>AIR-2:</u> Consistent with guidance from the BAAQMD, the following actions shall be required of construction contracts and specifications for the project:</p> <ul style="list-style-type: none"> The construction contractor shall ensure the idling time of diesel-powered construction equipment is 2 minutes or less. The construction contractor shall utilize off-road equipment (more than 50 horsepower) used in the construction of the project (i.e., owned, leased, and subcontractor vehicles) that achieves a project wide fleet-average 20 percent nitrogen oxide reduction and 45 percent particulate matter reduction compared to the most recent ARB fleet average. Acceptable options for reducing emissions include the use of late model engines, low-emission diesel products, alternative fuels, engine retrofit technology, after-treatment products, add-on devices such as particulate filters, and/or other options that are available. All construction equipment, diesel trucks, and generators shall be equipped with Best Available Control Technology for emission reductions of nitrogen oxides and particulate matter. The project construction contractor shall use equipment that meets the ARB's most recent certification standard for off-road heavy duty diesel engines. 	<p>Building Division</p>	<p>Review and approve the air pollution control plan. Verify that air quality control measures in Mitigation Measure AIR-2 are included.</p>	<p>Prior to issuance of building or grading permit</p>	<p>No issuance of building or grading permit</p>

Table 1 Continued

Mitigation Measures	Monitoring Responsibility	Monitoring and Reporting Action	Monitoring Schedule	Non-Compliance Sanction
<p>D. Noise</p> <p>NOISE-1: The following measures shall be implemented during construction of the project:</p> <p>(a) To minimize construction noise impacts on nearby residents and businesses, and to be consistent with Chapter 8.06 of the City's Municipal Code, standard construction activities that exceed stated noise limits shall be permitted only between the hours of 8:00 a.m. and 6:00 p.m. from Monday to Friday.</p> <p>(b) To reduce daytime construction-related noise impacts to the maximum feasible extent, the project sponsor shall develop a site-specific noise reduction program subject to City review and approval, which includes the following measures:</p> <ul style="list-style-type: none"> • Signs shall be posted at the construction site that include permitted construction days and hours, a day and evening contact number for the job site, and a day and evening contact number for the City. The signs shall be posted at all entrances to the construction site upon the commencement of construction for the purpose of informing contractors and subcontractors and all other persons at the construction site of the basic requirements of the Noise Ordinance of the Municipal Code. The sign shall be at least 5 feet above ground level and shall consist of a white background with black letters. • A pre-construction meeting shall be held with the job inspectors and the general contractor/on-site project manager to confirm that noise mitigation protocols are in place prior to the issuance of a building permit (including the establishment of construction hours, neighborhood notification, posted signs, etc.). • Equipment and trucks used for project construction shall utilize the best available noise control techniques (e.g., improved mufflers, equipment redesign, use of intake silencers, ducts, engine enclosures, and acoustically attenuating shields or shrouds). • Impact tools (e.g., jack hammers, pavement breakers, and rock drills) used for project demolition or construction activities shall be hydraulically or electrically powered wherever possible to avoid noise associated with compressed-air exhaust from pneumatically powered tools. Where use of pneumatic tools is unavoidable, an exhaust muffler on equipment with compressed-air exhaust systems shall be used; this muffler can lower noise levels, which could achieve a reduction of 5 dBA. Quieter procedures shall be used, such as drills rather than impact equipment, whenever feasible. 	<p>Building Division</p>	<p>1) Verify that construction activities occur only during permitted hours</p> <p>2) Ensure that noise control measures and features are in place, including the temporary sound barrier</p>	<p>1) Ongoing throughout the construction period</p> <p>2) Prior to issuance of a building or grading permit</p>	<p>1) Impose stop work order</p> <p>2) No issuance of building or grading permit</p>

Table 1 Continued

Mitigation Measures	Monitoring Responsibility	Monitoring and Reporting Action	Monitoring Schedule	Non-Compliance Sanction
<p><u>NOISE-1 Continued</u></p> <ul style="list-style-type: none"> Stationary noise sources shall be located as far from sensitive receptors as possible and they shall be muffled and enclosed within temporary sheds; or insulation barriers or other measures shall be incorporated to the extent feasible. No piece of powered equipment shall generate noise in excess of 85 dBA at 50 feet. Powered equipment is defined by the City to be a motorized device powered by electricity or fuel used for construction, demolition, and property or landscape maintenance or repairs. Powered equipment includes but is not limited to: parking lot sweepers, saws, sanders, motors, pumps, generators, blowers, wood chippers, vacuums, drills and nail guns (but specifically excluding internal fuel combustion engine leaf blowers). Prior to construction, a temporary sound barrier shall be constructed along the project's western property line adjacent to the existing residential properties that border the project site. The temporary sound barrier shall extend from the project property line at College Avenue to the project property line at Partridge Avenue. This temporary sound barrier shall be constructed at the minimum height of 6 feet above the proposed finished pad elevation with a minimum surface weight of 4 pounds per square foot (or with any commercially available sound barrier material that has an equivalent noise reduction coefficient as a material with a minimum surface weight of 4 pounds per square foot) and shall be constructed so that vertical or horizontal gaps are eliminated. This temporary barrier shall remain in place through the construction phase in which heavy construction equipment, such as excavators, bulldozers, scrapers, loaders, rollers, pavers, and dump trucks are operating within 100 feet of the western project site boundary. 				
<p><u>NOISE-2a:</u> In order to ensure that windows can remain closed for prolonged periods of time to meet the interior noise standard of 45 dBA CNEL established by the City, an alternative form of ventilation, such as air conditioning or noise-attenuated passive ventilation systems, shall be included in all proposed dwelling units.</p>	Building Division and Planning Division	Verify that project plans show alternative form of ventilation in each building	Prior to issuance of building permit	No issuance of building permit

Table 1 Continued

Mitigation Measures	Monitoring Responsibility	Monitoring and Reporting Action	Monitoring Schedule	Non-Compliance Sanction
<p>NOISE-2b: In order to meet the interior noise standard of 45 dBA CNEL established by the City, all proposed dwelling units that would be located within 45 feet of the centerline of the outermost travel lane of El Camino Real shall be constructed to have an overall minimum STC rating of STC-35, and all exterior doors and windows shall have a minimum rating of STC-33. Quality control shall be exercised in construction to ensure all air-gaps and penetrations of the building shell are controlled and sealed.</p>	<p>Building Division and Planning Division</p>	<p>Verify that project plans show appropriate STC rating for affected units.</p>	<p>Prior to issuance of building permit</p>	<p>No issuance of building permit</p>
<p>NOISE-3: The project sponsor shall ensure that project plans submitted for a building permit include documentation that proposed stationary equipment shall not generate noise that exceeds 60 dBA L_{eq} during daytime hours and 50 dBA L_{eq} during nighttime hours, as measured at any point on a neighboring residential property nearest where the noise source at issue generates the highest noise level.</p>	<p>Building Division and Planning Division</p>	<p>Verify that project plans contain proper documentation for stationary equipment</p>	<p>Prior to issuance of building permit</p>	<p>No issuance of building permit</p>
<p>E. Public Services and Utilities</p>				
<p><i>There are no significant Public Services and Utilities impacts.</i></p>				
<p>F. Aesthetics</p>				
<p>AES-1: The project applicant shall prepare a lighting plan and photometric study and submit them to the City for review and approval prior to issuance of a building permit. City staff shall review the plan to ensure that any outdoor lighting for the project is oriented downwards and is designed to minimize lighting or glare off-site.</p>	<p>Building Division</p>	<p>Review and approve lighting plan and photometric study.</p>	<p>Prior to issuance of building permit</p>	<p>No issuance of building permit</p>

Source: LSA Associates, 2012.

ATTACHMENT C

***DRAFT* – Conditions of Approval**

Use Permit, Architectural Control, Tentative Subdivision Map

321-389 El Camino Real, 603-607 College Avenue, and 612 Partridge Collectively Known as 389 El Camino Real

Conditions

1. Development of the project shall be substantially in conformance with the plans by Dahlin Group, BKF and Gates Associates, dated received by the Planning Division on June 18, 2012, consisting of 80 plan sheets, except as modified by the conditions contained herein.
2. The Use Permit, Architectural Control, and Tentative Subdivision Map shall expire two years from the date of approval if the applicant does not submit a complete building permit application within that time. The Community Development Director may extend this date per Municipal Code Section 16.82.170. Within two years from the date of approval of the tentative map, the applicant shall submit a Final Map for review and approval of the City Engineer.
3. Minor modifications to building exteriors and locations, fence styles and locations, signage, and significant landscape features may be approved by the Community Development Director or designee, based on the determination that the proposed modification is consistent with other building and design elements of the approved Use Permit and will not have an adverse impact on the character and aesthetics of the site. The Director may refer any request for revisions to the plans to the Planning Commission for architectural control approval. A public hearing could be called regarding such changes if deemed necessary by the Planning Commission.
4. Major modifications to building exteriors and locations, fence styles and locations, signage, and significant landscape features may be allowed subject to obtaining an architectural control permit from the Planning Commission, based on the determination that the proposed modification is compatible with the other building and design elements of the approved Use Permit and will not have an adverse impact on the character and aesthetics of the site. A public hearing could be called regarding such changes if deemed necessary by the Planning Commission.
5. Major revisions to the development plan which involve material changes, or expansion or intensification of development require public hearings by the Planning Commission and City Council.

6. The project shall comply with all aspects of the California Building Code in effect at the time of Building permit application.
7. Concurrent with the submittal for a demolition permit, the applicant shall submit a tree preservation plan to address the protection of the heritage redwood and existing street trees to remain, detailing the location of and methods for all tree protection measures, as described in the arborist report. The project arborist shall submit a letter confirming adequate installation of the tree protection measures. The applicant shall retain an arborist throughout the term of the project, and the project arborist shall submit periodic inspection reports to the Building Division. The heritage tree preservation plan shall be subject to review and approval by the Planning Division prior to issuance of a demolition permit.
8. Concurrent with the submittal for a demolition permit, the applicant shall submit a plan for: 1) construction safety fences around the periphery of the construction area, 2) dust control, 3) air pollution control, 4) erosion and sedimentation control, 5) tree protection fencing, and 6) construction vehicle parking. The plans shall be subject to review and approval by the Building, Engineering, and Planning Divisions prior to issuance of a demolition permit. The fences and erosion and sedimentation control measures shall be installed according to the approved plan prior to commencing demolition.
9. Consistent with BAAQMD's Best Management Practices for Construction, the following actions shall be included in the dust control plan subject to review and approval by the Building Division prior to issuance of a demolition permit:
 - All exposed surfaces (e.g., parking areas, staging areas, soil piles, graded areas, and unpaved access roads) shall be watered two times per day.
 - All haul trucks transporting soil, sand, or other loose material off-site shall be covered.
 - All visible mud or dirt tracked-out onto adjacent public roads shall be removed using wet power vacuum street sweepers at least once per day. The use of dry power sweeping is prohibited.
 - All vehicle speeds on unpaved roads shall be limited to 15 mph.
 - All roadways, driveways, and sidewalks to be paved shall be completed as soon as possible.
 - Building pads shall be laid as soon as possible after grading unless seeding or soil binders are used.
 - Idling times shall be minimized either by shutting equipment off when not in use or reducing the maximum idling time to 5 minutes (as required by the California airborne toxics control measure, Title 13, Section 2485 of the California Code of Regulations [CCR]). Clear signage shall be provided for construction workers at all access points.
 - All construction equipment shall be maintained and properly tuned in accordance with the manufacturer's specifications. All equipment shall be checked by a certified mechanic and determined to be running in proper condition prior to operation.

- A publicly visible sign shall be posted with the telephone number and person to contact at the City regarding dust complaints. This person shall respond and take corrective action within 48 hours. The BAAQMD's phone number shall also be visible to ensure compliance with applicable regulations.
(Mitigation Measure AIR-1)

10. Consistent with guidance from the BAAQMD, the following actions shall be included in the air pollution control plan subject to review and approval by the Building Division prior to issuance of a demolition permit:

- The construction contractor shall ensure the idling time of diesel-powered construction equipment is 2 minutes or less.
- The construction contractor shall utilize off-road equipment (more than 50 horsepower) used in the construction of the project (i.e., owned, leased, and subcontractor vehicles) that achieves a project wide fleet average 20 percent nitrogen oxide reduction and 45 percent particulate matter reduction compared to the most recent ARB fleet average. Acceptable options for reducing emissions include the use of late model engines, low-emission diesel products, alternative fuels, engine retrofit technology, after-treatment products, add-on devices such as particulate filters, and/or other options that are available.
- All construction equipment, diesel trucks, and generators shall be equipped with Best Available Control Technology for emission reductions of nitrogen oxides and particulate matter.
- The project construction contractor shall use equipment that meets the ARB's most recent certification standard for off-road heavy duty diesel engines.

(Mitigation Measure AIR-2)

11. Prior to demolition permit issuance, all buildings that are proposed for demolition shall be surveyed for asbestos-containing materials under the National Emissions Standards for Hazardous Air Pollutants (NESHAP) guidelines. All potentially friable asbestos-containing materials shall be removed prior to building demolition in accordance with NESHAP guidelines and BAAQMD Regulation 11, Rule 2: Hazardous Materials; Asbestos Demolition, Renovation and Manufacturing. The BAAQMD's Enforcement Division shall be consulted prior to commencing demolition of a building containing asbestos materials.

12. Prior to demolition permit issuance, a survey of painted surfaces on all buildings at the site shall be conducted. Based on the results of the survey, if lead-based paint is still bonded to the building surfaces, its removal is not required prior to demolition. If lead-based paint is peeling, flaking, or blistered, it shall be removed prior to demolition in accordance with state requirements. It is assumed that such paint will become separated from the building components during demolition activities; thus, it must be managed and disposed as a separate waste stream. Any debris or soil containing lead paint or coating must be disposed at landfills that have acceptance criteria for the waste being disposed. The project shall follow the requirements outlined by California Occupational Safety and Health Administration (Cal-OSHA)

Lead in Construction Standard, Title 8, California Code of Regulations (CCR) 1532.1 during demolition activities. These regulations include employee training, employee air monitoring, and dust control.

13. Prior to demolition permit and/or building permit issuance, the applicant shall comply with the requirements of Chapter 12.48 (Salvaging and Recycling of Construction and Demolition Debris) of the City of Menlo Park Municipal Code.
14. Prior to recordation of the Final Map, the existing structures shall be demolished after obtaining a demolition permit.
15. Concurrent with the application for a Final Map, the applicant shall submit covenants, conditions and restrictions (CC&Rs) for the approval of the City Engineer and the City Attorney. The Final Map and the CC&Rs shall be recorded concurrently and shall include administration of the Transportation Demand Management (TDM) Program. The TDM Program shall be consistent with the City of Menlo Park's Transportation Impact Analysis Guidelines. Potential TDM measures include the following:
 - A commute assistance kiosk. A kiosk or bulletin board that provides information on alternative modes of transportation available in the area;
 - Subsidized public transit passes. As part of homeowners or membership fees, a subsidized pass for public transit may be provided to residents;
 - Carpool matching assistance. A person or database to link residents traveling to similar locations, to allow for carpooling;
 - Vanpools. Vanpools are generally privately-sponsored and provide pick-up and drop-off services for commuters who work and live in the same general area;
 - Shuttle service to area transit hubs. Privately-sponsored vehicles transport residents between dwelling units and area transit hubs such as SamTrans bus stops, BART Stations, and the Menlo Park Caltrain Station; and,
 - Bicycle facilities. For residential projects, these facilities would generally include bicycle racks and bicycle lockers (although residents would be expected to store bicycles in their residences).
 - The TDM Program, which could be shared with that of other residential developments or businesses in the area, shall be reviewed and approved by the Planning and Transportation Divisions.

(Mitigation Measure TRANS-1b)

16. Concurrent with the application submittal for the Final Map, the applicant shall submit a Grading and Drainage Plan, including an Erosion and Sedimentation Control Plan, for review and approval of the City Engineer. The Grading and Drainage Plan shall be prepared based on the City's Grading and Drainage Plan Guidelines and Checklist and the Project Applicant Checklist for the National Pollution Discharge Elimination System (NPDES) Permit Requirements. The Grading and Drainage Plan shall be approved prior to issuance of a grading permit.

17. The application submittal for the Final Map shall include the following abandonments and dedications:
 - Abandonment of Alto Lane;
 - Abandonment of the existing storm drain easement; dedicate to the City the new utility easements, storm drain easements; and,
 - Dedication of the Pedestrian Access Easement (PAE) along El Camino Real.
18. As part of a complete Final Map application, the applicant shall submit a complete application for a pedestrian access easement for the portion of the proposed sidewalk along El Camino Real located on private property, subject to the review of the Planning and Engineering Divisions. Concurrent with Final Map approval, the easement shall be approved by the City Council and documentation showing proof of recordation with the San Mateo County Recorder's Office shall be provided.
19. Prior to recordation of the Final Map, the applicant shall install new improvements as shown on the project plans per City and Caltrans standards along the entire property frontage subject to the review and approval of the Engineering Division. The applicant shall obtain an encroachment permit, from the appropriate reviewing jurisdiction, prior to commencing any work within the right-of-way or public easements. If determined appropriate and subject to the approval of the Engineering Division, the applicant may provide a bond for the completion of the work subsequent to the recordation of the Final Map.
20. Concurrent with the application for an encroachment permit for frontage improvements, the applicant shall submit a Sidewalk Protection Plan detailing an alternate pedestrian path along El Camino Real while the frontage improvements are under construction, subject to the review and approval of the Engineering Division.
21. Prior to recordation of the Final Map, the applicant shall install new utilities to the point of service subject to review and approval of the City Engineer. All electric and communication lines servicing the project shall be placed underground. Each lot/unit shall have separate utility service connections. If determined appropriate and subject to the approval of the Engineering Division, the applicant may provide a bond for the completion of the work subsequent to the recordation of the Final Map.
22. Prior to the recordation of the Final Map, the applicant shall enter into a Subdivision Improvement Agreement and provide a bond for the completion of site improvements, subject to the approval of the Engineering Division.
23. Prior to recordation of the Final Map, the applicant shall pay any applicable recreation fees (in lieu of dedication) per the direction of the City Engineer in compliance with Section 15.16.020 of the Subdivision Ordinance. The estimated recreation in-lieu fee is \$704,000 (based on \$4 million value of acreage).
24. Concurrent with the application for a grading permit, the applicant shall submit a soil management plan (SMP) for the northwestern half of the project site between El

Camino Real and Alto Lane. The SMP shall indicate how soils excavated from this area will be screened for potential hydrocarbon contamination and managed (segregation, storage, sampling, and disposal). The SMP shall also describe the mitigation, notification, and sampling measures that will be implemented if hydrocarbon vapors or visual signs of contamination are encountered during soil grading and excavation. The SMP shall be reviewed and approved by the San Mateo County Health Department, and written confirmation obtained from the San Mateo County Health Department demonstrating approval of the SMP shall be submitted to the Planning and Building Divisions, prior to the issuance of a grading permit.

25. Concurrent with the application for a grading permit, the applicant shall submit a draft "Stormwater Treatment Measures Operations and Maintenance (O&M) Agreement" with the City subject to review and approval by the Engineering Division. With the executed agreement, the property owner is responsible for the operation and maintenance of stormwater treatment measures for the project. The agreement shall run with the land and shall be recorded by the applicant with the San Mateo County Recorder's Office. The applicant shall enter into and record a Stormwater Treatment Measures Operations and Maintenance Agreement prior to finalizing the building permit for the first residential unit.
26. Concurrent with the application for a grading permit, the applicant shall submit an updated Hydrology Report for review and approval by the Public Works Department. The Hydrology Report shall confirm that the project does not result in increased storm water runoff as measured by the peak flow rate for a 10-year storm and shall also confirm that the on-site depressed garages will not be subject to flooding during a 10-year storm. If the Hydrology Report shows an increase of runoff (over the existing conditions runoff), then the applicant shall implement modifications to the project to ensure that neither impact occurs subject to review and approval of the Planning and Engineering Divisions.
27. Concurrent with the application for a grading permit, the applicant shall submit a plan for any new utility installations or upgrades for review and approval of the Planning, Engineering and Building Divisions. Landscaping shall properly screen all utility equipment that is installed outside of a building and that cannot be placed underground. The plan shall show exact locations of all meters, back flow prevention devices, transformers, junction boxes, relay boxes, and other equipment boxes.
28. Prior to grading permit issuance, the applicant shall provide documentation of the recordation of the Final Map at the County Recorder's Office for review and approval of the Engineering Division and the Planning Division. Application for a grading permit may be made prior to recordation.
29. A complete building permit application will be required for any remediation work that requires a building permit. No remediation work that requires approval of a building permit shall be initiated until the applicant has received building permit approvals for

that work. All building permit applications are subject to the review and approval of the Building Division.

30. Prior to building permit issuance, the applicant shall submit a detailed landscape plan, including the size, species, and location, and irrigation plan for review and approval by the Planning Division and the Public Works Department. The plan shall allow for sight distance visibility and comply with the Water Efficient Landscaping Ordinance (Municipal Code Chapter 12.44). The landscaping shall be installed prior to final building inspection.
31. Concurrent with the submittal for a building permit, the applicant shall submit revised landscape elevations for the proposed metal trellis along the shared property line with 301 El Camino Real for review and approval by the Planning Division. To ensure adequate traffic safety and visibility, the trellis shall not exceed two feet in height within the first ten feet of the street curb. After the first ten feet, the trellis shall step up in height to seven feet up to the existing adjacent building located at 301 El Camino Real. The trellis may reach a maximum of nine feet in height along the existing building wall.
32. Concurrent with the submittal for a building permit, the applicant shall submit a lighting plan, providing the location, architectural details and specifications for all exterior lighting subject to review and approval by the Planning Division. The lighting plan shall minimize glare and confirm that there is no spillover onto adjacent properties and the public right-of-way. (Mitigation Measure AES-1)
33. Concurrent with the submittal for a building permit, a design-level geotechnical investigation report shall be submitted to the Building Division for review and confirmation that the proposed development fully complies with the California Building Code. The report shall determine the project site's surface geotechnical conditions and address potential seismic hazards. The report shall identify building techniques appropriate to minimize seismic damage.
34. Prior to building permit issuance, the applicant shall comply with all requirements of the Building Division, Engineering Division, and Transportation Division that are directly applicable to the project.
35. Prior to building permit issuance, the applicant shall comply with all Sanitary District, Menlo Park Fire Protection District, Recology, and utility companies' regulations that are directly applicable to the project.
36. Plans and specifications for upgrading any sewer facilities shall be submitted to the West Bay Sanitary District (WBSD) for approval and issuance of the appropriate permits prior to the approval of the Final Map. The project shall upgrade the sewer facilities to which it connects as designated by the WBSD; specific improvements would be determined at the final design level.

37. Prior to building permit issuance, the applicant shall submit documentation subject to the approval of the Planning and Building Divisions demonstrating that the proposed residential units shall be designed with an adequate alternative form of ventilation, such as air conditioning or noise-attenuated passive ventilation systems, to meet the interior noise standard of 45 dBA CNEL. (Mitigation Measure NOISE-2a)
38. Prior to building permit issuance, the applicant shall ensure that project plans submitted for a building permit show that all proposed dwelling units that would be located within 45 feet of the centerline of the outermost travel lane of El Camino Real shall be constructed to have an overall minimum STC rating of STC-35, and all exterior doors and windows shall have a minimum rating of STC-33. Quality control shall be exercised in construction to ensure all air-gaps and penetrations of the building shell are controlled and sealed. (Mitigation Measure NOISE-2b)
39. Prior to building permit issuance, the applicant shall ensure that project plans submitted for a building permit include documentation that proposed stationary equipment shall not generate noise that exceeds 60 dBA L_{eq} during daytime hours, and 50 dBA L_{eq} during nighttime hours, as measured at any point on a neighboring residential property nearest where the noise source at issue generates the highest noise level. (Mitigation Measure NOISE-3)
40. Prior to issuance of each applicable building permit, the applicant shall pay the following fees associated with the project:
 - The applicant shall pay all applicable school impact fees associated with the project.
 - The applicant shall pay the applicable Building Construction Street Impact Fee.
41. Prior to building permit issuance, the applicant shall pay the transportation impact fee per the direction of the Transportation Division in compliance with Chapter 13.26 of the Municipal Code. The current estimated transportation impact fee is \$46,074, although the final fee shall be the fee in effect at the time of payment.
42. The following measures shall be implemented during construction of the project:
 - (a) To minimize construction noise impacts on nearby residents and businesses, and to be consistent with Chapter 8.06 of the City's Municipal Code, standard construction activities that exceed stated noise limits shall be permitted only between the hours of 8:00 a.m. and 6:00 p.m. from Monday to Friday.
 - (b) To reduce daytime construction-related noise impacts to the maximum feasible extent, the applicant shall develop a site-specific noise reduction program subject to Building Division review and approval, which includes the following measures:
 - Signs shall be posted at the construction site that include permitted construction days and hours, a day and evening contact number for the disturbance coordinator at the job site, and a contact number for the City. The signs shall be posted at all entrances to the construction site upon the

commencement of construction for the purpose of informing contractors and subcontractors and all other persons at the construction site of the basic requirements of the Noise Ordinance of the Municipal Code. The sign shall be at least 5 feet above ground level and shall consist of a white background with black letters.

- A pre-construction meeting shall be held with the City building inspectors and the general contractor/on-site project manager to confirm that noise mitigation protocols are in place prior to the issuance of a building permit (including the establishment of construction hours, neighborhood notification, posted signs, etc.).
- Equipment and trucks used for project construction shall utilize the best available noise control techniques (e.g., improved mufflers, equipment redesign, use of intake silencers, ducts, engine enclosures, and acoustically attenuating shields or shrouds).
- Impact tools (e.g., jack hammers, pavement breakers, and rock drills) used for project demolition or construction activities shall be hydraulically or electrically powered wherever possible to avoid noise associated with compressed-air exhaust from pneumatically powered tools. Where use of pneumatic tools is unavoidable, an exhaust muffler on equipment with compressed-air exhaust systems shall be used; this muffler can lower noise levels, which could achieve a reduction of 5 dBA. Quieter procedures shall be used, such as drills rather than impact equipment, whenever feasible.
- Stationary noise sources shall be located as far from sensitive receptors as possible and they shall be muffled and enclosed within temporary sheds; or insulation barriers or other measures shall be incorporated to the extent feasible.
- No piece of powered equipment shall generate noise in excess of 85 dBA at 50 feet. Powered equipment is defined by the City to be a motorized device powered by electricity or fuel used for construction, demolition, and property or landscape maintenance or repairs. Powered equipment includes but is not limited to: parking lot sweepers, saws, sanders, motors, pumps, generators, blowers, wood chippers, vacuums, drills and nail guns (but specifically excluding internal fuel combustion engine leaf blowers).
- Prior to construction, a temporary sound barrier shall be constructed along the project's western property line adjacent to the existing residential properties that border the project site. The temporary sound barrier shall extend from the project property line at College Avenue to the project property line at Partridge Avenue. This temporary sound barrier shall be constructed at the minimum height of 6 feet above the proposed finished pad elevation with a minimum surface weight of 4 pounds per square foot (or with any commercially available sound barrier material that has an equivalent noise reduction coefficient as a material with a minimum surface weight of 4 pounds per square foot) and shall be constructed so that vertical or horizontal gaps are eliminated. This temporary barrier shall remain in place through the construction phase in which heavy construction equipment, such as

excavators, bulldozers, scrapers, loaders, rollers, pavers, and dump trucks are operating within 100 feet of the western project site boundary.
(Mitigation Measure NOISE-1)

THIS DOCUMENT IS RECORDED FOR THE BENEFIT
OF THE CITY OF MENLO PARK AND
IS EXEMPT FROM FEE PER
GOVERNMENT CODE SECTIONS 27383 AND 6103.

WHEN RECORDED MAIL TO:)
)
COMMUNITY DEVELOPMENT DEPARTMENT)
CITY OF MENLO PARK)
701 Laurel Street)
Menlo Park, CA 94025-3483)
)
)

BELOW MARKET RATE FOR-SALE AGREEMENT
389 EL CAMINO REAL

BELOW MARKET RATE FOR-SALE AGREEMENT

This Below Market Rate For-Sale Agreement ("Agreement") is made as of this ____ day of _____ 2012 by and between THE CITY OF MENLO PARK, a California municipality ("City") and 389 EL CAMINO ASSOCIATES, LLC, a California limited liability company ("Owner"), with respect to the following:

RECITALS

A. Owner is the owner of certain real property located in the City of Menlo Park, County of San Mateo, State of California ("Property"), more particularly described in Exhibit A attached hereto. The Property is commonly known as 389 El Camino Real and consists of Assessor's Parcel Numbers 071-412-430-2, 071-412-220-7, 071-412-230-6, 071-412-170-4 and 071-412-250-4.

B. Pursuant to City Municipal Code Chapter 16.96, the City's BMR Housing Ordinance ("BMR Ordinance"), and the City's Below Market Rate Housing Program Guidelines ("Guidelines") attached hereto as Exhibit B, Owner is required to enter into this Agreement for the benefit of the City to insure compliance with the City's BMR Ordinance and the Guidelines, which is a prerequisite to obtaining final development approvals and "Final Inspection" of the units from the Building Division.

C. Owner plans to redevelop the Property by constructing a total of twenty-six (26) new attached and detached for-sale single-family residential units of which three (3) shall be below market rate units ("BMR Units"), as required by, and in full compliance with the City's BMR Ordinance and the Guidelines.

D. The BMR Units shall be sold to third parties who meet the eligibility requirements set forth in the BMR Ordinance and the Guidelines, and with prices determined in accordance with this Agreement.

E. This Agreement is for the benefit of Owner and the City. The deeds to the BMR Units shall contain restrictions that limit the sales price of the BMR Units in accordance with the BMR Ordinance and the Guidelines. These deed restrictions relating to the three (3) BMR Units shall be binding on the future owners of those units.

NOW, THEREFORE, the parties agree as follows:

1. The three (3) BMR Units are to be completed and sold in accordance with the BMR Ordinance and the Guidelines with the appropriate deed restrictions. For purposes of Section 8 of the Guidelines, a BMR Unit shall be deemed "available for purchase" when the City has issued a letter that states that the BMR Unit meets the requirements of the Guidelines and satisfies the provisions of this Agreement. The letter will be issued when the

BMR Unit is substantially ready for occupancy, as reasonably determined by the City's Community Development Director, and when the BMR Unit has passed Final Inspection by the Building Division.

2. Section 5.1 of the Guidelines requires the BMR Units to generally be of the same size as the market rate units and be distributed throughout the development. The locations of the three (3) BMR Units are shown as BMR Unit #s 2, 8 and 13 on Exhibit C attached hereto. The floor plans showing the size and layout of the BMR Units are shown on Exhibit D attached hereto.

3. The streetscape elevations of the BMR Units will be as approved by the Planning Commission.

4. The exterior materials used in the construction of the BMR Units will be similar and indistinguishable from those used on the market rate units. The interior finishes of the BMR Units shall be similar to those of the market rate units, except for upgrades purchased by individual buyers.

5. Each BMR Unit shall be affordable to households which are U.S. Department of Housing and Urban Development ("HUD") low or State lower income eligible as defined in Section 50079.5 of the California Health and Safety Code, as described in the Guidelines, and are of the smallest household size eligible for the BMR Unit on the BMR waiting list maintained by the City on the date that the Sales Price is set, as more particularly described below. The BMR Sales Price shall be calculated according to the following formula by reference to the definitions and standards set forth in Sections 6.1 and 6.2, below.

6.1 The "**Sales Price**" shall be calculated by adding the cash down payment, defined in 6.2.10., below, to the Maximum Mortgage Amount, defined in Section 6.1.6, below, less lender and escrow fees and costs incurred by the buyer. The Sales Price shall be set before the commencement of the sale process for the BMR Units.

6.1.1 Calculate the "**Smallest Household Size**": The household with the smallest number of persons eligible for the BMR Unit, as shown in Section 14, Table C (Occupancy Standards) of the Guidelines.

6.1.2. The current "**Maximum Eligible Income**" shall be the most current State Income Limit for San Mateo County, Lower Income category, as published by the State of California Department of Housing and Community Development, for the Smallest Household Size.

6.1.3. Calculate the "**Maximum Allowable Monthly Housing Expenses**": Multiply the Maximum Eligible Income by thirty three percent (33%) and divide by twelve (12).

6.1.4. Calculate the "**Actual Monthly Housing Expenses**": Add the following costs associated with a particular BMR Unit, as more particularly described in Paragraph 6.2 below, and divide by twelve (12): (a) any loan fees, escrow fees and other closing costs (amortized over 360 months) and/or private mortgage insurance associated therewith; (b) property taxes and assessments; (c) fire, casualty insurance and flood insurance, if required; (d) property maintenance and repairs, deemed to be One Hundred Dollars (\$100) per month; (e) a reasonable allowance for utilities as set forth in the Guidelines, not including telephones, and (f) homeowners association fees, if applicable, but less the amount of such homeowners association fees allocated for any costs attributable to (c), (d) or (e) above.

6.1.5. Calculate the "**Maximum Monthly Mortgage Payment Amount**": Subtract the Actual Monthly Housing Expenses from the Maximum Allowable Monthly Housing Expenses.

6.1.6. Determine the "**Maximum Mortgage Amount**": Determine the amount of mortgage that a lender would loan, based upon the Maximum Monthly Mortgage Payment Amount and based upon the down payment found to be the lowest that lenders are willing to accept in a survey of lenders as described below. Survey and take the average of at least three local lenders who regularly make home loans at a typical housing expense ratio to first-time buyers in the price range of the BMR home on the day that the price is set. The mortgage amount shall be for a 30-year fixed rate mortgage with standard fees, closing costs and no points, and shall be less than or equal to the Maximum Monthly Mortgage Amount.

6.2. The calculation of the Sales Price shall be based upon the factors defined below. These definitions conform to the eligibility and underwriting standards established by the major secondary mortgage market investors, such as the Federal National Mortgage Association ("Fannie Mae") and the Federal Home Loan Mortgage Corporation ("Freddie Mac").

6.2.1. Mortgage Interest Rate. The mean average of contract interest rates on the date that the Sales Price is set, for fixed rate, 30-year "Conforming" mortgages (presently \$417,000 or less, as such amount may be adjusted from time to time as the maximum amount of FHA Conforming mortgages), or for jumbo mortgages if applicable, as quoted by three local retail lenders. The three local retail lenders shall be selected at random by the City from the list of lenders certified by San Mateo County to make first mortgage loans with Mortgage Credit Certificates.

6.2.2. Points. The mean average of points quoted by three local lenders that make mortgage loans to first time home buyers in the City of Menlo Park on the date that the Sales Price is set for fixed rate, 30 year mortgages of \$417,000 or less, or for jumbo mortgages if applicable, which lenders are selected on a random basis by the City. Points are a one-time fee paid to a lender for making a loan. One point is

equal to one percent of the loan amount.

6.2.3. Lender/Escrow Fees. The mean average of fees charged by three local lenders that make mortgage loans to homebuyers, which lenders are selected on a random basis by the City, plus escrow company fees, for such items as title insurance, appraisal, escrow fees, document preparation and recording fees.

6.2.4. Loan to Value Ratio. The maximum ratio of the dollar amount of a Conforming mortgage to the sales price of a home which a lender is willing to approve at a given point in time. For purposes of this Agreement, the Loan to Value Ratio shall be calculated as the mean average of the maximum Loan to Value Ratios as quoted by three local lenders selected on a random basis by the City from a list of lenders who actively make loans to homebuyers and who participate in the Mortgage Credit Certificate program.

6.2.5. Housing Expense Ratio. The mean average of the housing expense ratio as reported on the date that the sales price is set, for fixed rate, 30-year mortgages of \$417,000 or less, or for jumbo mortgages if applicable, by three local lenders that make mortgage loans to homebuyers in the City of Menlo Park, which lenders are selected on a random basis by the City. Housing expense is defined as the sum of the annual mortgage payment (including principal and interest), and annual payments for taxes, homeowners association dues, insurance, property maintenance and repairs, a reasonable allowance for utilities according to the San Mateo County Housing Authority Utility Financial Allowance Chart which is periodically updated and amended, and any secondary financing (but excluding any portion of the aforementioned expenses covered by homeowners association dues). To determine the ratio, this sum is divided by gross annual income.

6.2.6. Homeowners Insurance. Calculated as the mean average of the annual cost of insurance quoted by two or three local brokers, based on their experience, for a housing unit of the price, room configuration, location, construction material and structure type of the subject BMR Unit. Flood insurance costs, if required shall be calculated by this same method.

6.2.7. Private Mortgage Insurance. The mean average of the annual cost of private mortgage insurance quoted by two or three local lenders, based on their experience, for a housing unit of the price, location, and structure type of the subject BMR Unit.

6.2.8. Taxes. The tax rate as reported by the San Mateo County Assessor's Office.

6.2.9. Homeowners' Dues. Reported by the developer and as set forth in the Public Report issued by the California Department of Real Estate for the

project.

6.2.10. Down Payment. Cash portion paid by a buyer from his own funds, as opposed to that portion of the purchase price which is financed. For the purpose of calculating the BMR Sales Price, the down payment will be defined as the mean average of the smallest down payment required by the two or three local lenders surveyed.

6.3. The Sales Price shall be agreed upon in writing by Owner and the City's Community Development Director no later than the date of the Final Inspection, or at an earlier date agreed to by the City's Community Development Director, and before the process begins to find a buyer.

7. As a condition precedent to a Final Inspection of any market rate unit at least one (1) BMR Unit shall have passed Final Inspection, and no more than nine (9) market rate units shall have passed Final Inspection until a second BMR Unit passes Final Inspection. In any event, the last BMR Unit must pass Final Inspection before the last market rate unit passes Final Inspection.

8. If there is a standard pre-sale requirement by the BMR applicant's lender for a certain percentage of units in the project to be sold before the BMR applicant's lender will close escrow on the loan, then the time for the City's purchase or the buyer's purchase will be extended until that requisite number of units has closed.

9. This Agreement shall be binding on and inure to the benefit of the parties hereto and any respective assigns and or owners of the property. Either party may freely assign this Agreement without the consent of the other. However, to be valid, an assignment of this Agreement must be in writing.

10. This Agreement is a covenant running with the land for the benefit of the City and all lands owned by the City within the limits of the City.

11. If any legal action is commenced to interpret or enforce this Agreement or to collect damages as a result of any breach of this Agreement, the party prevailing shall be entitled to recover all reasonable attorney's fees and costs incurred in such action from the other party.

12. Owner shall record this Agreement in the Office of the County Recorder of San Mateo prior to the recording of a final subdivision map for any portion of the Property and shall provide a copy of such recorded agreement to the City.

13. This Agreement shall be governed by and construed in accordance with the laws of the State of California.

14. The terms of this Agreement may not be modified or amended except by an instrument in writing executed by each of the parties hereto.

15. The exhibits attached hereto are hereby incorporated herein by this reference for all purposes.

16. This Agreement supersedes any prior agreements, negotiations and communications, oral or written, and contains the entire agreement between the parties as to the subject matter hereof.

17. If any portion of this Agreement as applied to either party or to any circumstances shall be adjudged by a court to be void or unenforceable, such portion shall be deemed severed from this Agreement and shall in no way effect the validity or enforceability of the remaining portions of this Agreement.

18. Any and all obligations or responsibilities of Owner under this Agreement shall terminate upon the recording of the grant deeds conveying the BMR Units to qualified third party purchasers in accordance with the terms and provisions of this Agreement, the recording of the deed restrictions against such BMR Units, and/or the payment of the in lieu fees, if applicable, to be paid through escrow, as set forth in Section 4.3 of the Guidelines.

19. The execution and delivery of this Agreement shall not be deemed to be for the benefit of the third party purchasers of the BMR Units or any other third party and any and all obligations and responsibilities of Owner under this Agreement are to the City for whose benefit this Agreement has been entered into. No third party purchaser of a BMR or market rate unit, homeowners' association or any other third party shall obtain any rights or standing to complain that the BMR Units were not constructed, designed, sold or conveyed in accordance with this Agreement, or the BMR Ordinance and the Guidelines as a result of this Agreement. Furthermore, the acceptance of this Agreement by the City, the acceptance of the interior specifications for the BMR Units and the conveyance of the BMR Units to qualified third parties shall conclusively indicate that Owner has complied with this Agreement and the BMR Ordinance and the Guidelines.

20. To the extent of any conflict between the terms and provisions of the Guidelines attached hereto as Exhibit B and the terms and provisions of the Agreement, the terms and provisions of this Agreement shall prevail.

Signatures on next page

IN WITNESS WHEREOF, the parties hereto have executed this Agreement as of the day and year first written above.

City of Menlo Park	389 El Camino Associates, LLC, a California limited liability company
By: _____ Name: Alex D.McIntyre Its: City Manager	By: Matteson Real Estate Equities, Inc., a California corporation, its Manager By: _____ Name: _____ Its: _____

Notarial acknowledgement for the City and 389 El Camino Associates, LLC are attached.

List of Exhibits

- Exhibit A: Property Description
- Exhibit B: BMR Guidelines
- Exhibit C: BMR Unit Locations
- Exhibit D: BMR Floor Plans

EXHIBIT A

LEGAL DESCRIPTION

EXHIBIT B

GUIDELINES

[The City's Below Market Rate Housing Program Guidelines as modified or amended as of May 10, 2011 are incorporated herein by this reference]

EXHIBIT C

BMR UNIT LOCATIONS

EXHIBIT D

BMR FLOOR PLANS

389 El Camino Real, Menlo Park Inclusionary Housing Plan



Submitted: April 24, 2012



Matteson Realty Services, Inc.
Matteson Real Estate Equities, Inc.
Matteson Management Services, Inc.
Matteson Development Partners, Inc.
JB Matteson, Inc.

April 24, 2012

Ms. Deanna Chow
Senior Planner, Planning Department
City of Menlo Park
701 Laurel Street
Menlo Park, CA 94025

Re: Inclusionary Housing Plan – 389 El Camino Real, Menlo Park

Dear Deanna,

This correspondence outlines the Inclusionary Housing Plan for our proposed 26-unit for-sale residential project for the site located at 389 El Camino Real, Menlo Park. This IHP includes the following:

- 1) Project Objectives
- 2) Project Description
- 3) Affordable Unit Count By Unit Type and Level of Affordability
- 4) Parameters for Establishing the Initial Sales Price
- 5) Characteristics of BMR Units
- 6) Eligibility Requirements for Households Applying to Purchase BMR Units
- 7) BMR Unit Purchase Process, Buyer Selection and Sale Procedures
- 8) Application of Government Code Section 65915, The State Density Bonus Law
- 9) Draft BMR For-Sale Agreement (the “BMR Housing Agreement”), Marked to Show Changes from the Template Supplied by Senior Planner

1) Project Objectives:

Our objective as the applicant is to develop a residential project that is economically feasible and contributes to the City of Menlo Park’s housing stock. In furtherance of that goal, our specific objectives for the project are as follows:

- Redevelop an underutilized site with a mixture of attached and detached single-family units that is compatible with the surrounding neighborhood
- Design the project in a way that is sensitive to the character of the Allied Arts Neighborhood to the west
- Encourage in-fill development in the City of Menlo Park and allow for a more vibrant mix and density of land uses
- Provide housing opportunities, including affordable housing, for existing and future residents of Menlo Park
- Create development that enhances the visual character of the El Camino Real corridor
- Locate a project in close proximity to a regional transportation corridor with good local access from major streets and freeways, and

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- Locate a project in close proximity (i.e., easy access by foot and/or bike) to transit services and other major local and regional services and employment centers, including the Safeway grocery shopping complex, the Stanford Shopping Center, the Stanford Hospital, the Menlo Park City Civic Center, and the Menlo Park Caltrain Station.

We have noted that in the environmental impact reports for the Bohannon development on the east side of the City as well as similar analysis of the former Cadillac dealership site on El Camino Real, a significant “jobs-housing imbalance” has occurred in the City as commercial development has outpaced residential development. One of the key benefits of the 389 El Camino Real project is to continue to address that imbalance by providing new housing in proximity to Downtown and transit services, while especially also including the development *on site* of badly needed affordable housing units.

2) Project Description:

The 389 El Camino Real project involves the redevelopment of an approximately 1.23 acre site located along the El Camino Real corridor, between College Avenue on the North and Partridge Avenue on the south. The vast majority of the site consists of a paved parking lot that was the site of the former Anderson Truck Sales. The project would also demolish a small 1,280 square foot uninhabited single family residence constructed between 1910 and 1925 that fronts on Partridge Avenue, and one 4,250 square foot partially occupied triplex building constructed in 1948 that fronts on College Avenue. The project would involve the construction of twenty-six (26) new residential units, consisting of six (6) detached single-family homes, three (3) semi-attached single-family homes, and seventeen (17) attached townhouse units. The townhomes are arranged in a series of four rows perpendicular to El Camino Real; one single family home would face Partridge Avenue, one single family home would face College Avenue, and the balance of the detached and semi-attached single family homes would be located parallel to El Camino Real along the rear of the property that adjoins the Allied Arts Residential neighborhood. A schematic site plan and landscaped site plan are enclosed for your reference.

Two of the twenty-six units (8%) are three-story townhome units that have two bedrooms and two baths and range from 1,316 to 1,342 square feet. Fifteen of the units (58%) are three-story townhome units that have three bedrooms and three to three and one-half baths and range from 1,465 to 2,011 square feet. Nine of the units (35%) are the single family homes containing four bedrooms and two and a half baths and range from 1,934 to 2,059 square feet.

The project includes 18,315 square feet of open space (approximately 34% of the site), of which 7,256 square feet is private open space clustered around the nine small-lot single family units, while 11,059 square feet is shared open space, divided into a small “pocket park” located on College Avenue centered on a heritage Redwood Tree, as well as a large landscaped area adjacent to El Camino Real that contains seating, lawns, landscaping, barbeques, a decorative trellis, and a fountain.

Every residence, including all townhomes, has an enclosed two-car garage, totaling fifty-two (52) spaces. An additional ten (10) commonly shared spaces, including two (2) handicapped spaces, are provided on the site for visitors and are located adjacent to the large open space fronting El Camino Real.

3) Affordable Unit Count and Level of Affordability:

Three (3) of the twenty six (26) residences shall be set aside on site as affordable units for “Low Income” families (the “BMR Units”). These 3 “low” units are designated as Unit #2, Unit #8 and Unit #13 as shown on the attached diagram. Please also refer to the attached floor plans for each of the units.

4) Parameters for Establishing the Initial Sales Price:

The initial maximum sales price for the BMR Units will be established in substantial compliance with the City of Menlo Park’s Below-Market-Rate Housing Program Guidelines (the “BMR Guidelines”) and as reflected in the attached Draft BMR Housing Agreement.

5) Characteristics of BMR Units:

The unit types, floor plans, sizes and locations were outlined in Paragraph 3 above. The other characteristics of the BMR Units, including Design and Materials as well as Legal Characteristics shall be as set forth in the BMR Guidelines.

6) Eligibility Requirements for Households Applying to Purchase BMR Units:

The Eligibility Requirements as set forth in Section 6 of the BMR Guidelines shall apply to purchasers of the BMR Units.

7) BMR Unit Purchase Process, Buyer Selection and Sale Procedures:

The BMR Unit Purchase Process, Buyer Selection and Sale Procedures shall be as set forth in Section 8 of the BMR Guidelines. The Matteson Companies have recently completed the sales of 90 units in a condominium project in San Carlos that included BMR units and similar procedures, and we are prepared to cooperate and work with the City to accomplish the sale of the BMR Units as contemplated by the BMR Guidelines.

8) Application of Government Code Section 65915, State Density Bonus:

389 El Camino is being submitted subject to the State Density Bonus Law, Government Code Section 65915 and relevant amendments.

Density Bonus: The project is providing three (3) “Low Income” units, which exceeds the affordability requirements for the project as mandated by the City’s Below Market Rate Guidelines. Pursuant to GC Section 65915, the provision of three (3) “Low Income” units entitles the Developer to a density bonus of 26%, or 6 units, resulting in a maximum permitted density of 27 units. The development plan, as submitted, consists of 26 units or 1 unit less than the permitted maximum density. The applicability of GC Section 65915 and this density bonus calculation have been confirmed by the City Attorney.

Incentives / Concessions: GC Section 65915 entitles us to request and receive from the City one “financially sufficient” incentive or concession, in order to accommodate the BMR Units as well as the additional units provided by the Density Bonus described above. The concession we have requested relates to the FAR necessary to accommodate the project as designed while not sacrificing quality of the units or the livability of the development. The purpose of this incentive / concession is to offset the significant subsidy created as a result of the restricted sales price on the three (3) designated Below Market Rate residences.

Development Standard Waivers: As provided for in GC Section 65915, in order to construct the proposed 26 unit project, we are entitled to receive waivers or variances to certain development standards in the Menlo Park zoning ordinances applicable to the project site. During the design revision process for the project over the past two years, the Matteson Companies met extensively with City Staff and the Neighborhood Task Force for College and Partridge Avenues to (a) address the concerns of the neighbors and City Staff to the original project design, and (b) to find ways to reduce the number of Development Standard Waivers to only those absolutely needed in order to accommodate the project as envisioned, including the three (3) BMR Units and the five (5) additional units the project contains over and above the original zoning pursuant to GC Section 65915.

There are now a total of five (5) Development Standard Waivers:

- One Development Standard Waiver relates to a minor internal setback (not impacting project neighbors or frontages) between the R-3 zoned parcel (four homes total) and the C-4 zoned parcel (22 homes total).
- One Development Standard Waiver relates to Lot Coverage in the R-3 zone only, and could be eliminated if the rear homes were returned to three (3) stories from two (2), but lowering the height of these homes is a critical issue for the neighbors, and we thus advocate leaving these homes at the lower height limit and having a lot coverage in the R-3 zone of 44.7% instead of 30% as the zoning of the R-3 district would normally call for. The project requires no Development Standard Waiver for Lot Coverage in the C-4 zoning area.
- A third Development Standard Waiver relates to Landscaped Area, again only in the R-3 zoned area. Similarly with Lot Coverage, this Development Standard waiver could be eliminated if the rear homes were returned to three (3) stories, but for the reasons expressed above we believe usage of the waiver is more appropriate. As a result, our Landscaped Area in the R-3 zone is 42.9% vs. a target of 50%.

- The final two Development Standard Waivers relate to the same issue – FAR. The issue relates to two Development Standard Waivers solely because it impacts both the R-3 Zone as well as the C-4 Zone. FAR is the Development Standard Waiver most directly linked to the Density Bonus and the accommodation of the BMR Units and the additional units allowed as a bonus under State Density Bonus Law. Additional FAR is logically needed to absorb the additional units.

No Development Standard Waivers are requested for building heights, perimeter setbacks, paving, lot area, width, depth or parking.

Parking Standards: Pursuant to GC 65915, the Developer for the project at 389 El Camino Real is requesting, and is entitled, to the use of State parking standards as follows:

2 and 3 Bedroom Units	2 on-site parking spaces
4 Bedroom Units	2.5 on-site parking space

Guest and handicap parking are included in the above totals. The spaces may be offered as tandem spaces and as uncovered spaces. The use of the State parking standards is not considered an incentive or concession. We are actually exceeding these minimum requirements in the project as submitted. While the project would be required to have 57 spaces, 62 spaces are being provided.

9) **Draft BMR Housing Agreement:**

See the attached draft of a BMR Housing Agreement, prepared in accordance with the BMR Guidelines. This draft was prepared from a template supplied by City Planning Staff and has been marked to reflect changes to that draft.

We would appreciate your review and comment on this Inclusionary Housing Plan at your earliest convenience.

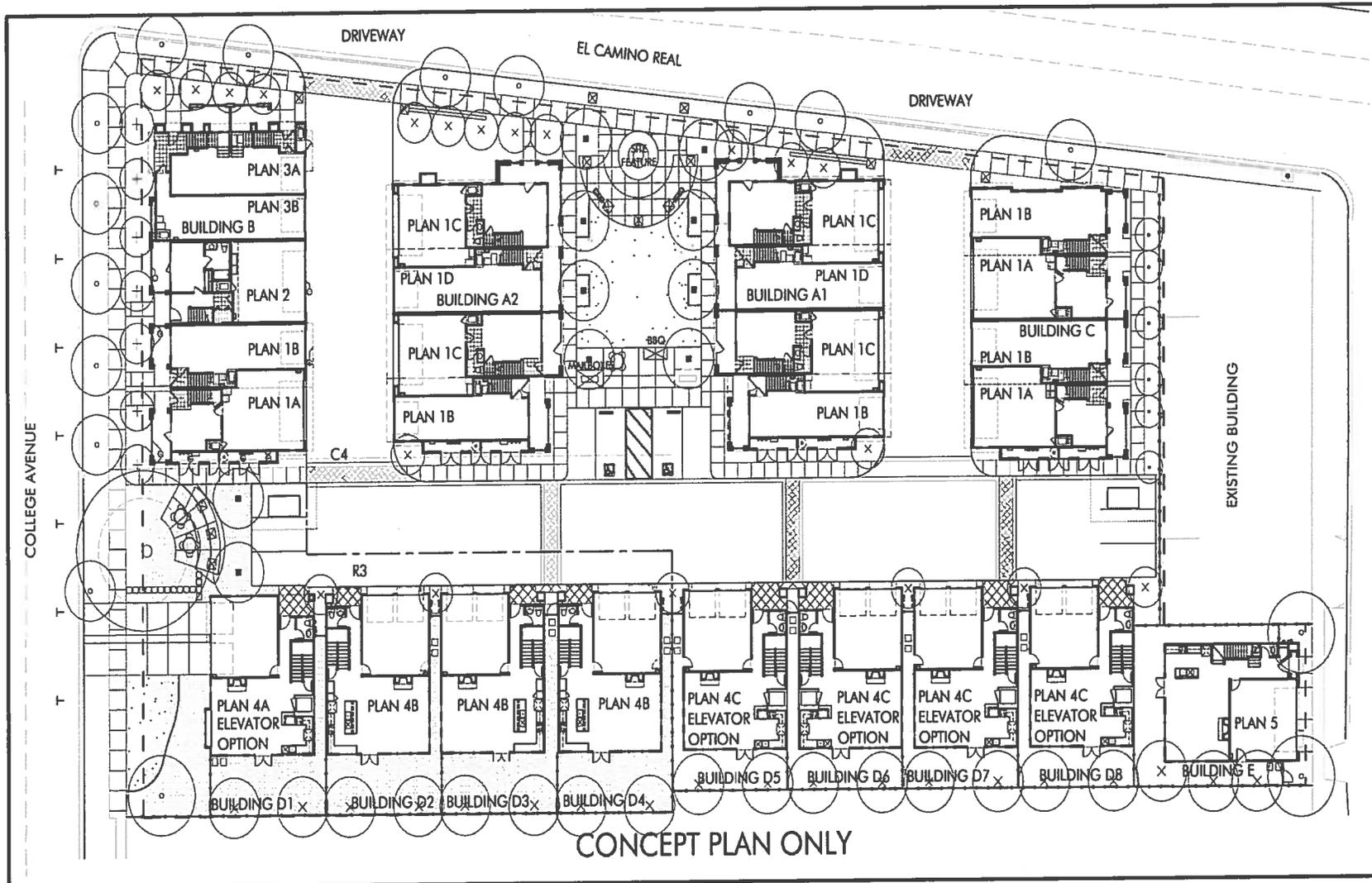
389 EL CAMINO ASSOCIATES, LLC
By: Matteson Real Estate Equities, Inc.
Manager



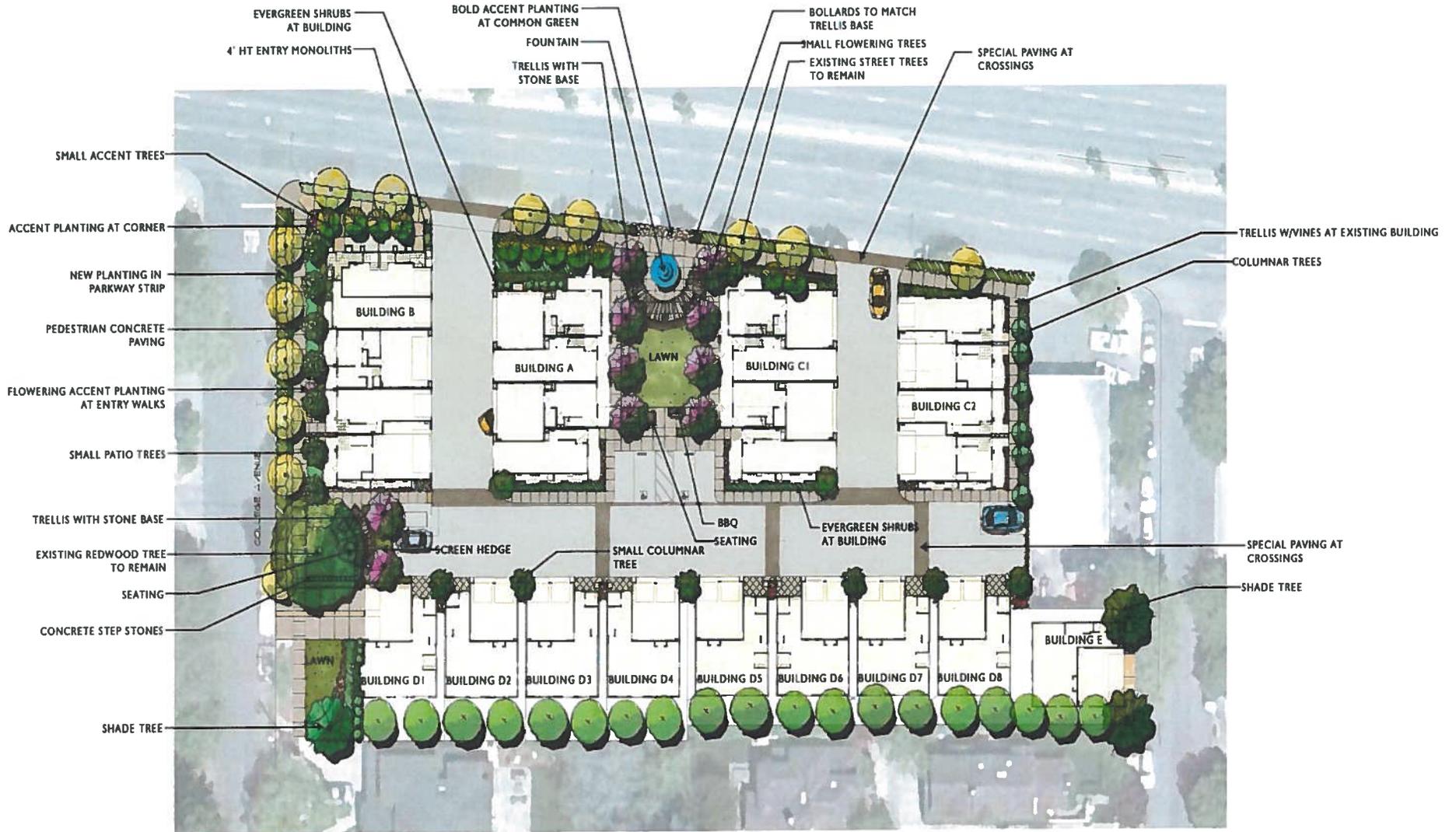
Matt Matteson
President

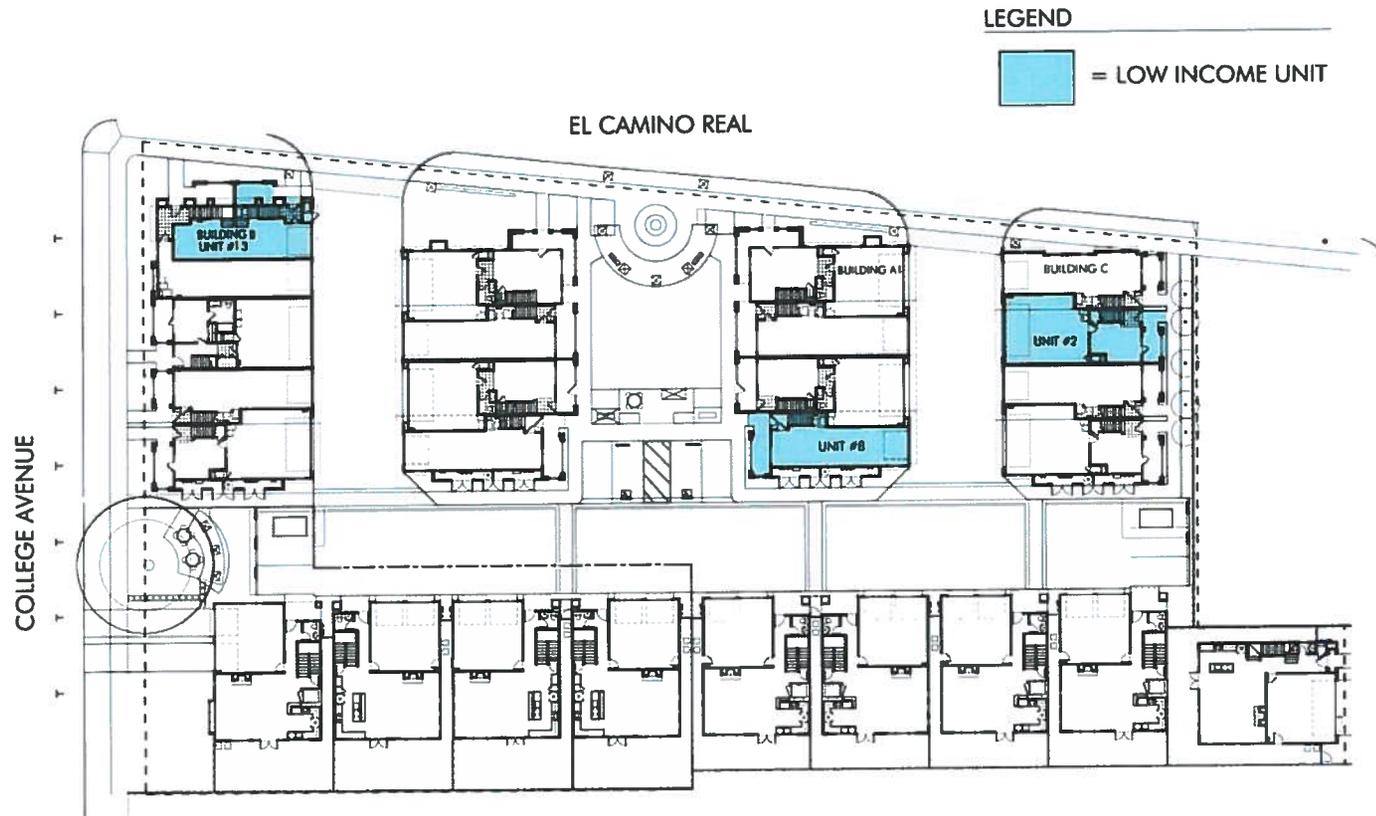
Cc: Mr. William R. Garrett, Esq., Hanna & Van Atta
Mr. David Blackwell, Allen Matkins et al.

Project Site Plan – March 19, 2012



Landscape Plan





LEGEND

= LOW INCOME UNIT

UNIT # 2 PLAN 1A
 TYPE: TOWNHOUSE
 STORIES: 3
 BEDROOMS: 3
 BATHROOMS: 3
 DENS: 1
 SQUARE FOOTAGE: 1,605 SF

UNIT # 8 PLAN 1B
 TYPE: TOWNHOUSE
 STORIES: 3
 BEDROOMS: 3
 BATHROOMS: 3
 SQUARE FOOTAGE: 1,493 SF

UNIT # 13 PLAN 3A
 TYPE: TOWNHOUSE
 STORIES: 3
 BEDROOMS: 2
 BATHROOMS: 2.5
 SQUARE FOOTAGE: 1,342 SF

LOW INCOME UNITS



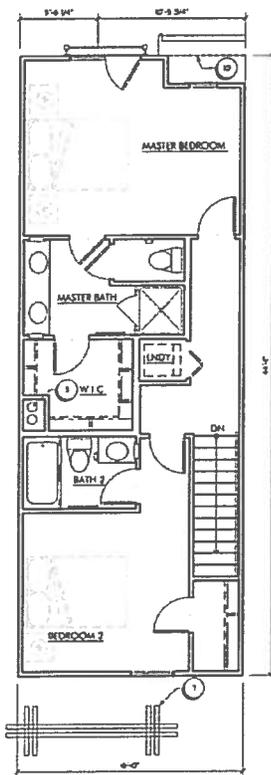
DAHLIN GROUP



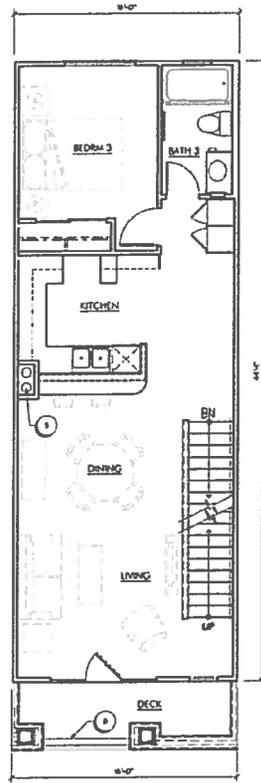
389 EL CAMINO, MENLO PARK, CA

MATTESON DEVELOPMENT PARTNERS, INC.

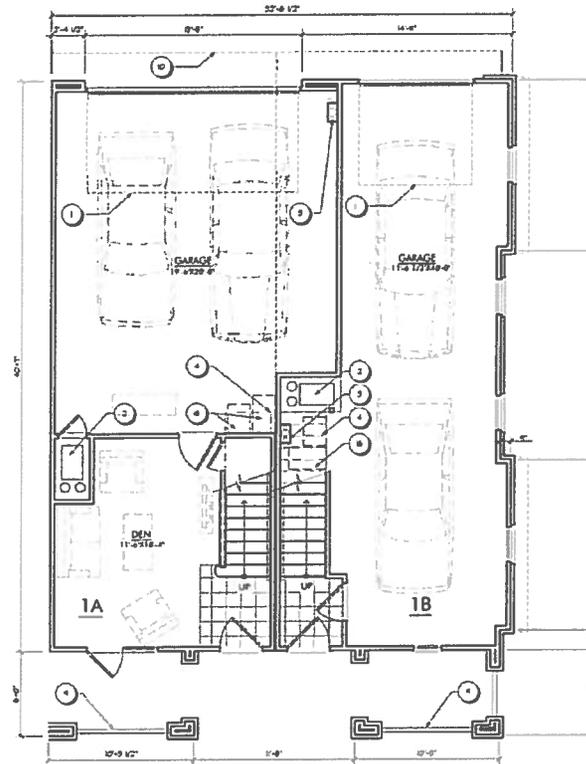
3843 OWING DRIVE
 PLEASANTON, CA 94588
 925-211-7200
 925-211-7201 Fax



PLAN 1B - UNITS 3, 16
PLAN 1A - UNIT 2
THIRD FLOOR



PLAN 1B - UNITS 3, 16
PLAN 1A - UNIT 2
SECOND FLOOR



PLAN 1A - UNIT 2
FIRST FLOOR

PLAN 1B - UNIT 1
FIRST FLOOR

- FLOOR PLAN KEY NOTES:**
1. Overhead garage door above
 2. Furnace & ducts
 3. Tank-less water heater
 4. Line of stair above
 5. Duct space
 6. Roof access ladder
 7. Trellis- see elevation
 8. Guardrail
 9. Decorative railing
 10. Lines of floor above/ below
 11. Optional elevator/ storage
 12. Attic access
 13. Attic furnace
 14. Meter locations
 15. Fireplace
 16. Trash / recycling containers

UNIT # 2 PLAN 1A
TYPE: TOWNHOUSE
STORIES: 3
BEDROOMS: 3
BATHROOMS: 3
DENS: 1
SQUARE FOOTAGE: 1,605 SF

TOWNHOME - UNIT PLANS
 PLAN 1B - FLOOR PLAN

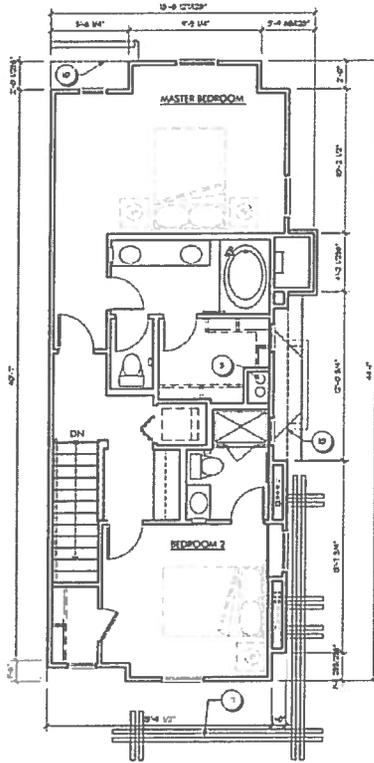
PROJECT NO: 271.009

DAHLIN GROUP

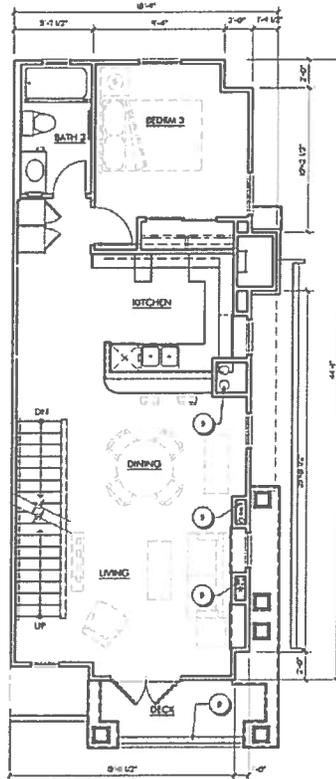
3845 OWENS DRIVE,
 PLEASANTON, CA 94588
 925.251.7000
 925.251.7201 Fax

A3.1a2

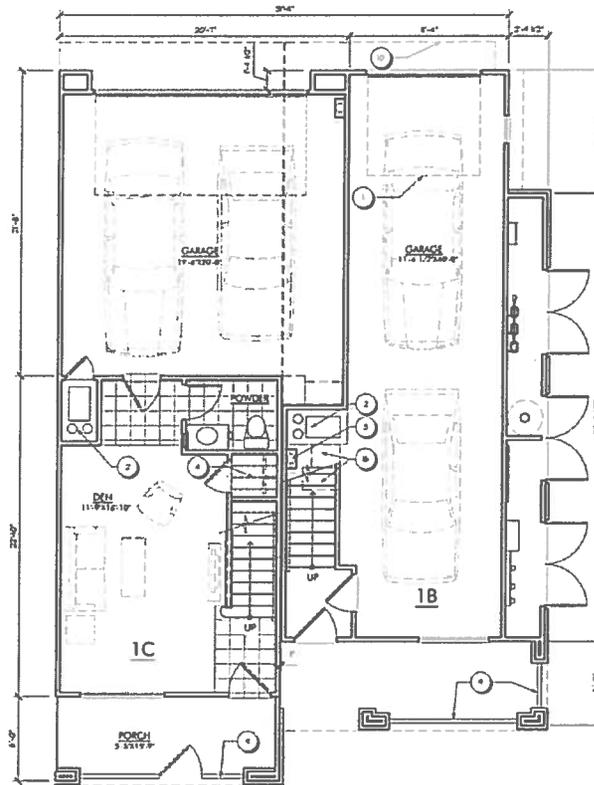
389 EL CAMINO, MENLO PARK, CA
 MATTESON DEVELOPMENT PARTNERS, INC.



PLAN 1B - UNITS 8, 12
THIRD FLOOR



PLAN 1B - UNITS 8, 12
SECOND FLOOR



PLAN 1C - 7, 11
FIRST FLOOR
SEE A3.1C2

PLAN 1B - UNITS 8, 12
FIRST FLOOR

FLOOR PLAN KEY NOTES: ①

1. Overhead garage door above
2. Furnace & ducts
3. Tank-less water heater
4. Line of stair above
5. Duct space
6. Roof access ladder
7. Trellis- see elevation
8. Guardrail
9. Decorative railing
10. Lines of floor above/ below
11. Optional elevator/ storage
12. Attic access
13. Attic furnace
14. Meter locations
15. Fireplace
16. Trash / recycling containers

UNIT # 8 PLAN 1B
TYPE: TOWNHOUSE
STORIES: 3
BEDROOMS: 3
BATHROOMS: 3
SQUARE FOOTAGE: 1,493 SF

TOWNHOME - UNIT PLANS
 PLAN 1C & 1D - FLOOR PLAN

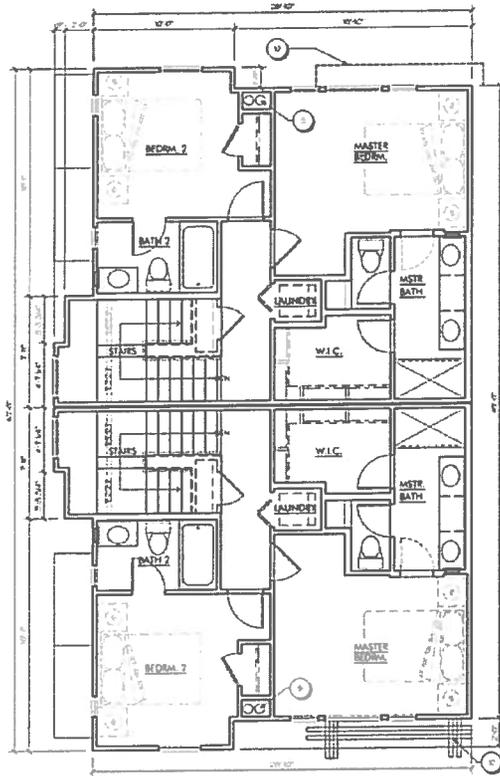


DAHLIN GROUP
 ARCHITECTS

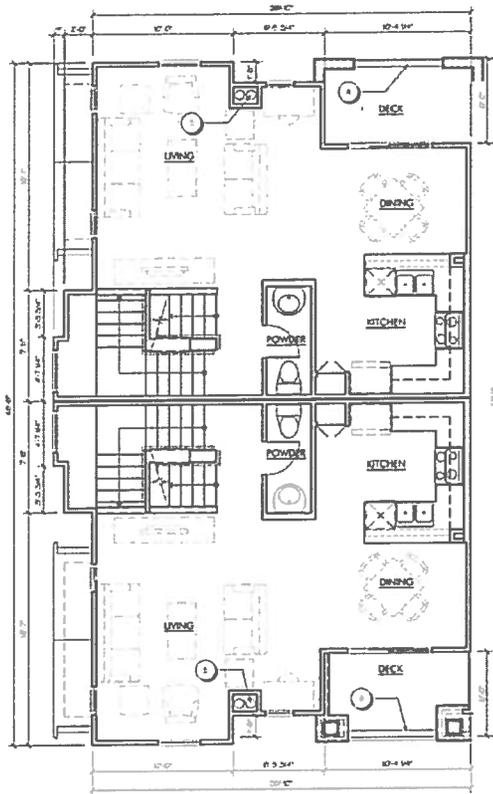
5645 OWENS DRIVE,
 PLEASANTON, CA 94568
 925.251.7200
 925.251.7201 Fax

A3.1c3

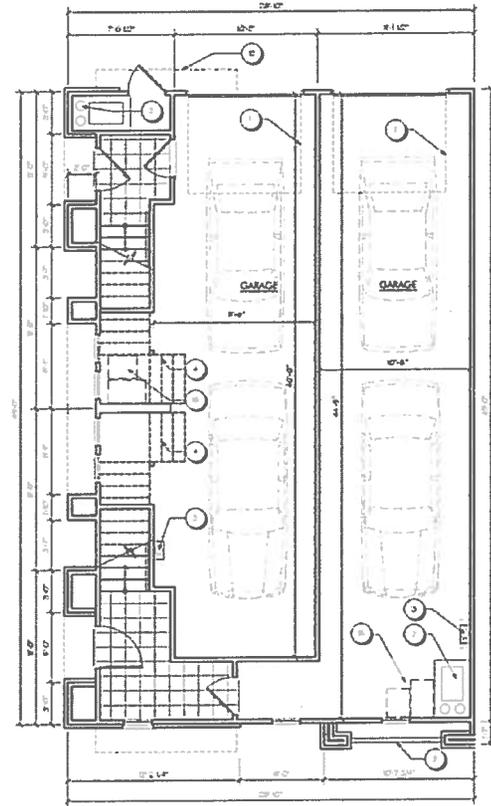
389 EL CAMINO, MENLO PARK, CA
 MATTESON DEVELOPMENT PARTNERS, INC.



PLAN 3A & 3B
THIRD FLOOR



PLAN 3A & 3B
SECOND FLOOR



PLAN 3A
FIRST FLOOR

PLAN 3B
FIRST FLOOR

- FLOOR PLAN KEY NOTES:**
1. Overhead garage door above
 2. Furnace & ducts
 3. Tank-less water heater
 4. Line of stair above
 5. Duct space
 6. Roof access ladder
 7. Trellis - see elevation
 8. Guardrail
 9. Decorative railing
 10. Lines of floor above/ below
 11. Optional elevator/ storage
 12. Attic access
 13. Attic furnace
 14. Meter locations
 15. Fireplace
 16. Trash / recycling containers

UNIT # 13 PLAN 3A
TYPE: TOWNHOUSE
STORIES: 3
BEDROOMS: 2
BATHROOMS: 2.5
SQUARE FOOTAGE: 1,342 SF

TOWNHOME - UNIT PLANS
 PLAN 3A & 3B - FLOOR PLANS



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A3.3a

389 EL CAMINO, MENLO PARK, CA
 MATTESON DEVELOPMENT PARTNERS, INC.

**March 19, 2012 – Planning Commission Study Session –
Elevation on College Avenue near El Camino Real**

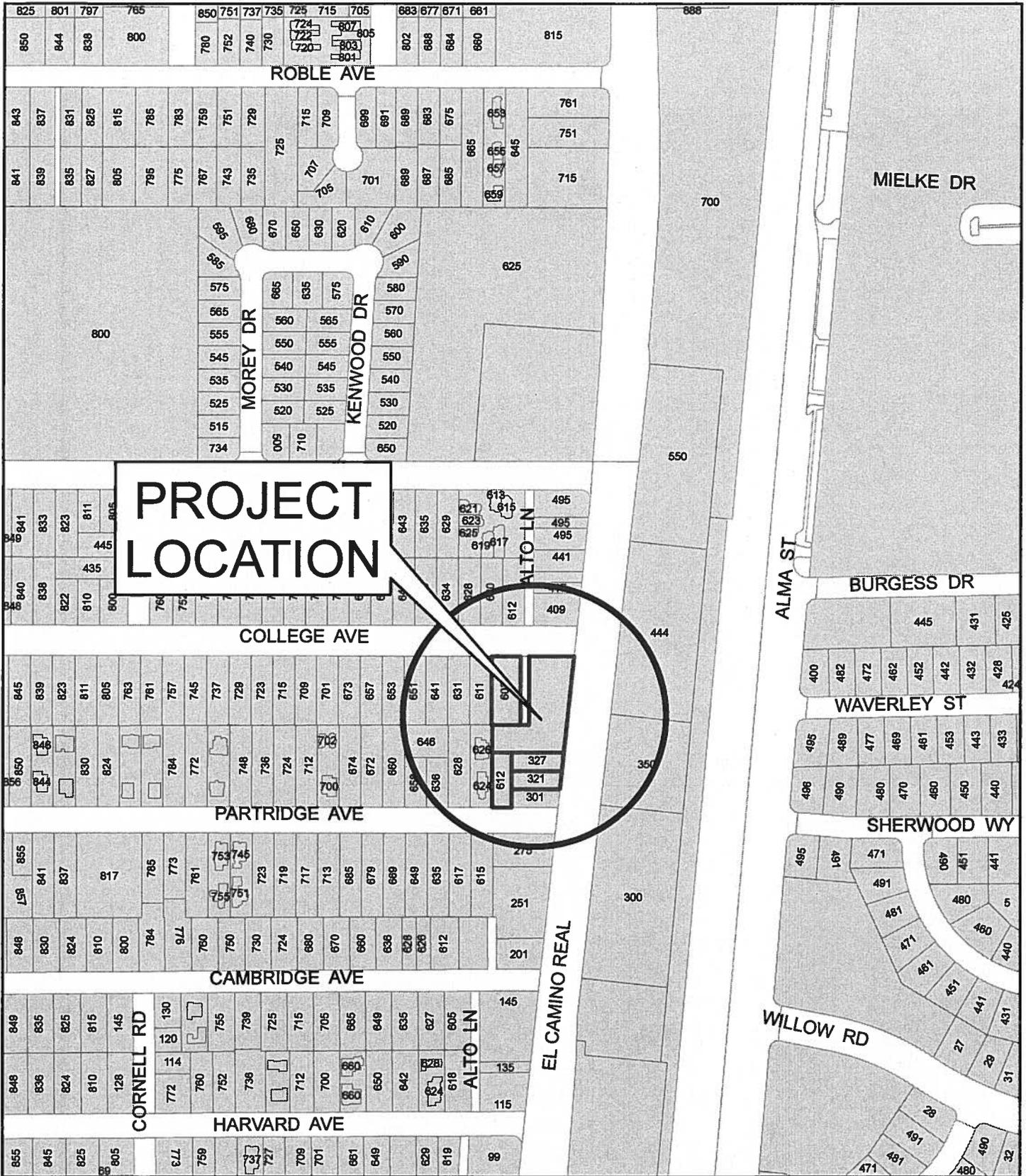


**March 19, 2012 - Planning Commission Study Session –
Elevation at Corner of El Camino and College Avenue**



**March 19, 2010 – Planning Commission Study Session –
Elevation on El Camino Real Planet Auto Repair Shop**

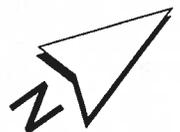




**PROJECT
LOCATION**

CITY OF MENLO PARK
 LOCATION MAP
 389 EL CAMINO REAL

DRAWN: KTP CHECKED: KTP DATE: 07/31/12 SCALE: 1" = 300' SHEET: 1



389 EL CAMINO REAL MENLO PARK, CA

BY MATTESON COMPANIES



PROJECT TEAM

DEVELOPER	THE MATTESON DEV PARTNERS, INC ONE LAGUNA DRIVE SUITE 200 REDWOOD CITY, CA 94061
ARCHITECT	DAHLIN GROUP 3845 CHINDS DRIVE PLEASANTON, CA 94588
CIVIL ENGINEER	BJP ENGINEERS 755 SHORELINE DRIVE, SUITE 200 REDWOOD CITY, CA 94063
LANDSCAPE ARCHITECT	GATES + ASSOCIATES 3871 CROWN CANYON RD. SAN RAMON, CA 94583
UTILITY CONSULTANT	PGA UTILITY DESIGN 4005 VILLAGE PARKWAY, SUITE 204 DUBLIN, CA 94568
ACOUSTIC ENGINEER	CHARLES H. SALTER & ASSOCIATES, INC. 70 SUTTER STREET SAN FRANCISCO, CA 94104



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A2 5s	PLAN 2S - 3rd FLOOR PLANS	
A2 5t	PLAN 2T - 3rd FLOOR PLANS	
A2 5u	PLAN 2U - 3rd FLOOR PLANS	
A2 5v	PLAN 2V - 3rd FLOOR PLANS	
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A2 6o	PLAN 3O - 3rd FLOOR PLANS	
A2 6p	PLAN 3P - 3rd FLOOR PLANS	
A2 6q	PLAN 3Q - 3rd FLOOR PLANS	
A2 6r	PLAN 3R - 3rd FLOOR PLANS	
A2 6s	PLAN 3S - 3rd FLOOR PLANS	
A2 6t	PLAN 3T - 3rd FLOOR PLANS	
A2 6u	PLAN 3U - 3rd FLOOR PLANS	
A2 6v	PLAN 3V - 3rd FLOOR PLANS	
A2 6w	PLAN 3W - 3rd FLOOR PLANS	
A2 6x	PLAN 3X - 3rd FLOOR PLANS	
A2 6y	PLAN 3Y - 3rd FLOOR PLANS	
A2 6z	PLAN 3Z - 3rd FLOOR PLANS	
A2 7	GREEN SHEET	

UNIT DATA

Plan Type	Unit #	Stories	Bedroom Type	Sq. Ft.	Total (plan type)	Total (bedroom type)	% of Total
1A	2	3	3 BD + D / 3.5 BTH	1853	3	14	12%
	4	3	3 BD + D / 3.5 BTH	1732			
	17	3	3 BD + D / 3.5 BTH	1732			
1B	1	3	3 BD / 3 BTH	1554	5		10%
	3	3	3 BD / 3 BTH	1471			
	6	3	3 BD / 3 BTH	1493			
	12	3	3 BD / 3 BTH	1493			
	16	3	3 BD / 3 BTH	1471			
1C	5	3	3 BD + D / 3.5 BTH	2094	4		15%
	7	3	3 BD + D / 3.5 BTH	1864			
	9	3	3 BD + D / 3.5 BTH	2094			
	11	3	3 BD + D / 3.5 BTH	1864			
1D	6	3	3 BD / 3 BTH	1582	2		7%
	19	3	3 BD / 3 BTH	1582			
	2	15	3 BD / 3.5 BTH	2039	1	1	4%
3A	13	3	2 BD / 2.5 BTH	1342	1	2	4%
3B	14	3	2 BD / 2.5 BTH	1410	1	2	4%
4A	18	2	4 BD / 2.5 BTH	2054	1	8	4%
	19	2	4 BD / 2.5 BTH	2056	3		12%
	20	2	4 BD / 2.5 BTH	2059			
	21	2	4 BD / 2.5 BTH	2059			
4C	22	2	4 BD / 2.5 BTH	2034	4		15%
	23	2	4 BD / 2.5 BTH	2034			
	24	2	4 BD / 2.5 BTH	2034			
	25	2	4 BD / 2.5 BTH	2034			
5	26	2	4 BD / 2.5 BTH	1825	1	1	4%
Total Unit Count					28	100%	

BUILDING DATA

BLDG	FIRST FLOOR S.F.	SECOND FLOOR S.F.	THIRD FLOOR S.F.	Total Buildings	Total S.F.
A1	943 SF	3,029 SF	2,959 SF	1	7,055 SF
A2	945 SF	3,039 SF	2,959 SF	1	7,055 SF
B	1,091 SF	3,449 SF	3,433 SF	1	8,105 SF
C	768 SF	7,853 SF	7,789 SF	1	16,527 SF
D1	788 SF	1,266 SF	N/A	1	2,054 SF
D2-D4	797 SF	1,262 SF	N/A	3	2,809 SF
D5-D8	772 SF	1,262 SF	N/A	4	2,834 SF
E	740 SF	1,182 SF	N/A	1	1,922 SF

PARKING REQUIREMENTS (San Parking Standards per Government Code Section 65913)

UNIT TYPE	SPACES/UNIT	# OF UNITS	SPACES REQ.
2 BED	2.0	7	4
3 BED	2.0	13	26
4 BED	2.5	9	23
TOTAL RESIDENTIAL			53

PARKING PROVIDED

COVERED STANDARD	COVERED TANDEM	UNCOVERED GUEST	ACCESSIBLE	TOTAL PROVIDED
34	18	8	7	67

OPEN SPACE

COMMON OPEN SPACE	
TOTAL COMMON OPEN SPACES	11,059 SF
COMMON OPEN SPACE PER UNIT	475 SF/UNIT
PRIVATE OPEN SPACE	
TOTAL PRIVATE OPEN SPACES	7,250 SF
TOTAL OPEN SPACE ((COMMON + PRIVATE)	18,315 SF
TOTAL OPEN SPACE PER UNIT ((COMMON + PRIVATE)	704 SF/UNIT
COMMON & PRIVATE OPEN SPACE	34% OF SITE AREA

SITE AREA

TOTAL SITE AREA	53,647 SQFT
PAVING/HARDSCAPE AREA	12,791 SQFT
PERCENTAGE OF SITE	24%
RESIDENTIAL AREA	24,554 SQFT
PERCENTAGE OF SITE	45.76%
LANDSCAPED AREA	16,375 SQFT
PERCENTAGE OF SITE	30.47%
TRELLISES	485 SQFT, 0.01%

PROJECT DATA

SITE DATA	
SITE APTN	071-412-270, 230, 230, 179, 430
SITE AREA (GROSS & NET)	53,647 SF / 1.23 ACRES
SITE ZONING	R2, C4
BUILDING DATA	
GROSS FLOOR AREA	
FIRST FLOOR	10,754 SF
SECOND FLOOR	73,665 SF
THIRD FLOOR	12,160 SF
TOTAL	66,581 SF

DEVELOPMENT PARAMETERS- R2 ZONE

FAR (FLOOR AREA RATIO)	8.231 / 11,146 = 0.74
SITE COVERAGE	4,983 / 11,146 = 0.45
NUMBER OF UNITS	4
MAXIMUM BUILDING HEIGHT	4 / .26 = 15.38 UNITS PER ACRE

DEVELOPMENT PARAMETERS- C4 ZONE

FAR (FLOOR AREA RATIO)	38,350 / 42,516 = 0.9
SITE COVERAGE	19,571 / 42,516 = 0.46
NUMBER OF UNITS	22
DENSITY	22 / 0.98 = 22.45 UNITS PER ACRE
MAXIMUM BUILDING HEIGHT	30 FEET

DEVELOPMENT PARAMETERS-COMBINED SITE

FAR (FLOOR AREA RATIO)	46,581 / 53,647 = 0.87
SITE COVERAGE	24,554 / 53,647 = 0.46
NUMBER OF UNITS	26
DENSITY	26 / 1.23 = 21.12 UNITS PER ACRE

CIVIL ENGINEERING DRAWINGS

101	THEATRE MAP COVER SHEET
102	THEATRE MAP EXISTING CONDITIONS PLAN
103	THEATRE MAP PROPOSED LOTS
104	THEATRE MAP PROPOSED SITE PLAN
105	THEATRE MAP HORIZONTAL CENTERLINE PLAN
106	THEATRE MAP GRADING PLAN
107	THEATRE MAP STREET PLAN
108	THEATRE MAP STREET SECTION AND DETAILS
109	THEATRE MAP FIRE TRUCK TURNING & VEHICULAR CIRC. EXHIBIT
110	THEATRE MAP EXISTING WETLANDS PLAN

UTILITY DRAWINGS

111	JOINT TRENCH TITLE SHEET
112	JOINT TRENCH PLAN

LANDSCAPE DRAWINGS

113	PLANT LIST
114	CONCEPTUAL LANDSCAPE PLAN
115	ELEVATIONS
116	FINLANDERMENTS
117	FIRE PLAN
118	SITE FURNITURE

COVER SHEET & PROJECT DATA

APRIL 6, 2012 PROJECT NO. 221-009

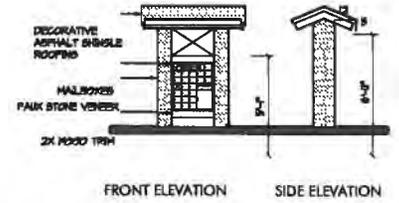
DAHLIN GROUP

389 EL CAMINO, MENLO PARK, CA

MATTESON DEVELOPMENT PARTNERS, INC.

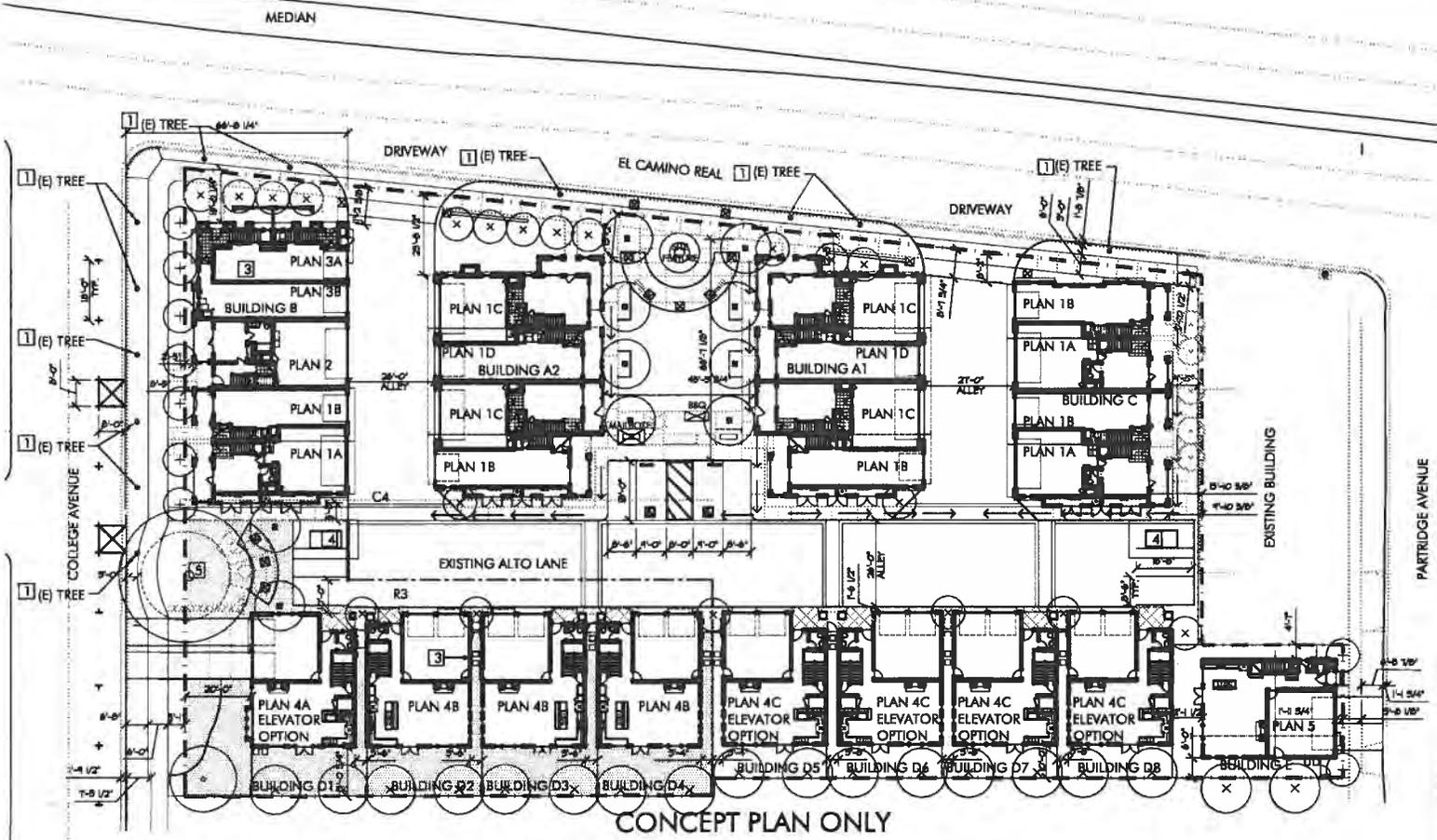
3845 CHINDS DRIVE, PLEASANTON, CA 94588
925.251.7200
925.251.7201 FAX

T01



1 MAILBOXES

- LEGEND**
- ← ACCESSIBLE ROUTE
 - ▣ ACCESSIBLE STALL PER CBC 11.09A.5
 - 1 EXISTING STREET TREES
 - 3 TRASH / RECYCLE LOCATION, TYP
 - 4 TRANSFORMER (BELOW GRADE)
 - 5 EXISTING REDWOOD TREE



CONCEPT PLAN ONLY

ARCHITECTURAL SITE PLAN
SCALE: 1/16" = 1'-0"

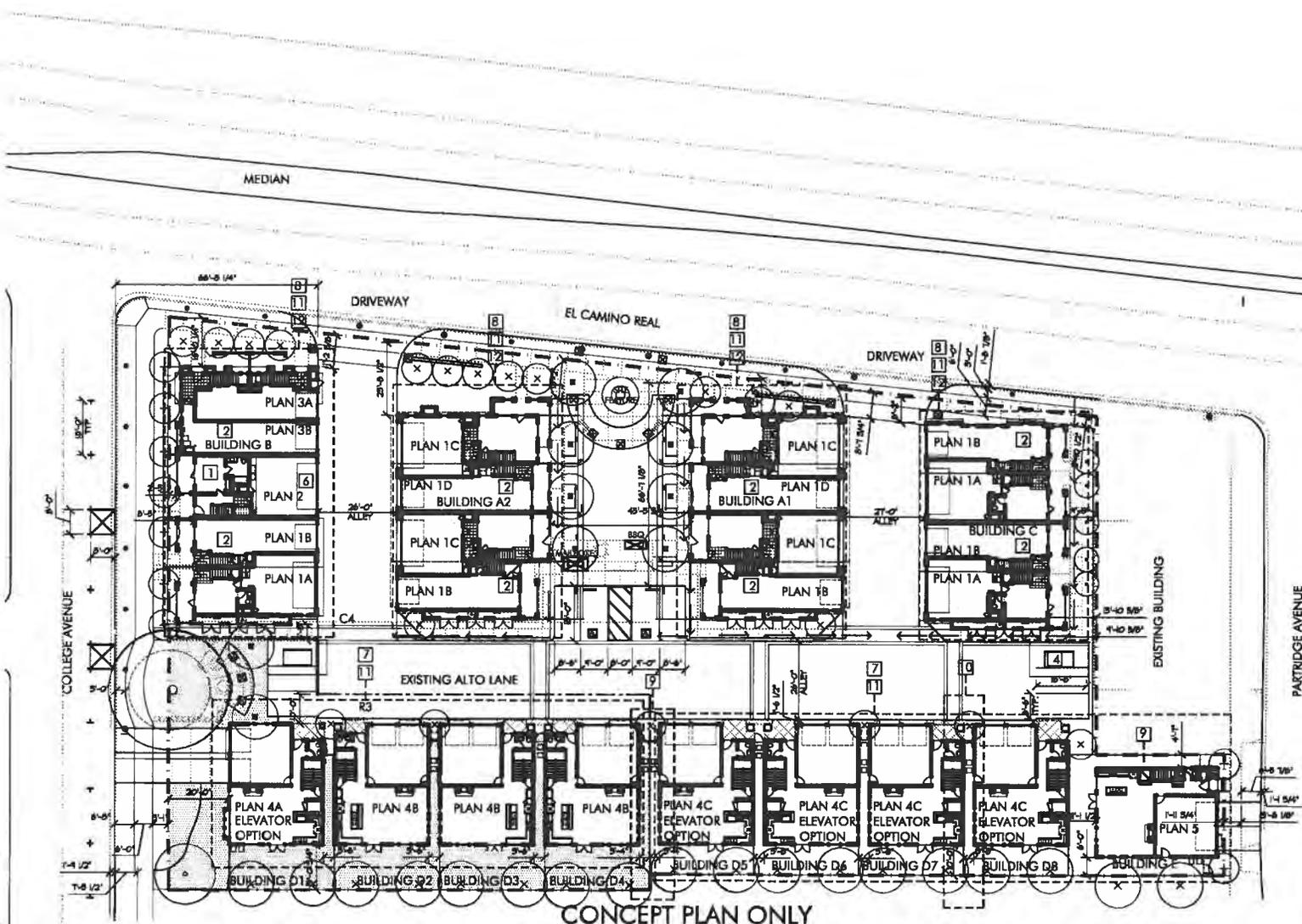
JUNE 8, 2012 PROJECT NO.: 221.007



389 EL CAMINO, MENLO PARK, CA
MATTESON DEVELOPMENT PARTNERS, INC.

3845 OWENS DRIVE,
PASADENA, CA 92386
952.251.7200
952.251.7201 Fax

A1.1a



- LEGEND**
- ← ACCESSIBLE ROUTE
 - ♠ ACCESSIBLE STALL PER CBC 11.09A.5
 - 1 ACCESSIBLE UNIT PER CBC 11.02A.3.1 EXCEPTION
 - 2 CARRIAGE UNIT PER CBC 1107A.1.A
-
- 6 ACCESSIBLE GARAGE DOOR PER CBC 1109A.8.1
 - 7 BUILDINGS ON THE SAME LOT CAN BE CONSIDERED AS ONE BUILDING PER CBC 705.3, EXCEPTION
 TOTAL ALLOWABLE AREA FOR TYPE VB WITH NFPA 13 TABLE 503 - AREA PER FLOOR: 7000 SF
 CBC 506.3 INCREASE FOR SPRINKLERS: 2 X = 14000 SF
 TOTAL ALLOWABLE AREA: 21000 SF
 ACTUAL AREA FOR BUILDINGS D1-D4: 8,231 SF
 ACTUAL AREA FOR BUILDINGS D5-E: 10,061 SF
 - 8 TOTAL ALLOWABLE AREA FOR TYPE VA WITH NFPA 13 TABLE 503 - AREA PER FLOOR: 12000 SF
 CBC 506.3 INCREASE FOR SPRINKLERS: 2 X = 24000 SF
 TOTAL ALLOWABLE AREA: 36000 SF
 ACTUAL AREA FOR BUILDINGS A1: 7,034 SF
 ACTUAL AREA FOR BUILDINGS A2: 7,034 SF
 ACTUAL AREA FOR BUILDINGS B: 7,999 SF
 ACTUAL AREA FOR BUILDINGS C: 6,426 SF
 - 9 1-HOUR WALLS REQUIRED PER CBC TABLE 602
 15% MAX; OPENINGS <5' PER TABLE 705.5
 PARAPET NOT REQUIRED IF ROOF CONSTRUCTED PER CBC 705.11.5
 - 10 EXTERIOR WALLS BETWEEN UNITS NOT REQUIRED TO BE FIRE RATED PER CBC 709.5
 - 11 ONLY ONE EXIT REQUIRED PER CBC 1015.1 EXCEPT, 2
 125' MAX; COMMON PATH OF EGRESS PER CBC 1014.3.5
 - 12 FIRE PARTITION WALLS SEPARATING UNITS REQUIRED PER CBC 420.2 & 709.1
 FLOORS & WALLS SEPARATING UNITS SHALL HAVE A STC RATING OF 50 PER CBC 1207.7
 FLOORS SEPARATING UNITS SHALL HAVE AN IIC RATING OF 50 PER CBC 1207.8

BUILDING CODE ANALYSIS
 SCALE: 1/16" = 1'-0"

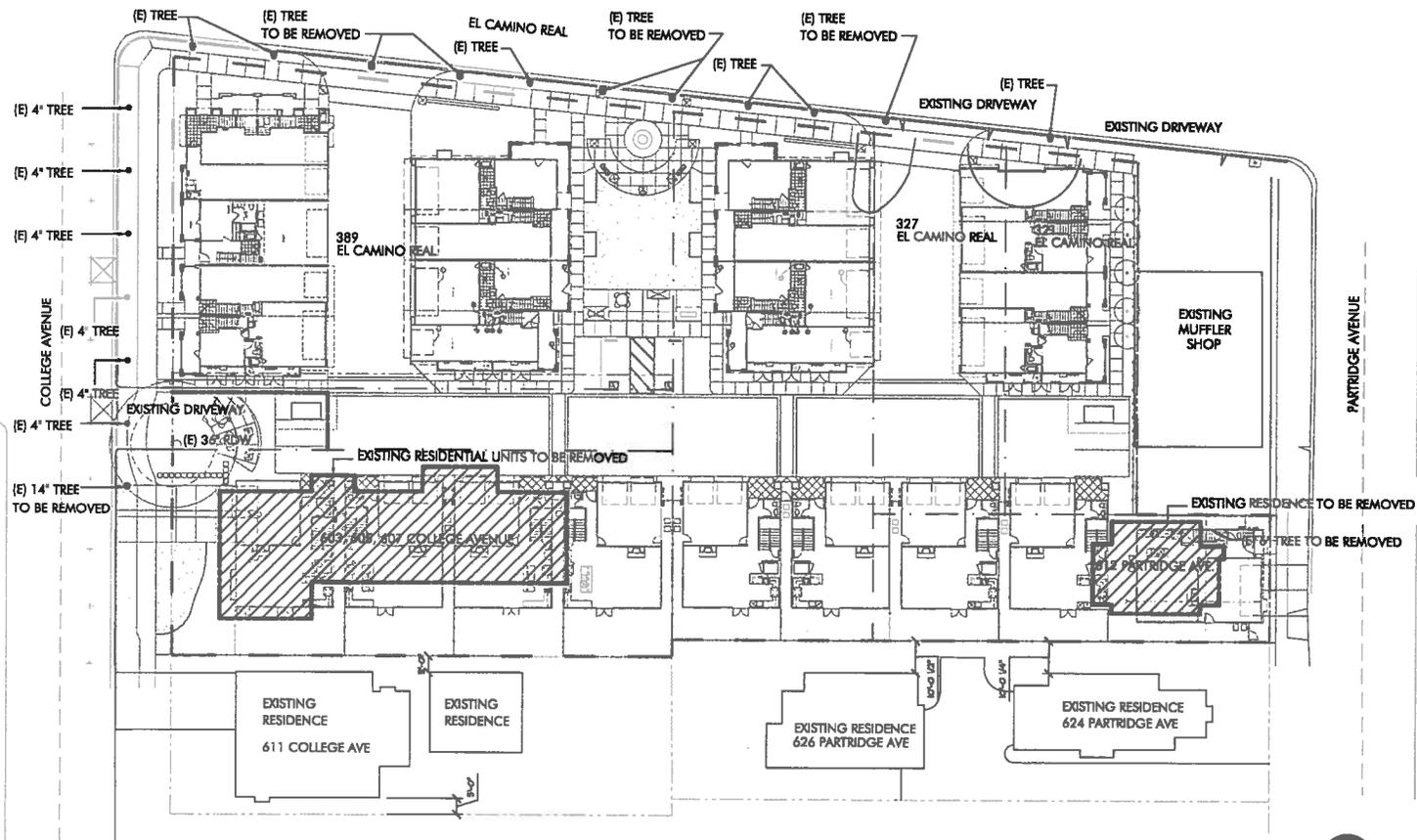
JUNE 8, 2012 PROJECT NO: 221.009



389 EL CAMINO, MENLO PARK, CA
 MATTESON DEVELOPMENT PARTNERS, INC.

3663 OWENS DRIVE,
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A1.1b



AREA PLAN
SCALE: 1/16" = 1'-0"

JUNE 8, 2012 PROJECT NO: 221.009

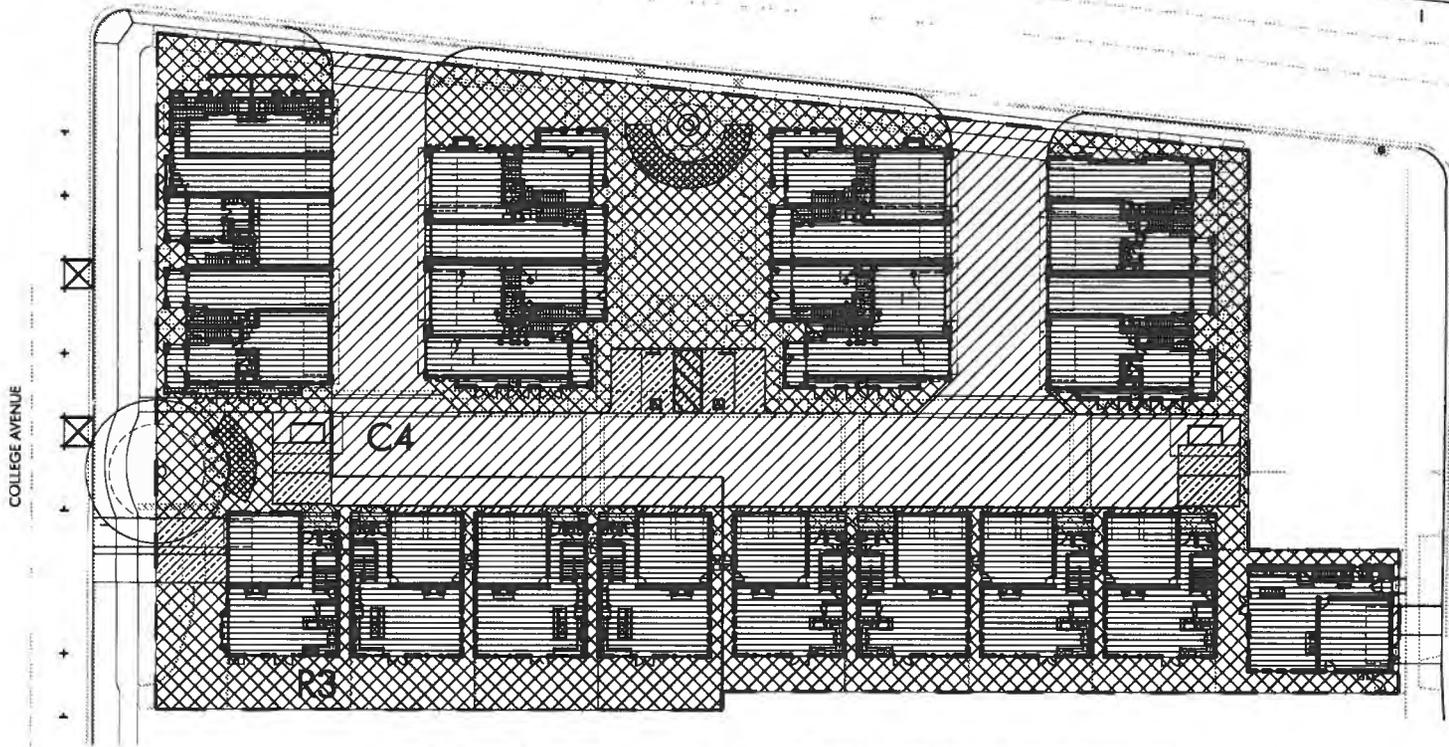


389 EL CAMINO, MENLO PARK, CA

MATTESSON DEVELOPMENT PARTNERS, INC.

5843 OWENS DRIVE,
PLEASANTON, CA 94588
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A1.2



LANDSCAPE AREA BREAKDOWN

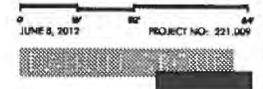
	REQUIRED	PROVIDED
R3:	50% 5,580 SF	42.93% 4,785 SF
C4:	10% 4,251 SF	28.28% 12,025 SF
SITE TOTAL:	18% 9,831 SF	30.42% 16,325 SF

PREVIOUS SUBMITTALS FOR THIS PROJECT HAD 3 STORY HOMES IN THE R3 LOT WHICH HAD A FOOTPRINT OF 870 SF EACH. THIS CURRENT SUBMITTAL HAS REPLACED THOSE 3 STORY HOMES WITH 2 STORY HOMES WHICH MOVED MORE SPACE TO THE GROUND FLOOR, THE HOMES NOW HAVING A FOOTPRINT OF 1,242 SF EACH. THIS HAS RESULTED IN LESS AVAILABLE SITE AREA FOR LANDSCAPING ON THE R3 LOT.

- LEGEND**
- PAVING/HARDSCAPE AREA
12,291 SQFT 22.9%
 - R3: 1020 (PAVING) + 368 (PERVIOUS) = 1,388 SQFT
C4: 10,644 (PAVING) + 588 (PERVIOUS) = 10,903 SQFT
 - RESIDENTIAL AREA (COUNTS TOWARDS BUILDING COVERAGE)
24,542 SQFT 45.73%
 - R3: 4,983 SQFT
C4: 19,585 SQFT
 - LANDSCAPE AREA
1,6325 SQFT 30.42%
 - R3: 4,417 (LANDSCAPE) + 368 (PERVIOUS) = 4,785 SQFT
C4: 11,437 (LANDSCAPE) + 588 (PERVIOUS) = 12,025 SQFT
 - TRELLIS AREA (COUNTS TOWARDS BUILDING COVERAGE)
485 SQFT .01%
 - PERVIOUS PAVING AREA
R3: 736 SQFT
C4: 1,176 SQFT
- NOTE:** PERVIOUS PAVING COUNTS AS 50% LANDSCAPE COVERAGE CREDIT. IT IS USED AT RESIDENT DRIVEWAY APRONS AND PARKING STALLS AS INDICATED.
- SEE LANDSCAPE SPECIFICATIONS FOR PERVIOUS PAVING MATERIAL



AREA CALCULATIONS
SCALE: 1/16" = 1'-0"

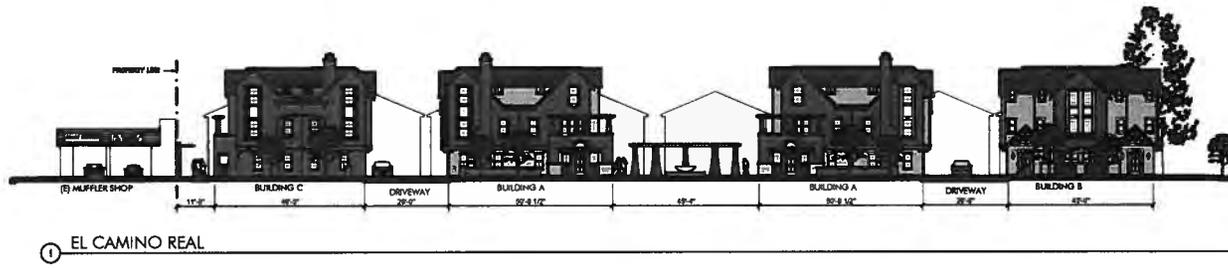
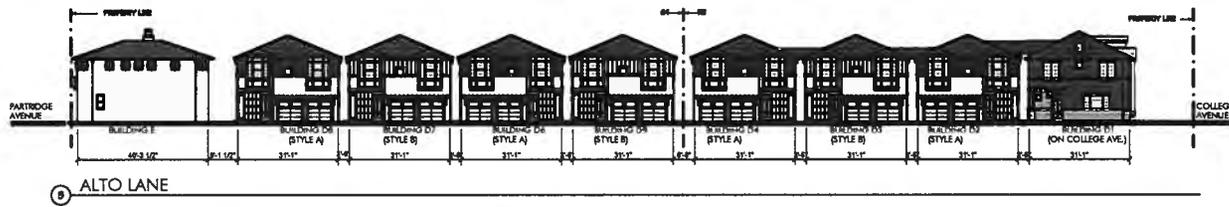


389 EL CAMINO, MENLO PARK, CA

MATTESON DEVELOPMENT PARTNERS, INC.

3865 OWENS DRIVE,
PLEASANTON, CA 94588
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A1.3



STREETSCAPES
 SCALE: 1/16"=1'-0"
 0 10' 20' 30'
 JUNE 8, 2012 PROJECT NO: 221.009
 DARTIN SCALE

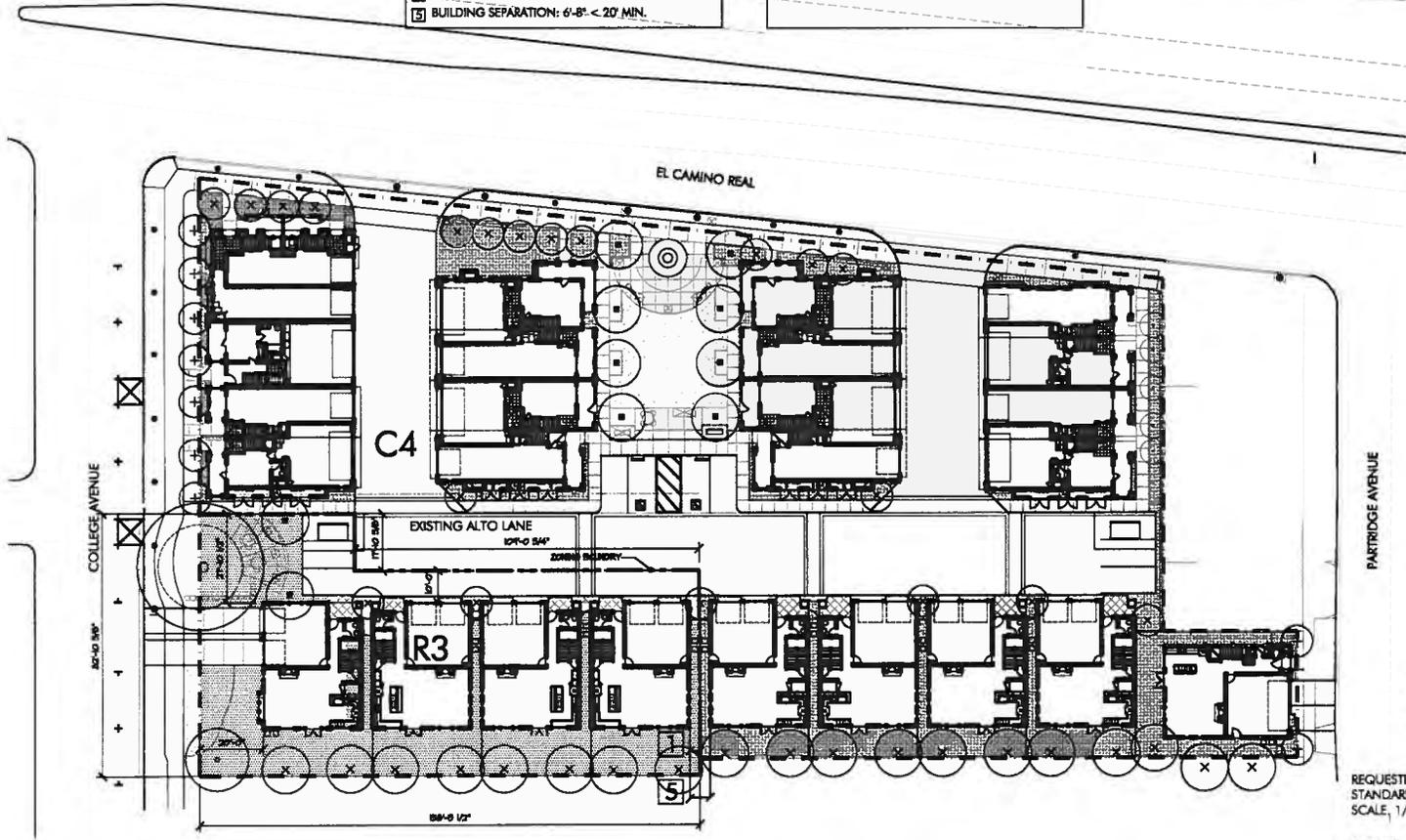
389 EL CAMINO, MENLO PARK, CA
 MATTESON DEVELOPMENT PARTNERS, INC.

5865 OWENS DRIVE
 PLEASANTON, CA 94588
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 925.251.7201 Fax

A1.4a

REQUESTED DEVELOPMENT STANDARD WAIVERS

- | | |
|--|--|
| <p>R3 WAIVERS:</p> <ul style="list-style-type: none"> 1 REAR SETBACK: 3'-4" < 15' MIN. 2 BUILDING COVERAGE: 45% > 30% MAX. 3 FAR: 74% > 45% MAX. 4 LANDSCAPE COVERAGE: 43% < 50% MIN. 5 BUILDING SEPARATION: 6'-8" < 20' MIN. | <p>C4 WAIVERS:</p> <ul style="list-style-type: none"> 1 FAR: 90% > 75% MAX. |
|--|--|



REQUESTED DEVELOPMENT
STANDARD WAIVERS
SCALE: 1/16"=1'-0"

DATE: 8, 2012 PROJECT NO: 221-009



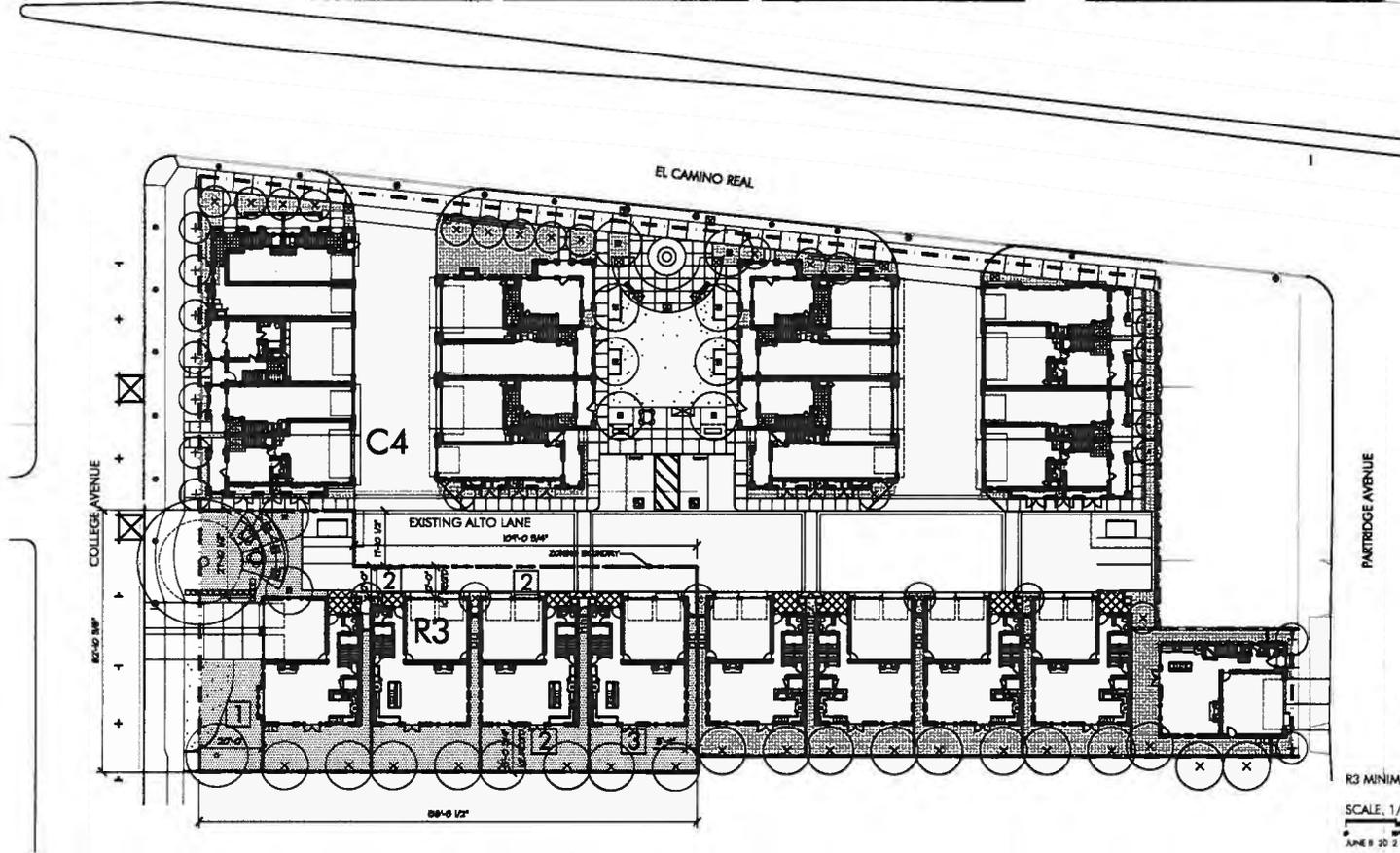
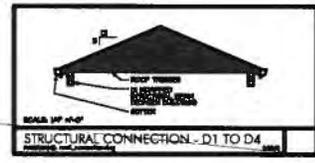
389 EL CAMINO, MENLO PARK, CA
MATTESON DEVELOPMENT PARTNERS, INC.

3445 OWENS DRIVE
PLASANTON, CA 94588
925.251.7900
925.251.7901 Fax

A1.5a

R3 MINIMUM SETBACK COMPLIANCE

REQUIRED: 1 FRONT SETBACK: 20' 2 SIDE YARD SETBACKS: 10' 3 REAR SETBACK: 15'	PROVIDED: 1 FRONT SETBACK: 20' 2 SIDE YARD SETBACKS: 10' TO 15'-4" 3 REAR SETBACK: 3'-4" (WAVES REQUIRED)	NOTE: ALL FOUR RESIDENCES IN THE R3 ARE STRUCTURALLY CONNECTED. (BUILDINGS D1, D2, D3 & D4). THIS STRUCTURAL CONNECTION BETWEEN THESE RESIDENCES ELIMINATES THE 20' REQUIRED DISTANCE BETWEEN STAND ALONE STRUCTURES IN THE
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389 EL CAMINO, MENLO PARK, CA
 MATTESON DEVELOPMENT PARTNERS, INC.

R3 MINIMUM SETBACK COMPLIANCE
 SCALE: 1/16" = 1'-0"
 JUNE 8 2012 PROJECT NO: 221.009
 DAHLIN GROUP

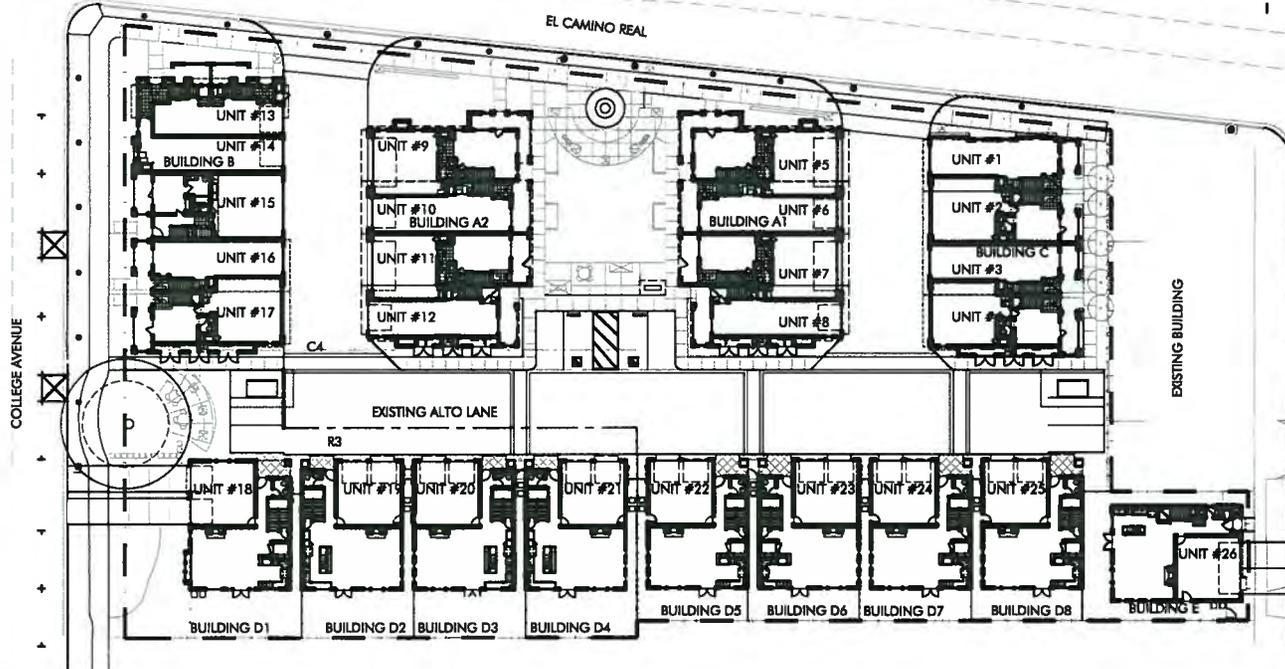
3842 OWING DRIVE
 PLEASANTON, CA 94588
 925.251.7200
 925.251.7201 Fax

A1.5b

AGGREGATE FLOOR AREA TABLE

Building	Area	Volume	Height	Notes
Building A1	1,200	12,000	10	
Building A2	1,200	12,000	10	
Building B	1,200	12,000	10	
Building C	1,200	12,000	10	
Building D1	1,200	12,000	10	
Building D2	1,200	12,000	10	
Building D3	1,200	12,000	10	
Building D4	1,200	12,000	10	
Building D5	1,200	12,000	10	
Building D6	1,200	12,000	10	
Building D7	1,200	12,000	10	
Building D8	1,200	12,000	10	
Building E	1,200	12,000	10	
TOTAL	12,000	120,000	10	

Building	Area	Volume	Height	Notes
Building A1	1,200	12,000	10	
Building A2	1,200	12,000	10	
Building B	1,200	12,000	10	
Building C	1,200	12,000	10	
Building D1	1,200	12,000	10	
Building D2	1,200	12,000	10	
Building D3	1,200	12,000	10	
Building D4	1,200	12,000	10	
Building D5	1,200	12,000	10	
Building D6	1,200	12,000	10	
Building D7	1,200	12,000	10	
Building D8	1,200	12,000	10	
Building E	1,200	12,000	10	
TOTAL	12,000	120,000	10	



- 18.04 Gross floor area**
- (A) "Gross floor area" applies to all zoning districts except the single-family residential and R-2 zoning districts and means the sum of the horizontal area of all areas within the surrounding solid units of a building covered by a roof measured to the outside surfaces of exterior walls or portions thereof subject to the clarifications in subsections (B), (C) and (D).
- (B) Gross floor area includes the following features of a building that meet the criteria of subsection (A) unless otherwise specified in subsection (C):
- (1) Areas of a basement with a finished ceiling height of six feet, six inches (6'6") or greater;
 - (2) Mezzanines, including equipment platforms and storage platforms, but excluding vertical shafting shafts and columns;
 - (3) Areas of an atrium with a floor to ceiling height of six feet, six inches (6'6") or greater;
 - (4) Equipment and utility areas containing mechanical equipment, electrical panels, meters, control boxes, switch boxes;
 - (5) Storage areas;
 - (6) Bay windows and similar projections or cantilevered areas; and
 - (7) Eaves, overhangs, and awnings.
- (C) Gross floor area excludes the following features of a building that meet the criteria of subsection (A):
- (1) Areas of a building or buildings that are designed as nonresidential or nonoccupiable spaces with unfinished walls, floors and ceilings, unless the use permitted (2)(b) of the maximum allowed gross floor area of the lot. To qualify for this exclusion, such spaces must have less than (2) or more of the following characteristics: a floor to ceiling height that is less than six feet, six inches (6'6"); limited access (i.e., the absence of the necessary physical space to provide a building code-compliant exit in door); unconditioned air (i.e., the air is neither heated nor cooled); no windows or skylights; and no enclosure. This exclusion may include areas of a building that would otherwise exceed the one-percent maximum limitation on building volume (C2);
 - (2) Areas of a building or buildings dedicated to the maintenance of non-generating equipment, such as building mechanical equipment and generators, not to exceed one percent (1%) of the maximum allowed gross floor area of the lot. This restriction applies to equipment utilized for the operation of the building systems and does not apply to equipment utilized in connection with a business operating from a building;
 - (3) All areas devoted to covered parking and related circulation for automobiles and bicycles, including garage ramps, below grade parking structures, and above grade parking structures;
 - (4) Covered porches and covered balconies provided that at least one end is open and unobstructed to the exterior except for columns or posts not more than twelve (12") in width and not more than two feet in height, four inches (4") in height;
 - (5) Vent shafts, such as building mechanical air ducts and chimneys; and
 - (6) Enclosures solely for trash and recycling.

GROSS FLOOR AREA SITE PLAN
SCALE: 1/16" = 1'-0"

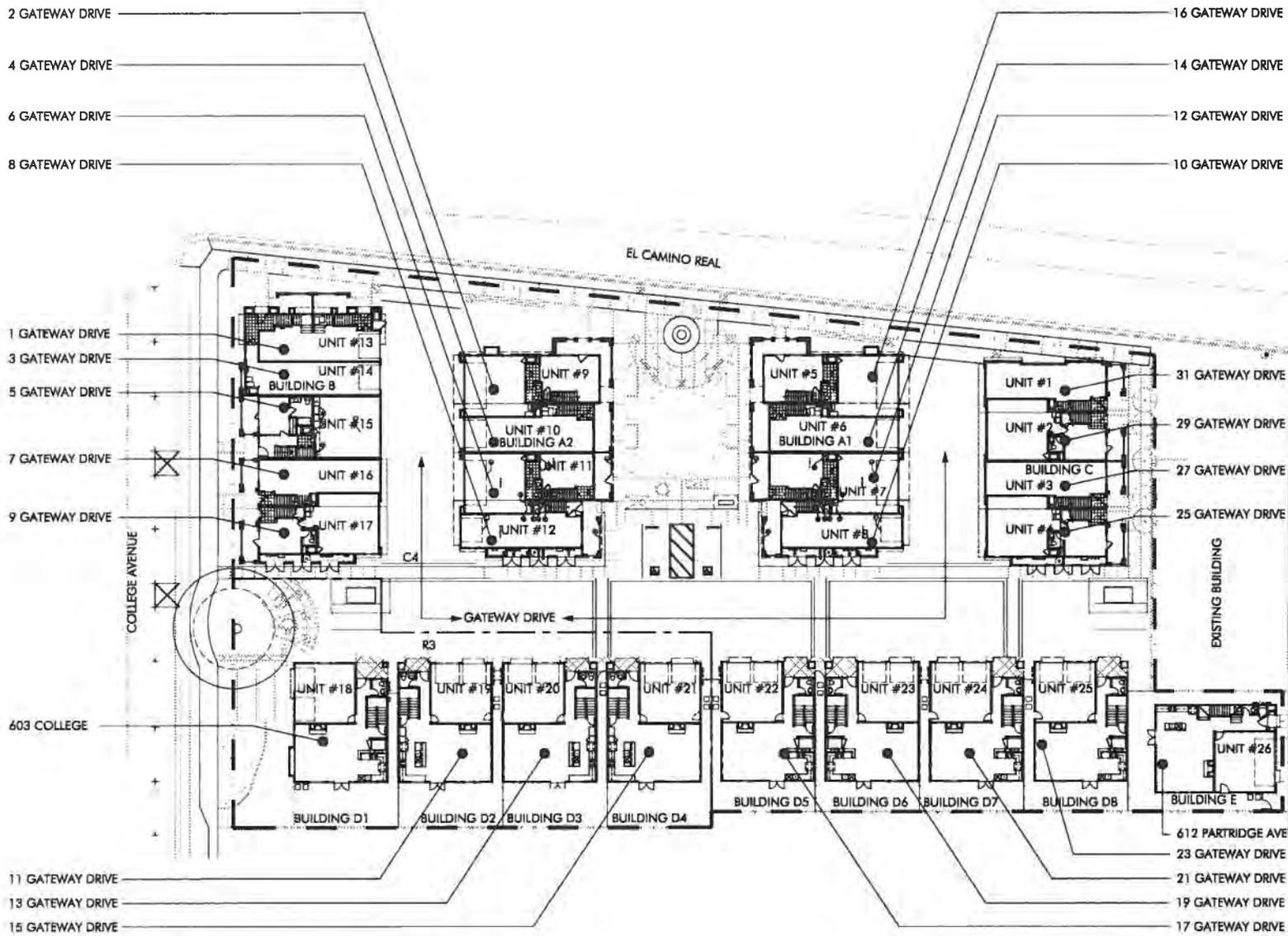
JUNE 8, 2012 PROJECT NO: 221.009

DAHLIN GROUP

389 EL CAMINO, MENLO PARK, CA
MATTESON DEVELOPMENT PARTNERS, INC.

3845 OWENS DRIVE,
PESQUERA, CA 94026
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925.251.7201 Fax

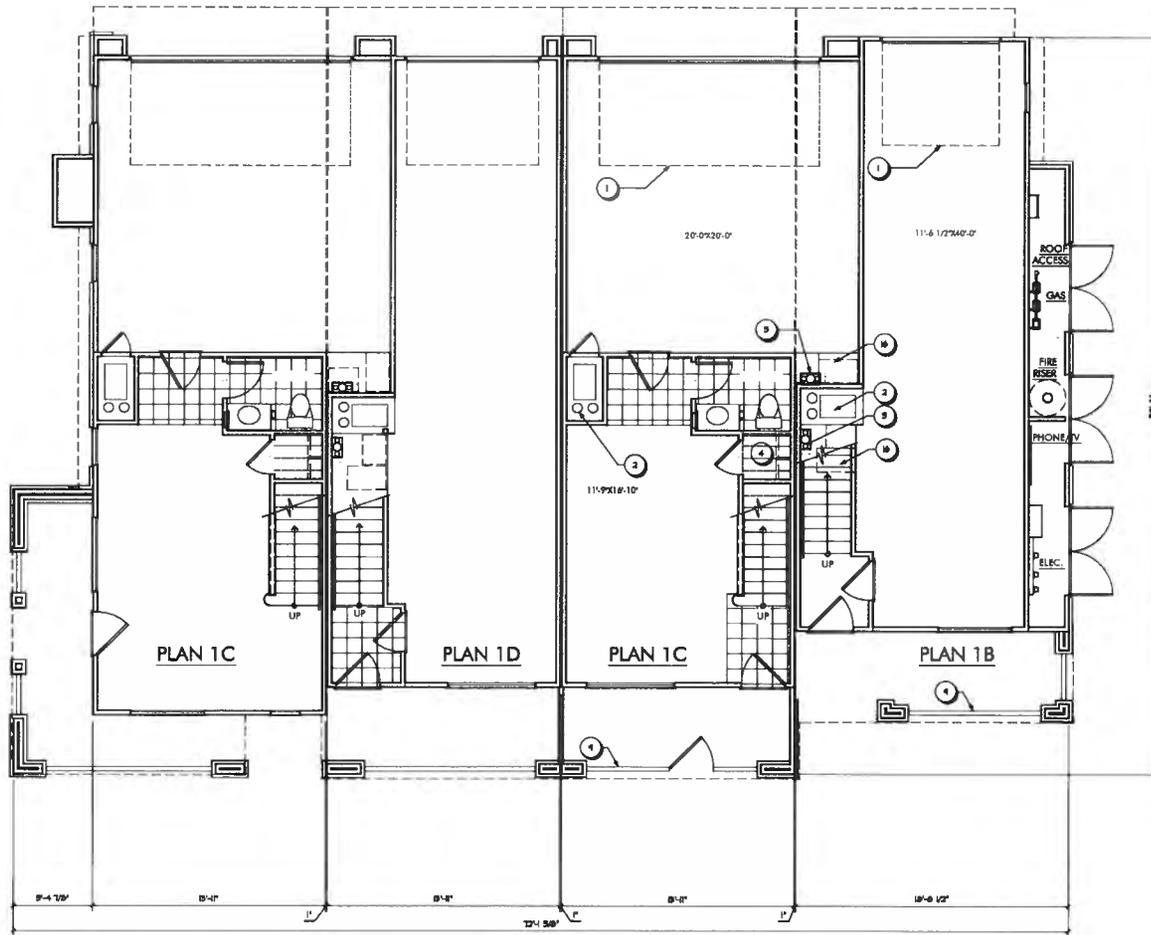
A1.6



389 EL CAMINO, MENLO PARK, CA

MATTESON DEVELOPMENT PARTNERS, INC.

ADDRESS PLAN
SCALE: 1/16" = 1'-0"
JUNE 8, 2012 PROJECT NO: 231.009



FOR FURTHER INFORMATION:
 SEE A3.1a1 FOR PLANS 1A & 1B
 SEE A3.1a2 FOR PLANS 1A & 1B
 SEE A3.1a3 FOR PLANS 1A & 1B
 SEE A3.1c1 FOR PLANS 1C & 1D
 SEE A3.1c2 FOR PLANS 1B, 1C & 1D
 SEE A3.1c3 FOR PLANS 1B & 1C
 SEE A3.2a FOR PLAN 2
 SEE A3.3a FOR PLANS 3A & 3B

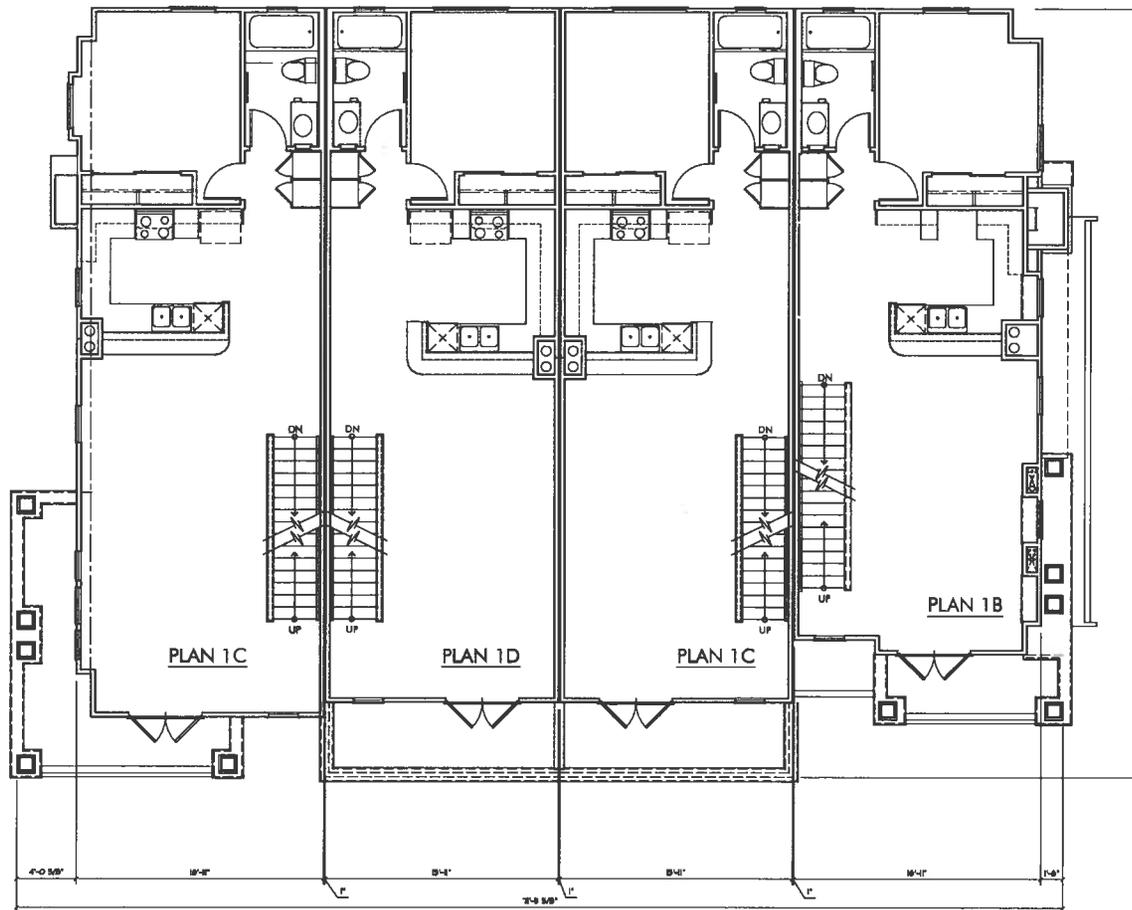
TOWNHOME - BUILDING PLAN
 BUILDING A1, A2 SIM. - FIRST FLOOR
 SCALE: 1/4"=1'-0"

JUNE 8, 2012 PROJECT NO: 221.009



389 EL CAMINO, MENLO PARK, CA

MATTESON DEVELOPMENT PARTNERS, INC.



FOR FURTHER INFORMATION:
 SEE A3.1a1 FOR PLANS 1A & 1B
 SEE A3.1a2 FOR PLANS 1A & 1B
 SEE A3.1a3 FOR PLANS 1A & 1B
 SEE A3.1c1 FOR PLANS 1C & 1D
 SEE A3.1c2 FOR PLANS 1B, 1C & 1D
 SEE A3.1c3 FOR PLANS 1B & 1C
 SEE A3.2a FOR PLAN 2
 SEE A3.3a FOR PLANS 3A & 3B

TOWNHOME - BUILDING PLAN
 BUILDING A1, A2 SIM. - SECOND FLR.
 SCALE: 1/4"=1'-0"

0 4 8 16
 JUNE 8, 2012 PROJECT NO: 221.009

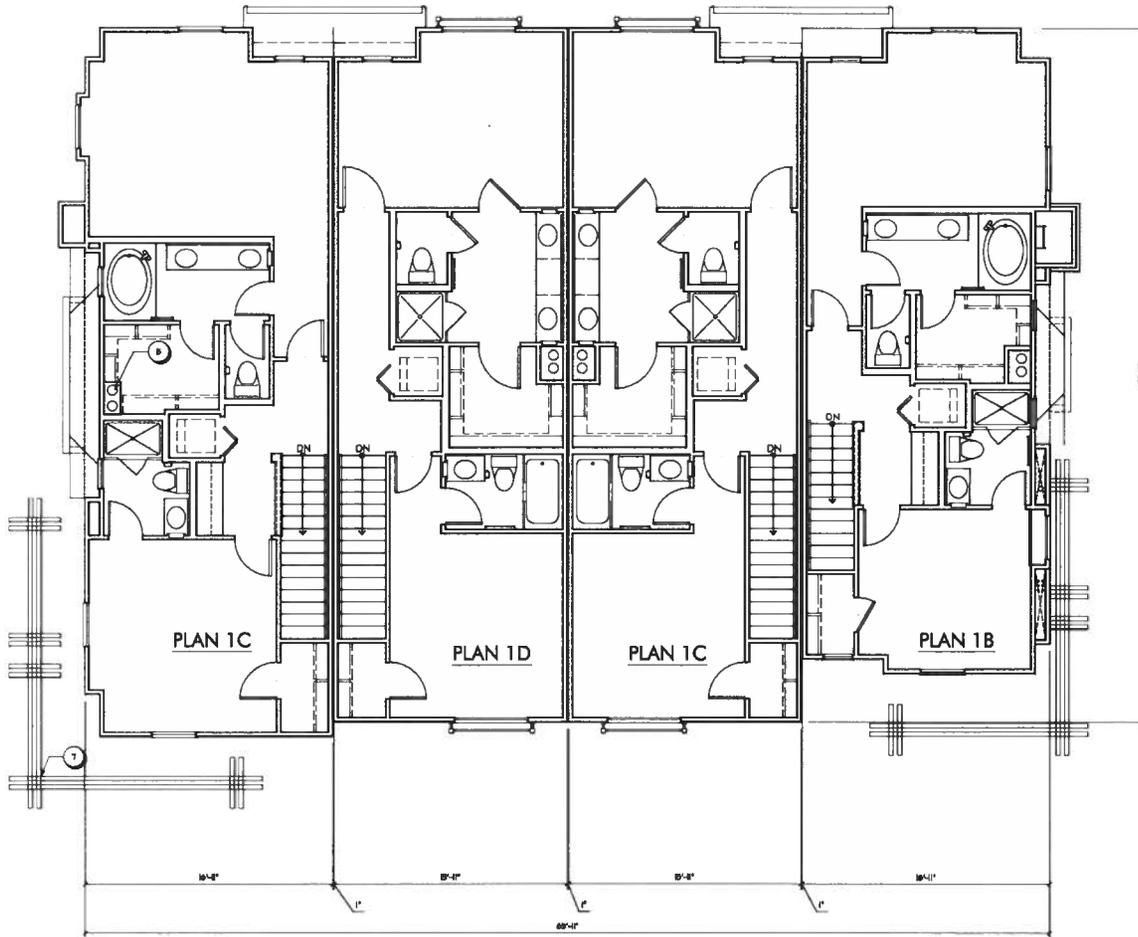


389 EL CAMINO, MENLO PARK, CA

MATTESON DEVELOPMENT PARTNERS, INC.

183
 183 COWS DRIVE
 MENLO PARK, CA 94028
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 925.251.7201 Fax

A2.1b



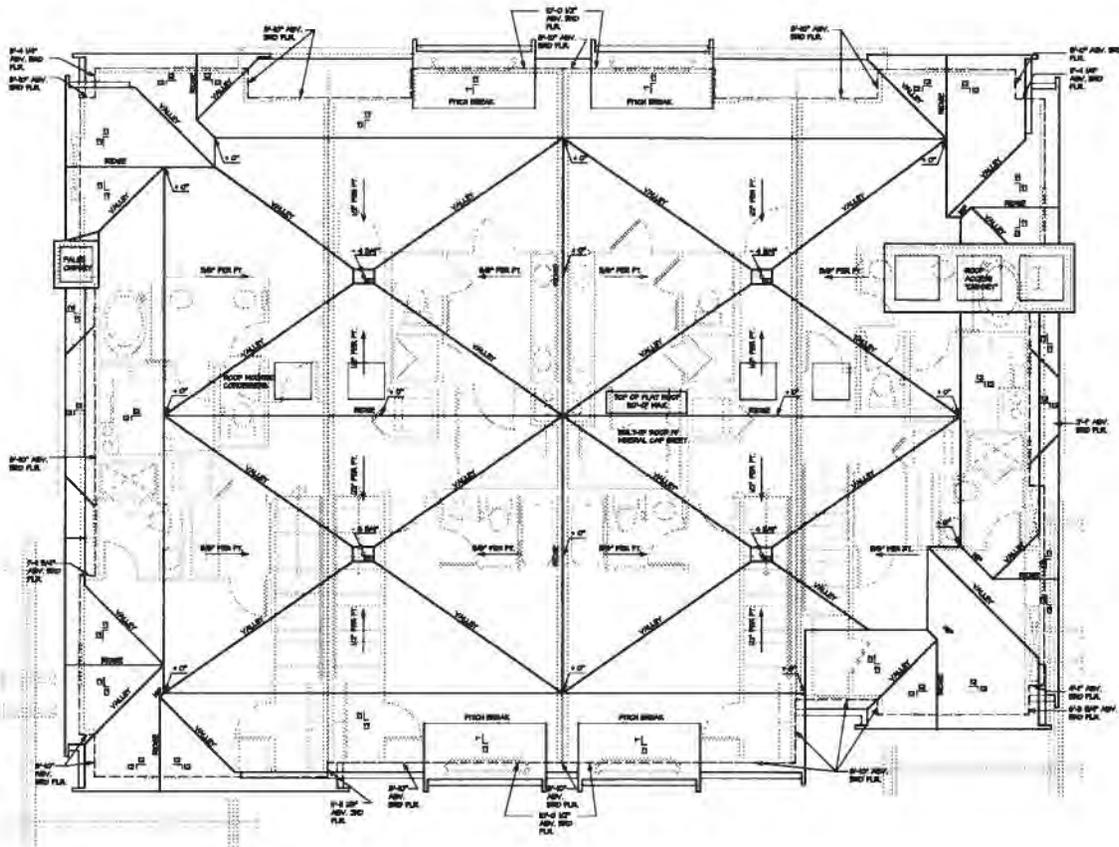
FOR FURTHER INFORMATION:
 SEE A3.1a1 FOR PLANS 1A & 1B
 SEE A3.1a2 FOR PLANS 1A & 1B
 SEE A3.1a3 FOR PLANS 1A & 1B
 SEE A3.1c1 FOR PLANS 1C & 1D
 SEE A3.1c2 FOR PLANS 1B, 1C & 1D
 SEE A3.1c3 FOR PLANS 1B & 1C
 SEE A3.2a FOR PLAN 2
 SEE A3.3a FOR PLANS 3A & 3B

TOWNHOME - BUILDING PLAN
 BUILDING A1, A2 SIM. - THIRD FLOOR
 SCALE: 1/4" = 1'-0"
 JUNE 8, 2012 PROJECT NO: 221.009



389 EL CAMINO, MENLO PARK, CA

MATTESON DEVELOPMENT PARTNERS, INC.



FOR FURTHER INFORMATION:
 SEE A3.1a1 FOR PLANS 1A & 1B
 SEE A3.1a2 FOR PLANS 1A & 1B
 SEE A3.1a3 FOR PLANS 1A & 1B
 SEE A3.1c1 FOR PLANS 1C & 1D
 SEE A3.1c2 FOR PLANS 1B, 1C & 1D
 SEE A3.1c3 FOR PLANS 1B & 1C
 SEE A3.2a FOR PLAN 2
 SEE A3.3a FOR PLANS 3A & 3B

TOWNHOME - BUILDING PLAN
 BUILDING A1, A2 SIM. - ROOF PLAN
 SCALE: 1/4" = 1'-0"

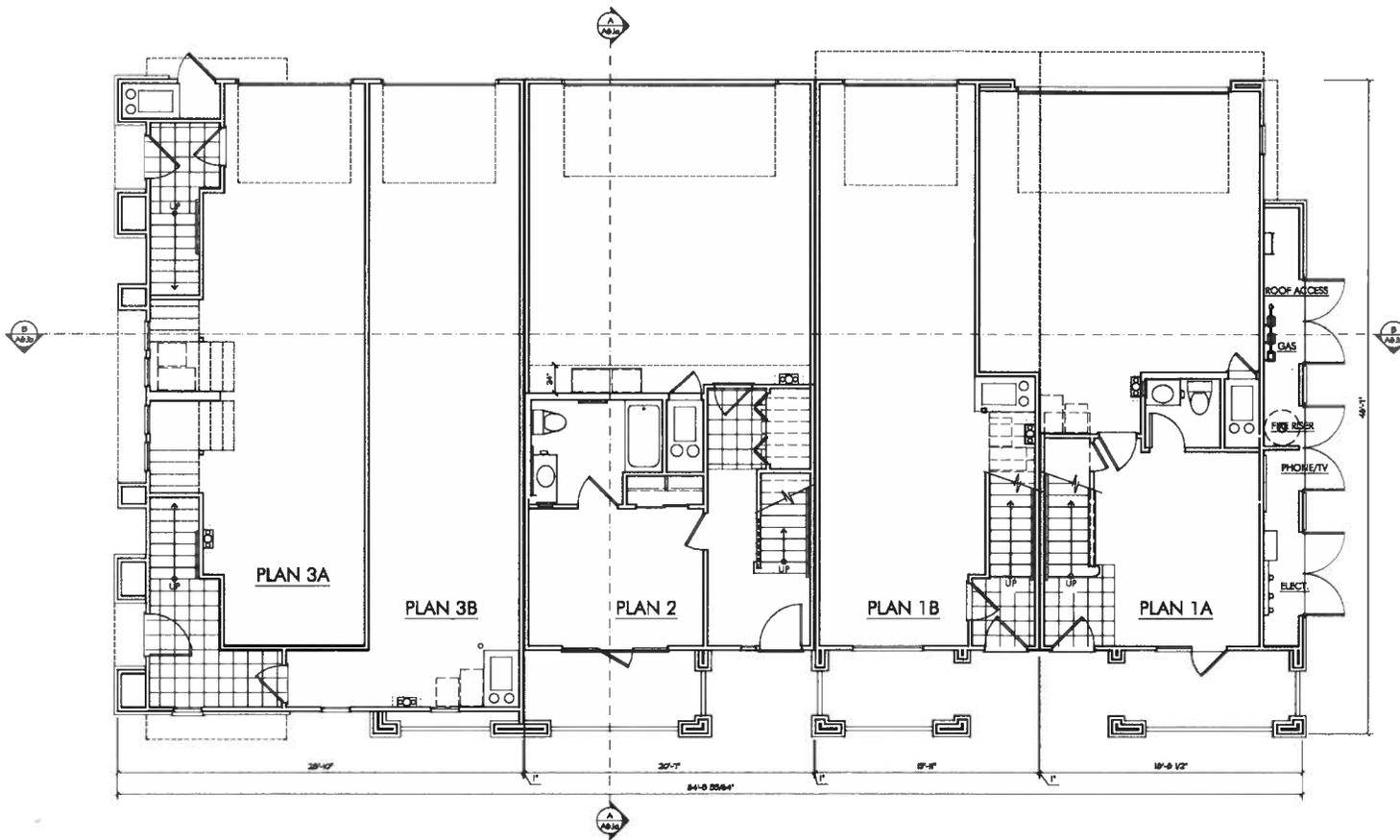
JUNE 8, 2012 PROJECT NO: 221 009



389 EL CAMINO, MENLO PARK, CA
 MATTESON DEVELOPMENT PARTNERS, INC.

5865 OWENS DRIVE,
 MENLO PARK, CA 94568
 TEL: 650 320 0000
 FAX: 650 320 0000

185 A2.1d



FOR FURTHER INFORMATION:
 SEE A3.1a1 FOR PLANS 1A & 1B
 SEE A3.1a2 FOR PLANS 1A & 1B
 SEE A3.1a3 FOR PLANS 1A & 1B
 SEE A3.1c1 FOR PLANS 1C & 1D
 SEE A3.1c2 FOR PLANS 1B, 1C & 1D
 SEE A3.1c3 FOR PLANS 1B & 1C
 SEE A3.2a FOR PLAN 2
 SEE A3.3a FOR PLANS 3A & 3B

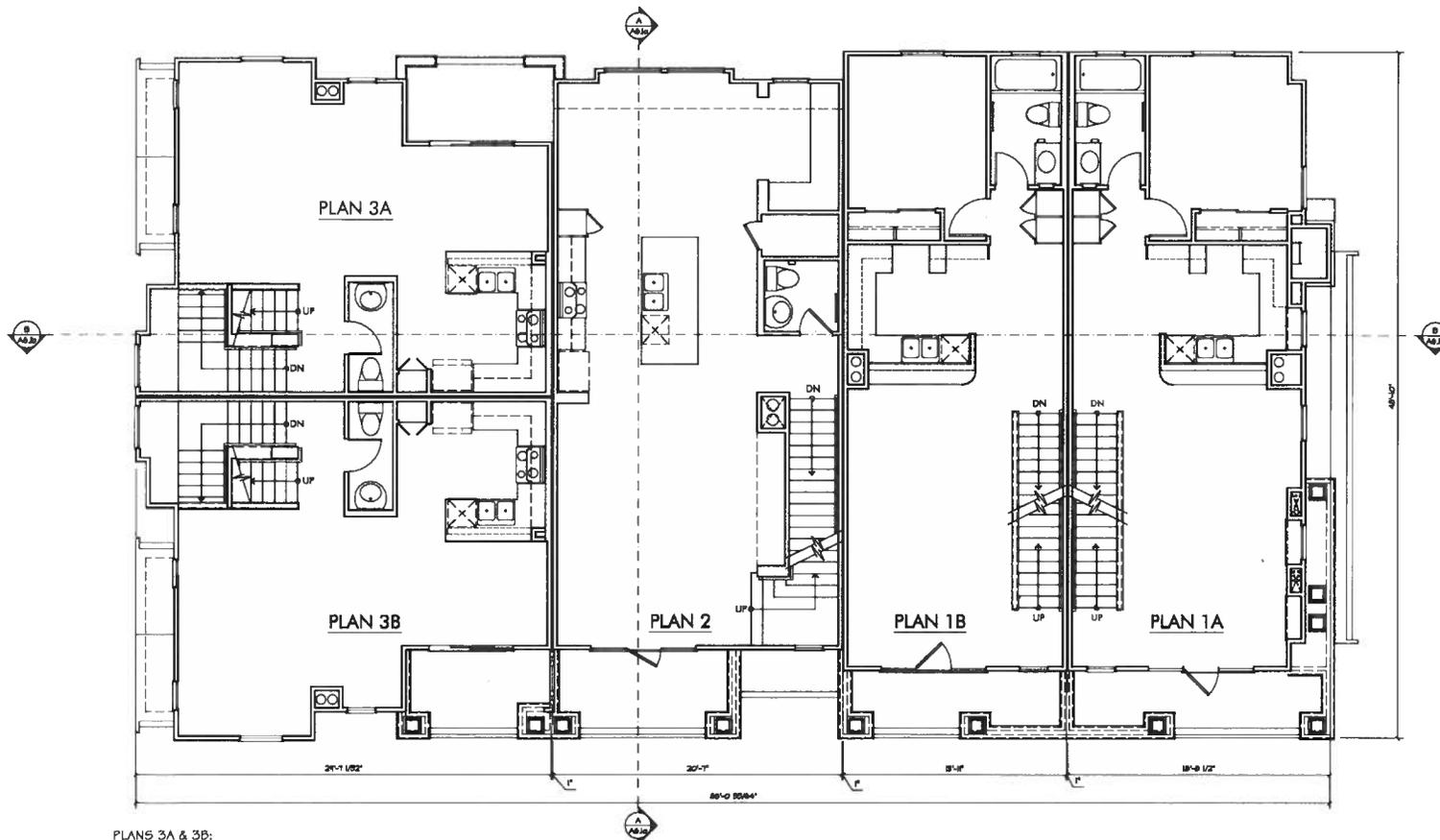
TOWNHOME - BUILDING PLAN
 BUILDING B - FIRST FLOOR
 SCALE: 1/4"=1'-0"

JUNE 8, 2012 PROJECT NO: 221.009



389 EL CAMINO, MENLO PARK, CA

MATTESON DEVELOPMENT PARTNERS, INC.



PLANS 3A & 3B:
GROSS AREA: 589 SQFT

FOR FURTHER INFORMATION:
 SEE A3.1a1 FOR PLANS 1A & 1B
 SEE A3.1a2 FOR PLANS 1A & 1B
 SEE A3.1a3 FOR PLANS 1A & 1B
 SEE A3.1c1 FOR PLANS 1C & 1D
 SEE A3.1c2 FOR PLANS 1B, 1C & 1D
 SEE A3.1c3 FOR PLANS 1B & 1C
 SEE A3.2a FOR PLAN 2
 SEE A3.3a FOR PLANS 3A & 3B

TOWNHOME - BUILDING PLAN
 BUILDING B - SECOND FLOOR
 SCALE: 1/4"=1'-0"

JUNE 8, 2012 PROJECT NO: 221.009



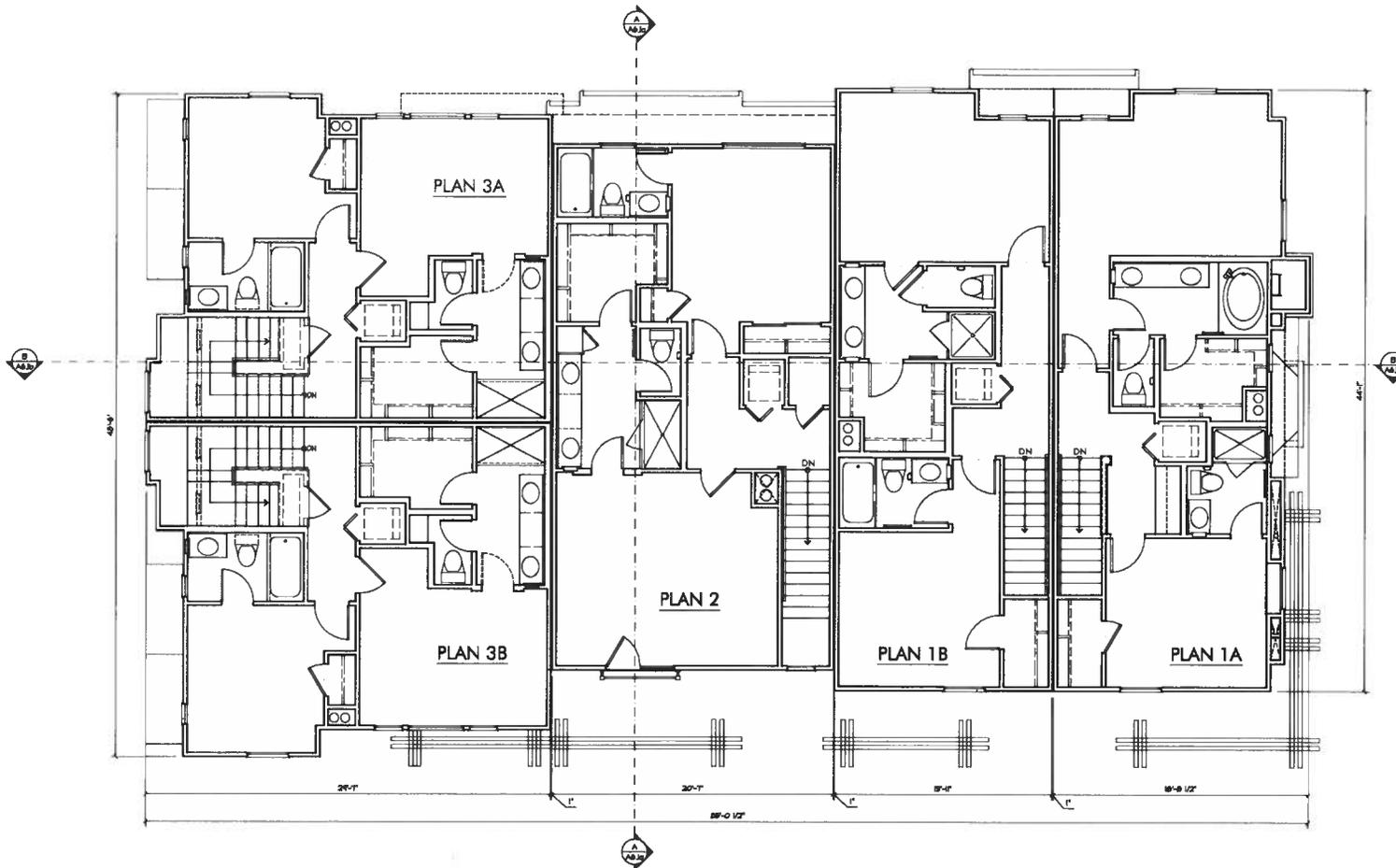
389 EL CAMINO, MENLO PARK, CA

MATTESON DEVELOPMENT PARTNERS, INC.

5665 OWENS DRIVE,
 FLEMING, CA 94550
 925.751.7201 Fax

187

A2.2b



FOR FURTHER INFORMATION:
 SEE A3.1a1 FOR PLANS 1A & 1B
 SEE A3.1a2 FOR PLANS 1A & 1B
 SEE A3.1a3 FOR PLANS 1A & 1B
 SEE A3.1c1 FOR PLANS 1C & 1D
 SEE A3.1c2 FOR PLANS 1B, 1C & 1D
 SEE A3.1c3 FOR PLANS 1B & 1C
 SEE A3.2a FOR PLAN 2
 SEE A3.3a FOR PLANS 3A & 3B

TOWNHOME - BUILDING PLAN
 BUILDING B - THIRD FLOOR
 SCALE: 1/4" = 1'-0"

JUNE 8, 2012 PROJECT NO: 221.009

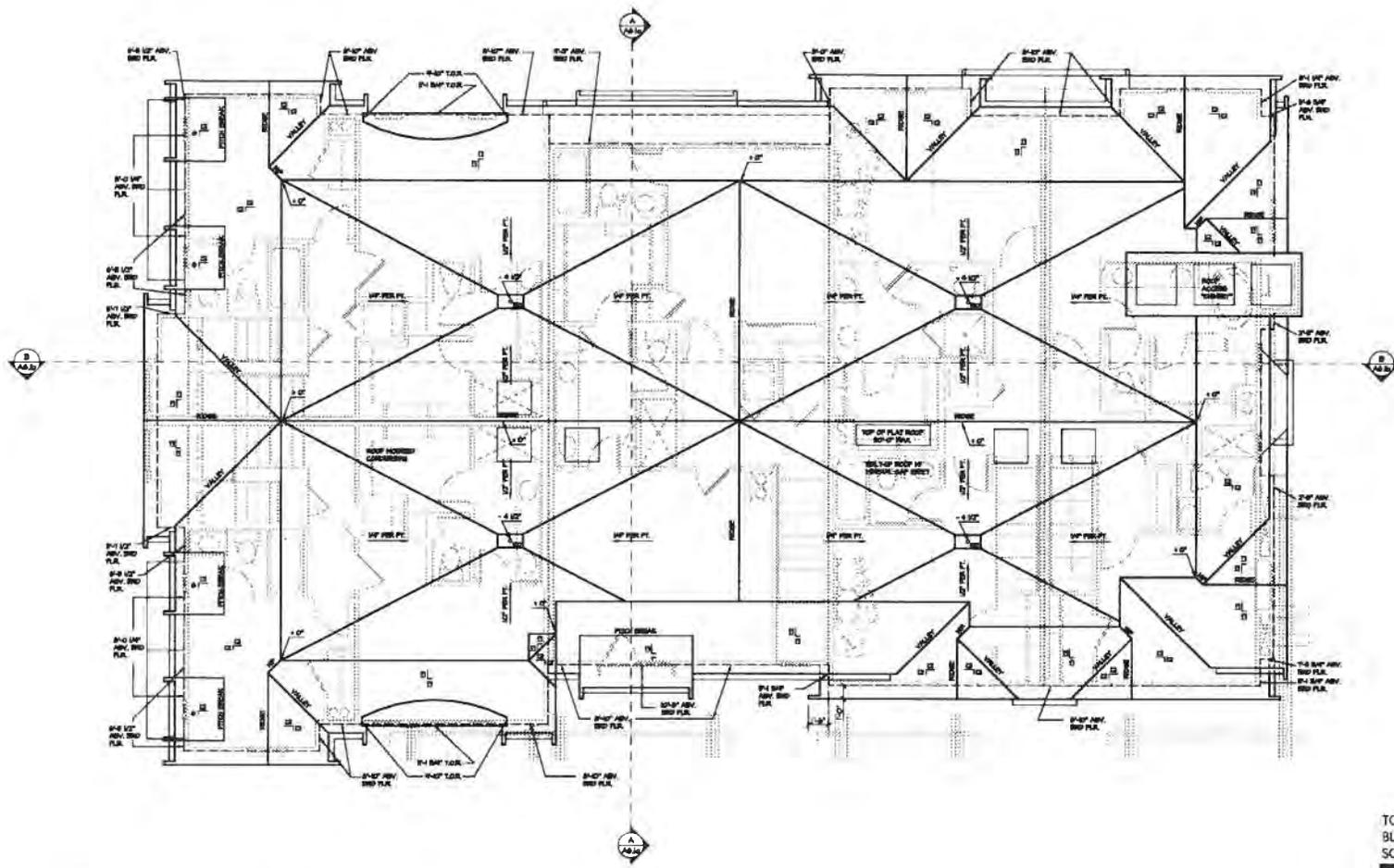


389 EL CAMINO, MENLO PARK, CA

MATTESON DEVELOPMENT PARTNERS, INC.

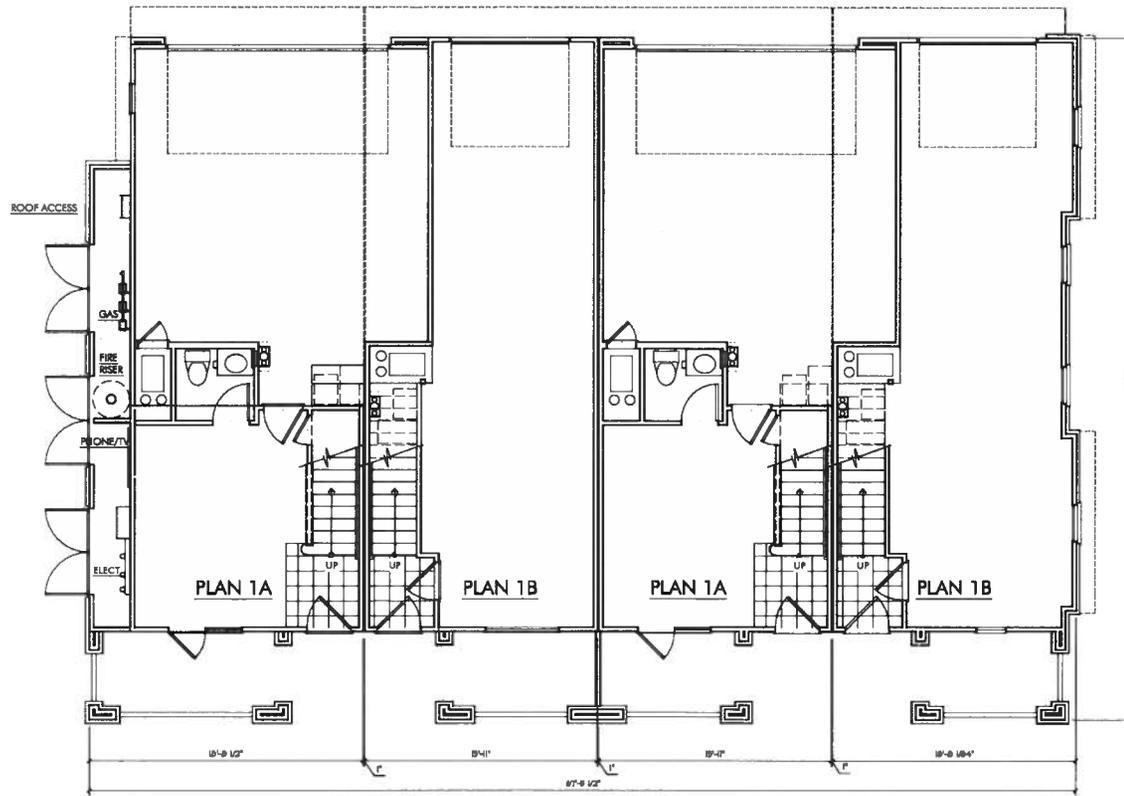
5665 OWENS DRIVE,
 PLEASANTON, CA 94550
 925.351.7200
 925.351.7201 Fax

A2.2c



TOWNHOME - BUILDING PLAN
 BUILDING B - ROOF PLAN
 SCALE: 1/4" = 1'-0"
 0 4 8
 JUNE 8, 2012 PROJECT NO: 221.009
 URBAN DESIGN

389 EL CAMINO, MENLO PARK, CA
 MATTESON DEVELOPMENT PARTNERS, INC.



FOR FURTHER INFORMATION:
 SEE A3.1a1 FOR PLANS 1A & 1B
 SEE A3.1a2 FOR PLANS 1A & 1B
 SEE A3.1a3 FOR PLANS 1A & 1B
 SEE A3.1c1 FOR PLANS 1C & 1D
 SEE A3.1c2 FOR PLANS 1B, 1C & 1D
 SEE A3.1c3 FOR PLANS 1B & 1C
 SEE A3.2a FOR PLAN 2
 SEE A3.3a FOR PLANS 3A & 3B

TOWNHOME - BUILDING PLAN
 BUILDING C - FIRST FLOOR
 SCALE: 1/4" = 1'-0"

JUNE 8, 2012 PROJECT NO: 221.009

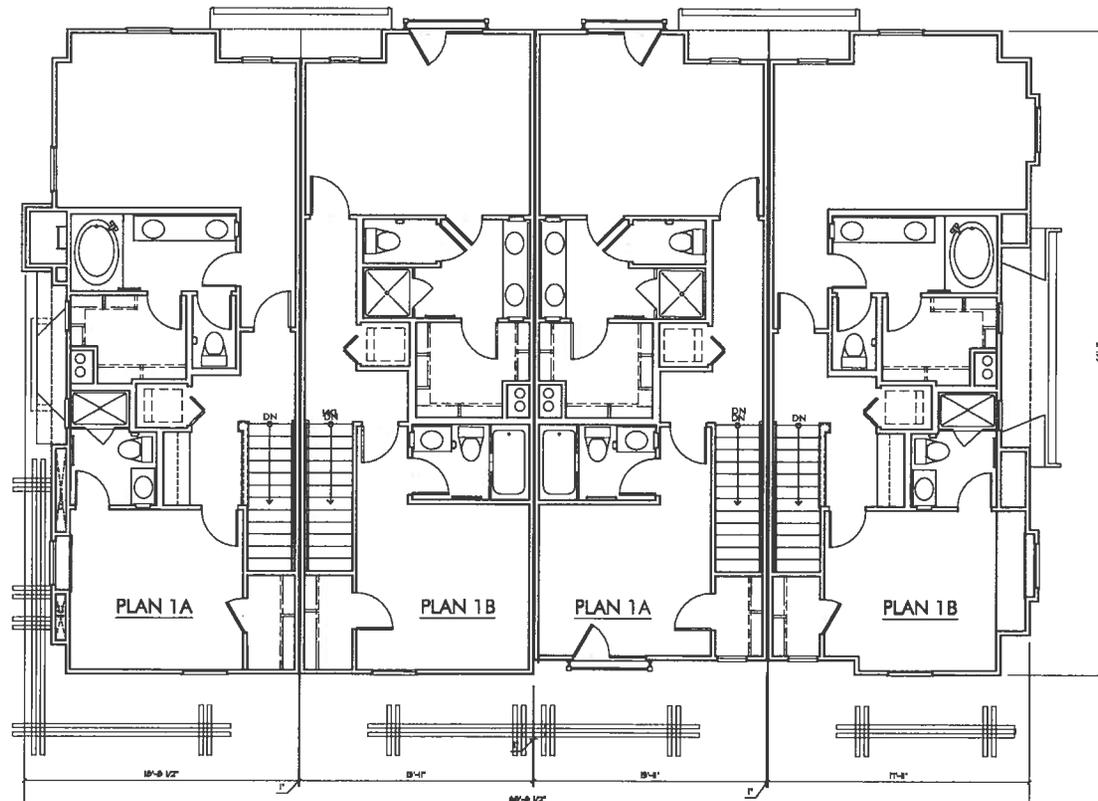


389 EL CAMINO, MENLO PARK, CA

MATTESON DEVELOPMENT PARTNERS, INC.

5865 OWENS DRIVE,
 PLEASANTON, CA 94588
 925.251.7300
 925.251.7201 Fax

A2.3a



FOR FURTHER INFORMATION:
 SEE A3.1a1 FOR PLANS 1A & 1B
 SEE A3.1a2 FOR PLANS 1A & 1B
 SEE A3.1a3 FOR PLANS 1A & 1B
 SEE A3.1c1 FOR PLANS 1C & 1D
 SEE A3.1c2 FOR PLANS 1B, 1C & 1D
 SEE A3.1c3 FOR PLANS 1B & 1C
 SEE A3.2a FOR PLAN 2
 SEE A3.3a FOR PLANS 3A & 3B

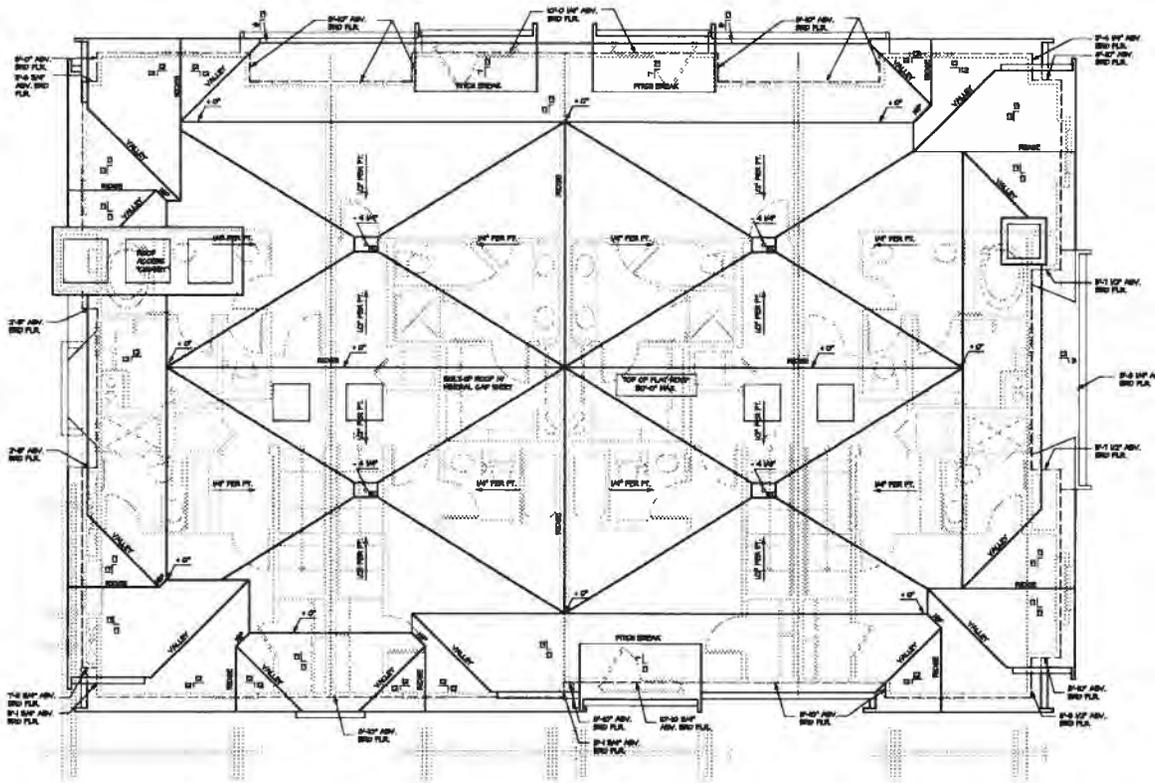
TOWNHOME - BUILDING PLAN
 BUILDING C - THIRD FLOOR
 SCALE: 1/4" = 1'-0"

JUNE 8, 2012 PROJECT NO: 221.009



389 EL CAMINO, MENLO PARK, CA

MATTESON DEVELOPMENT PARTNERS, INC.



TOWNHOME - BUILDING PLAN
 BUILDING C - ROOF PLAN
 SCALE: 1/4"=1'-0"
 JUNE 8, 2012 PROJECT NO: 221.009

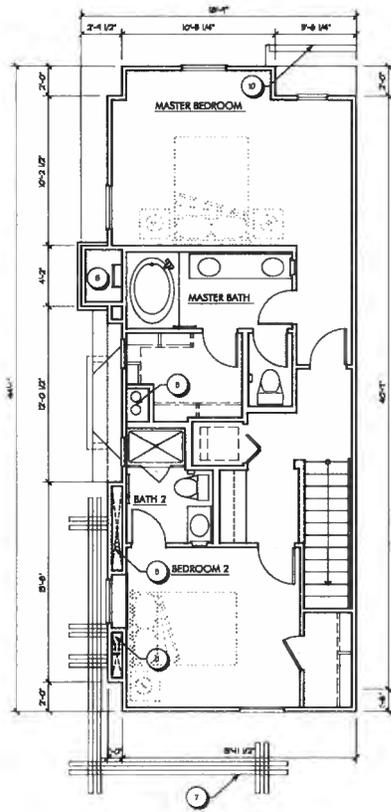
389 EL CAMINO, MENLO PARK, CA

MATTESON DEVELOPMENT PARTNERS, INC.

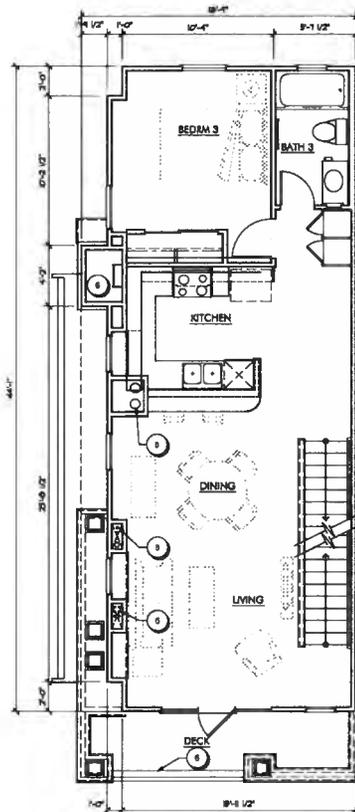


5865 OWENS DRIVE,
 FOLSOM, CA 95660
 TEL: 916.439.0000
 FAX: 916.439.7101

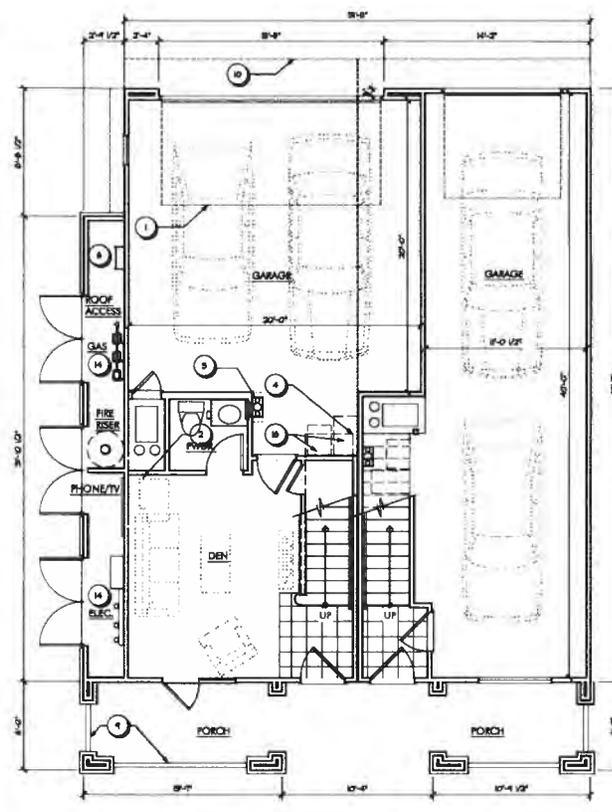
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PLAN 1A - UNITS 4, 17
THIRD FLOOR



PLAN 1A - UNITS 4, 17
SECOND FLOOR



PLAN 1A - UNITS 4, 17
FIRST FLOOR

PLAN 1B - UNITS 3, 16
FIRST FLOOR
SEE A3.1a2

- FLOOR PLAN KEY NOTES: ①
1. Overhead garage door above
 2. Furnace & ducts
 3. Tank-less water heater
 4. Line of stair above
 5. Duct space
 6. Roof access ladder
 7. Trellis- see elevation
 8. Guardrail
 9. Decorative railing
 10. Lines of floor above/ below
 11. Optional elevator/ storage
 12. Attic access
 13. Attic furnace
 14. Meter locations
 15. Fireplace
 16. Trash / recycling containers

TOWNHOME - UNIT PLANS
PLAN 1A FLOOR PLAN
SCALE: 1/4"=1'-0"

JUNE 8, 2012 PROJECT NO: 221.009

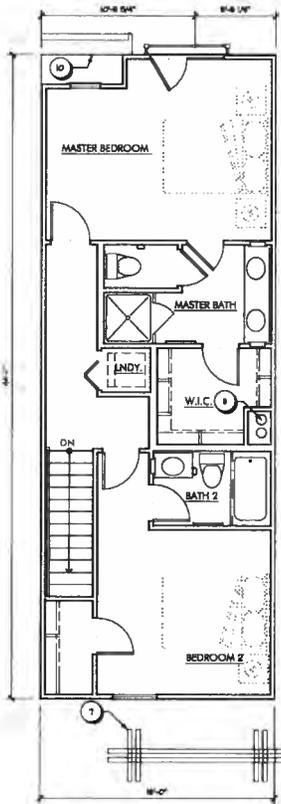


389 EL CAMINO, MENLO PARK, CA

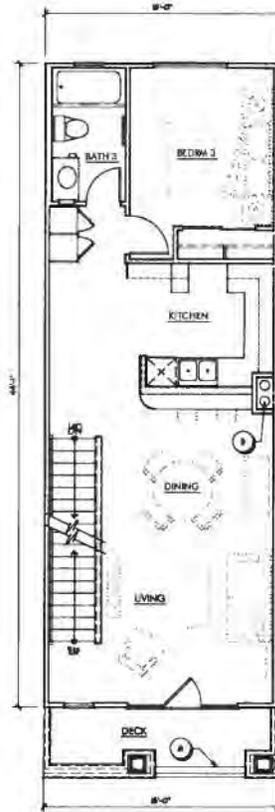
MATTESON DEVELOPMENT PARTNERS, INC.

SR&S OWING DEVL.
PLEASANTON, CA 94588
925.251.7200
925.251.7201 Fax

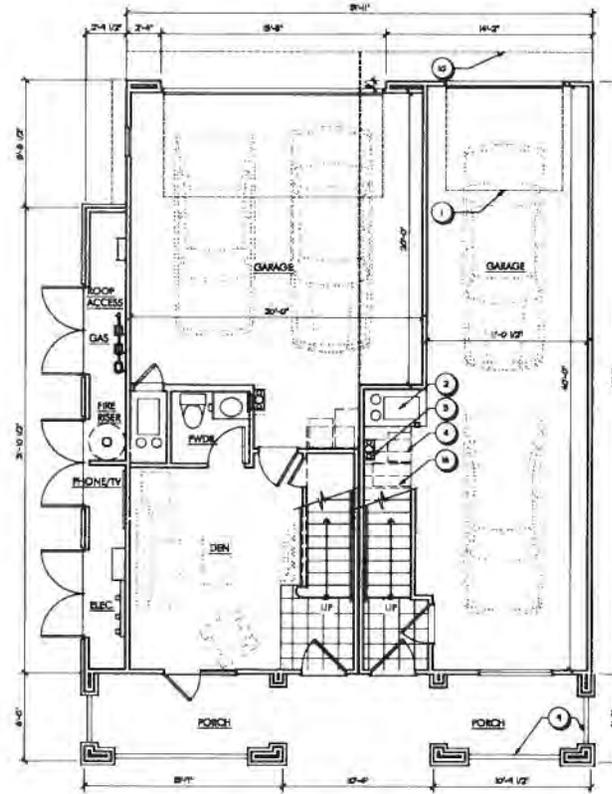
A3.1a1



PLAN 1B - UNITS 3, 16
 PLAN 1A SIM. - UNIT 2
 THIRD FLOOR



PLAN 1B - UNITS 3, 16
 PLAN 1A SIM. - UNIT 2
 SECOND FLOOR



PLAN 1A - UNITS 4, 17
 FIRST FLOOR
 SEE A3.1a1

PLAN 1B - UNITS 3, 16
 FIRST FLOOR

- FLOOR PLAN KEY NOTES:**
1. Overhead garage door above
 2. Furnace & ducts
 3. Tank-less water heater
 4. Line of stair above
 5. Duct space
 6. Roof access ladder
 7. Trellis - see elevation
 8. Guardrail
 9. Decorative railing
 10. Lines of floor above/below
 11. Optional elevator/storage
 12. Attic access
 13. Attic furnace
 14. Meter locations
 15. Fireplace
 16. Trash / recycling containers

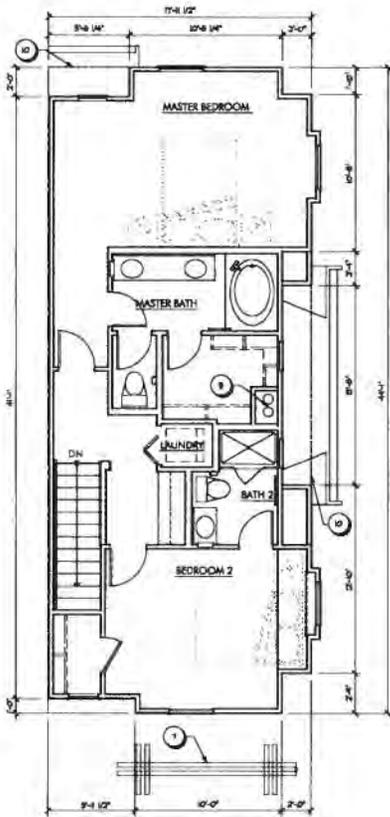
TOWNHOME - UNIT PLANS
 PLAN 1B - FLOOR PLAN
 SCALE: 1/4"=1'-0"

JUNE 8, 2012 PROJECT NO. 221,009

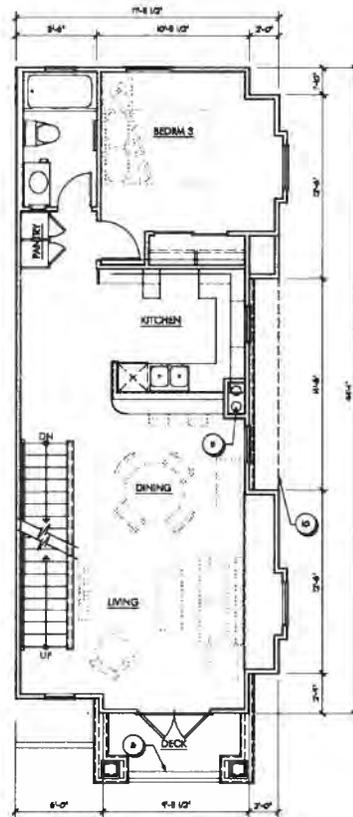


389 EL CAMINO, MENLO PARK, CA

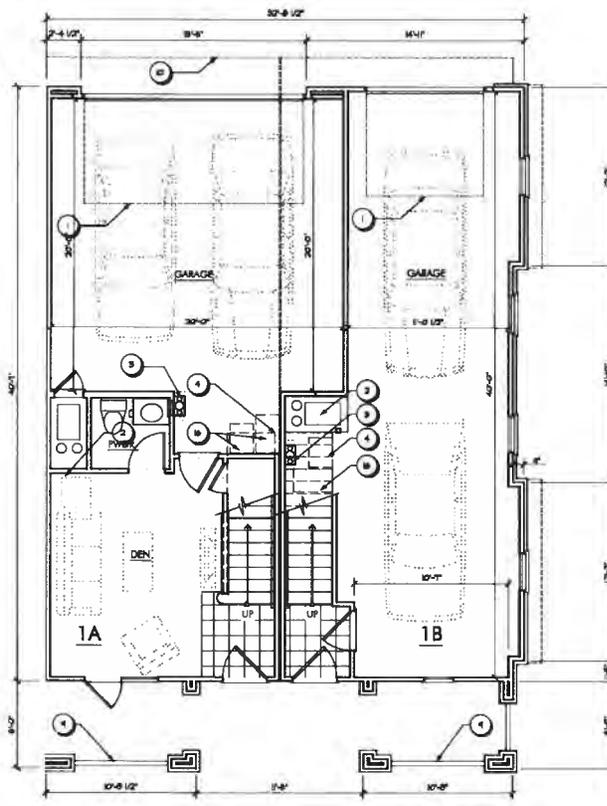
MATTESON DEVELOPMENT PARTNERS, INC.



PLAN 1B - UNIT 1
THIRD FLOOR



PLAN 1B - UNIT 1
SECOND FLOOR



PLAN 1A - UNIT 2
FIRST FLOOR

PLAN 1B - UNIT 1
FIRST FLOOR

- FLOOR PLAN KEY NOTES:** ①
1. Overhead garage door above
 2. Furnace & ducts
 3. Tank-less water heater
 4. Line of stair above
 5. Duct space
 6. Roof access ladder
 7. Trellis- see elevation
 8. Guardrail
 9. Decorative railing
 10. Lines of floor above/ below
 11. Optional elevator/ storage
 12. Attic access
 13. Attic furnace
 14. Meter locations
 15. Fireplace
 16. Trash / recycling containers

TOWNHOME - UNIT PLANS
PLAN 1A & 1B - FLOOR PLAN
SCALE: 1/4"=1'-0"

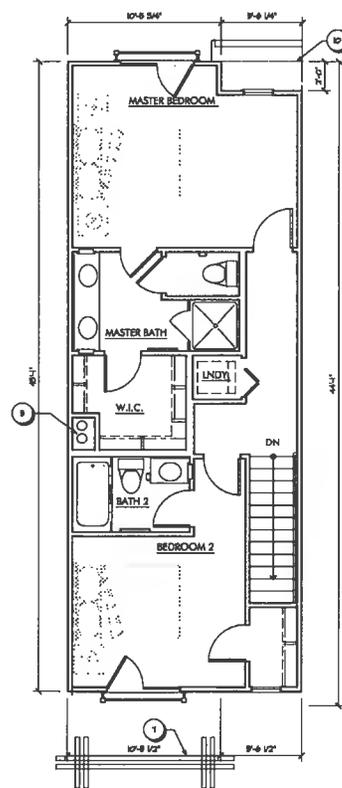
JUNE 8, 2012 PROJECT NO.: 221.009

389 EL CAMINO, MENLO PARK, CA

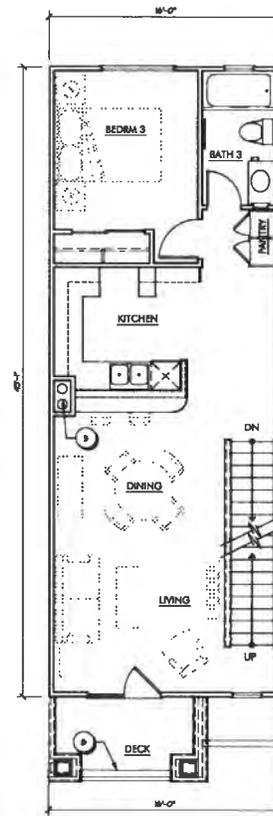
MATTESON DEVELOPMENT PARTNERS, INC.

5565 OWENS DRIVE,
PILASANTO, CA 94509
925.251.7200
925.251.7201 Fax

A3.1a3



PLAN 1A - UNIT 2
THIRD FLOOR



PLAN 1A - UNIT 2
SECOND FLOOR

- FLOOR PLAN KEY NOTES: ①
1. Overhead garage door above
 2. Furnace & ducts
 3. Tank-less water heater
 4. Line of stair above
 5. Duct space
 6. Roof access ladder
 7. Trellis- see elevation
 8. Guardrail
 9. Decorative railing
 10. Lines of floor above/ below
 11. Optional elevator/ storage
 12. Attic access
 13. Attic furnace
 14. Meter locations
 15. Fireplace
 16. Trash / recycling containers

TOWNHOME - UNIT PLANS
PLAN 1A - FLOOR PLAN
SCALE: 1/4"=1'-0"

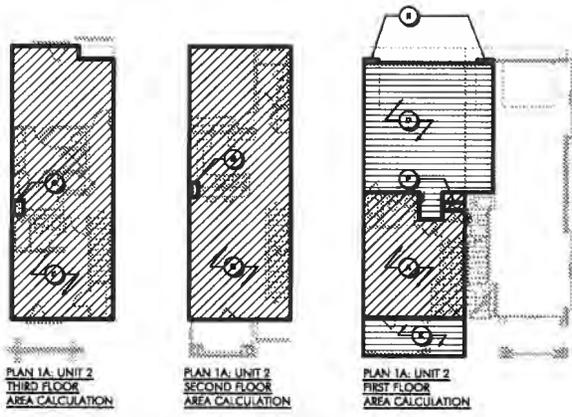
JUNE 8, 2012 PROJECT NO: 221.009

389 EL CAMINO, MENLO PARK, CA

MATTESON DEVELOPMENT PARTNERS, INC.

10
389 EL CAMINO DRIVE,
MENLO PARK, CA 94028
925.251.7201 Fax

A3.1a4



PLAN 1A - UNIT 2

FIRST FLOOR

A. FIRST FLOOR GROSS AREA: 427 SQFT

D. GARAGE AREA AREA: 289 SQFT

F. TRASH AREA INC IN GFA: 2 SQFT

H. EXCLUDED AREA:

SECOND FLOOR

B. SECOND FLOOR GROSS AREA: 5 SQFT

G. VENT SHAFT AREA: 883 SQFT

THIRD FLOOR

C. THIRD FLOOR GROSS AREA: 3 SQFT

G. VENT SHAFT AREA: 877 SQFT

UNIT TOTAL GROSS FLOOR AREA: 1,285 SQFT

BUILDING COVERAGE: PLAN 1A - UNIT 2

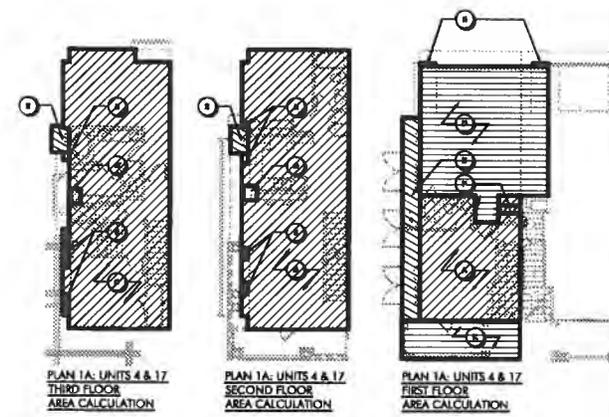
A. FIRST FLOOR GROSS AREA: 427 SQFT

D. GARAGE AREA AREA: 289 SQFT

H. EXCLUDED AREA: 2 SQFT

K. EXTERIOR BUILDING COVERAGE: 88 SQFT

BUILDING COVERAGE AREA: 808 SQFT



GFA: PLAN 1A - UNITS 4, 17

FIRST FLOOR

A. FIRST FLOOR GROSS AREA: 427 SQFT

D. GARAGE AREA AREA: 289 SQFT

F. TRASH AREA INC IN GFA: 2 SQFT

H. EXCLUDED AREA: 88 SQFT

E. UTILITY AREA: 88 SQFT

(ADDED TO BUILDING SQFT)

SECOND FLOOR

B. SECOND FLOOR GROSS AREA: 5 SQFT

G. VENT SHAFT AREA: 788 SQFT

H. EXCLUDED AREA: 5 SQFT

E. UTILITY AREA: 12 SQFT

THIRD FLOOR

C. THIRD FLOOR GROSS AREA: 3 SQFT

G. VENT SHAFT AREA: 708 SQFT

H. EXCLUDED AREA: 3 SQFT

E. UTILITY AREA: 5 SQFT

UNIT TOTAL GROSS FLOOR AREA: 1,721 SQFT

BUILDING COVERAGE: PLAN 1A - UNITS 4, 17

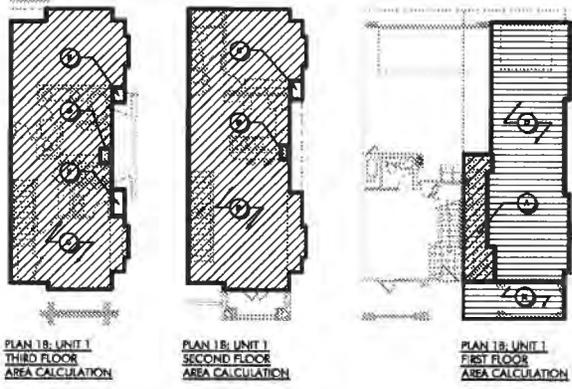
A. FIRST FLOOR GROSS AREA: 427 SQFT

D. GARAGE AREA AREA: 289 SQFT

H. EXCLUDED AREA: 2 SQFT

K. EXTERIOR BUILDING COVERAGE: 79 SQFT

BUILDING COVERAGE AREA: 808 SQFT



PLAN 1B - UNIT 1

FIRST FLOOR

A. FIRST FLOOR GROSS AREA: 480 SQFT

D. GARAGE AREA AREA: 86 SQFT

SECOND FLOOR

B. SECOND FLOOR GROSS AREA: 3 SQFT

G. VENT SHAFT AREA: 745 SQFT

H. EXCLUDED AREA: 6 SQFT

THIRD FLOOR

C. THIRD FLOOR GROSS AREA: 3 SQFT

G. VENT SHAFT AREA: 708 SQFT

H. EXCLUDED AREA: 0 SQFT

UNIT TOTAL GROSS FLOOR AREA: 1,284 SQFT

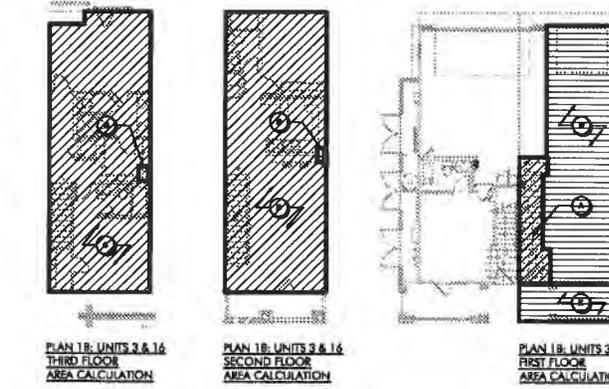
BUILDING COVERAGE: PLAN 1B - UNIT 1

A. FIRST FLOOR GROSS AREA: 480 SQFT

D. GARAGE AREA AREA: 86 SQFT

K. EXTERIOR BUILDING COVERAGE: 88 SQFT

BUILDING COVERAGE AREA: 654 SQFT



GFA: PLAN 1B - UNITS 3, 16

FIRST FLOOR

A. FIRST FLOOR GROSS AREA: 480 SQFT

D. GARAGE AREA AREA: 478 SQFT

SECOND FLOOR

B. SECOND FLOOR GROSS AREA: 3 SQFT

G. VENT SHAFT AREA: 698 SQFT

H. EXCLUDED AREA: 2 SQFT

THIRD FLOOR

C. THIRD FLOOR GROSS AREA: 3 SQFT

G. VENT SHAFT AREA: 687 SQFT

H. EXCLUDED AREA: 0 SQFT

UNIT TOTAL GROSS FLOOR AREA: 1,671 SQFT

BUILDING COVERAGE: PLAN 1B - UNITS 3, 16

A. FIRST FLOOR GROSS AREA: 480 SQFT

D. GARAGE AREA AREA: 478 SQFT

K. EXTERIOR BUILDING COVERAGE: 88 SQFT

BUILDING COVERAGE AREA: 654 SQFT

- CALCULATION PLAN KEY NOTES:**
- A. FIRST FLOOR GROSS AREA
 - B. SECOND FLOOR GROSS AREA
 - C. THIRD FLOOR GROSS AREA
 - D. GARAGE AREA
 - E. UTILITIES CLOSET/ ROOF ACCESS AREA
 - F. TRASH/ RECYCLING RECEPTACLE AREA
 - G. VENT SHAFT AREA
 - H. EXCLUDED AREAS PER 16.04.025 (C) (1)
 - J. PAU AREA IN GARAGE (INCLUDED IN GFA)
 - K. EXTERIOR BUILDING COVERAGE

- LEGEND:**
- AREA COUNTED AS UNIT GROSS SQUARE FOOTAGE
 - AREA COUNTED AS BUILDING GROSS SQUARE FOOTAGE
 - AREA INCLUDED IN BUILDING COVERAGE BUT NOT COUNTED AS GROSS SQUARE FOOTAGE

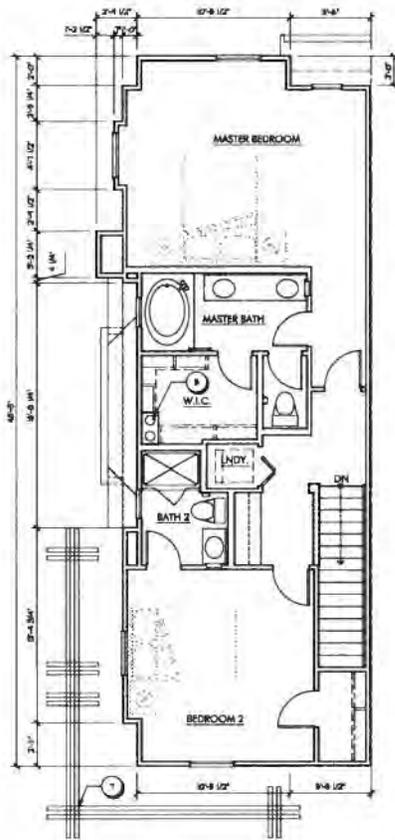
TOWNHOME - AREA CALCULATIONS
PLAN 1A & 1B
SCALE: 1/8"=1'-0"

JUNE 8, 2012 PROJECT NO: 221.00P

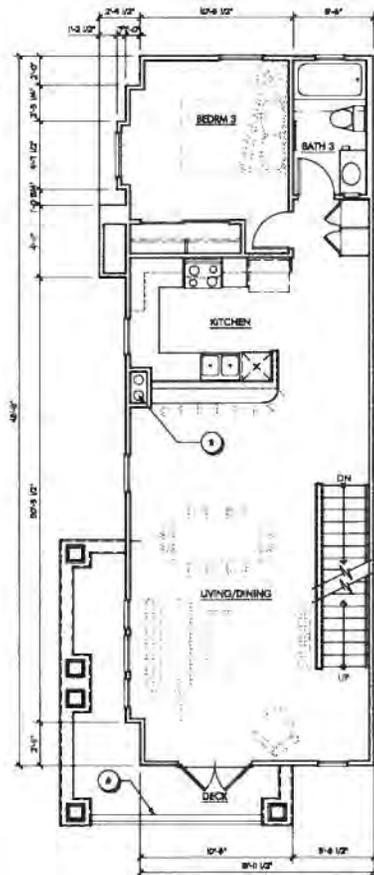
389 EL CAMINO, MENLO PARK, CA
 MATTESON DEVELOPMENT PARTNERS, INC.

3845 DOWNS DRIVE,
 PLASANTON, CA 94588
 925.251.7200

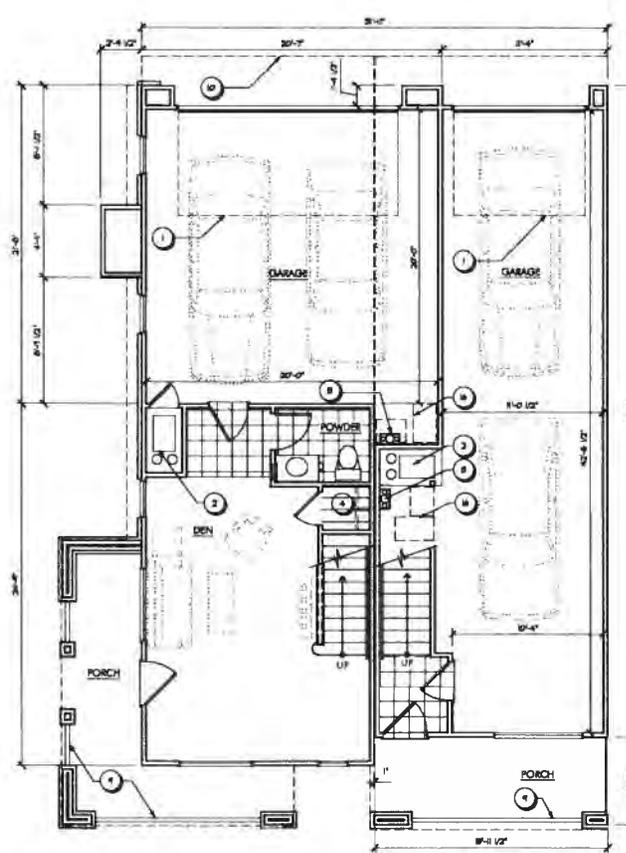
A3.1b



PLAN 1C UNITS 5, 9
THIRD FLOOR



PLAN 1C - UNITS 5, 9
SECOND FLOOR



PLAN 1C - UNITS 5, 9
FIRST FLOOR

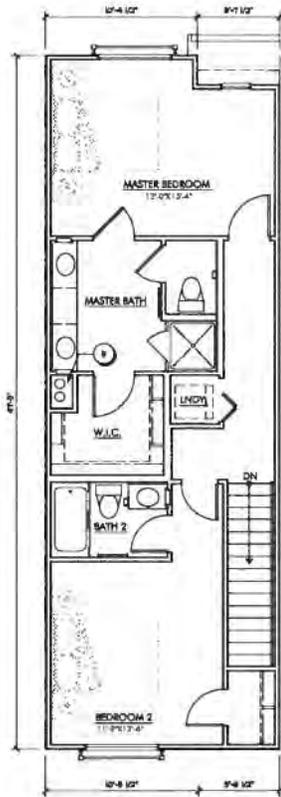
PLAN 1D - UNITS 6, 10
FIRST FLOOR

- FLOOR PLAN KEY NOTES:**
1. Overhead garage door above
 2. Furnace & ducts
 3. Tank-less water heater
 4. Line of stair above
 5. Duct space
 6. Roof access ladder
 7. Trellis- see elevation
 8. Guardrail
 9. Decorative railing
 10. Lines of floor above/ below
 11. Optional elevator/ storage
 12. Attic access
 13. Attic furnace
 14. Meter locations
 15. Fireplace
 16. Trash / recycling containers

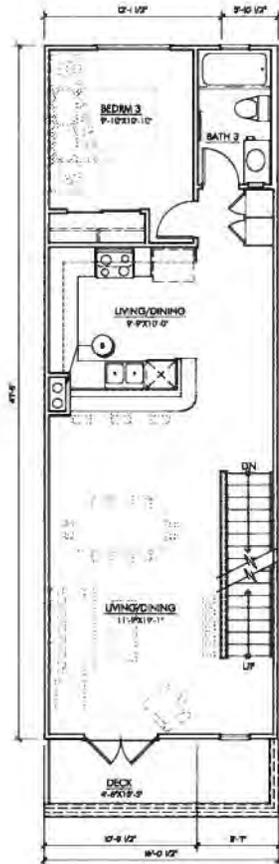
TOWNHOME - UNIT PLANS
 PLAN 1C & 1D - FLOOR PLAN
 SCALE: 1/4" = 1'-0"
 JUNE 8, 2012 PROJECT NO: 221.009



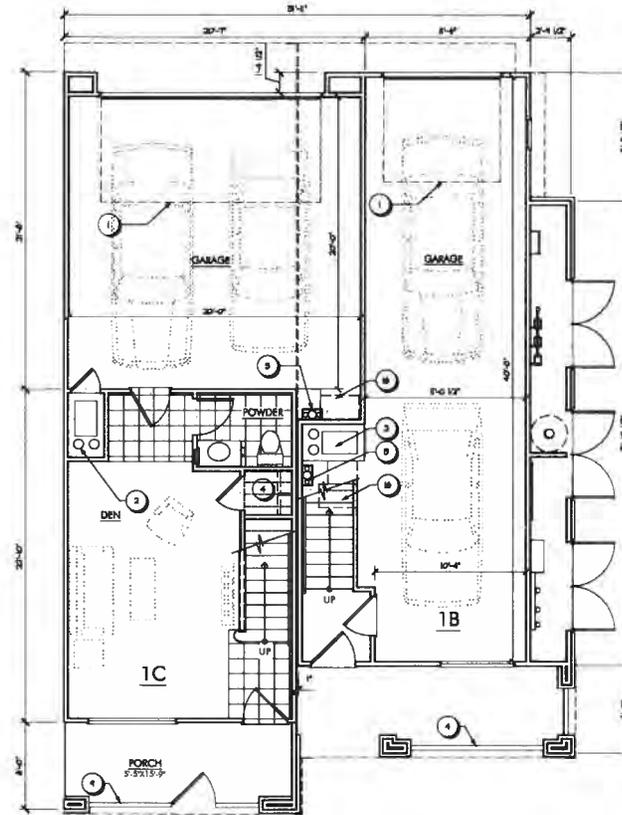
389 EL CAMINO, MENLO PARK, CA
 MATTESON DEVELOPMENT PARTNERS, INC.



PLAN 1C - UNITS 7, 11
 PLAN 1D SIM - UNITS 6, 10
 THIRD FLOOR



PLAN 1C - UNITS 7, 11
 PLAN 1D SIM - UNITS 6, 10
 SECOND FLOOR



PLAN 1C - UNITS 7, 11
 FIRST FLOOR

PLAN 1B - UNITS 8, 12
 FIRST FLOOR
 SEE A3.1 C3

- FLOOR PLAN KEY NOTES:**
1. Overhead garage door above
 2. Furnace & ducts
 3. Tank-less water heater
 4. Line of stair above
 5. Duct space
 6. Roof access ladder
 7. Trellis-see elevation
 8. Guardrail
 9. Decorative railing
 10. Lines of floor above/ below
 11. Optional elevator/ storage
 12. Attic access
 13. Attic furnace
 14. Meter locations
 15. Fireplace
 16. Trash / recycling containers

TOWNHOME - UNIT PLANS
 PLANS 1B & 1C - FLOOR PLAN
 SCALE: 1/4"=1'-0"
 JUNE 8, 2012 PROJECT NO: 231.009

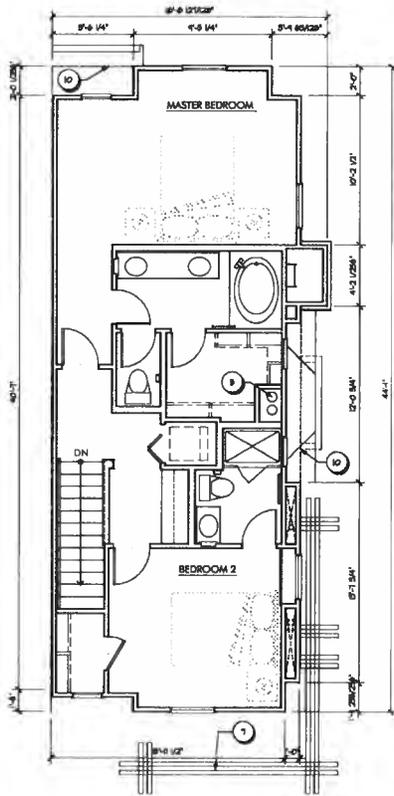
389 EL CAMINO, MENLO PARK, CA

MATTESON DEVELOPMENT PARTNERS, INC.

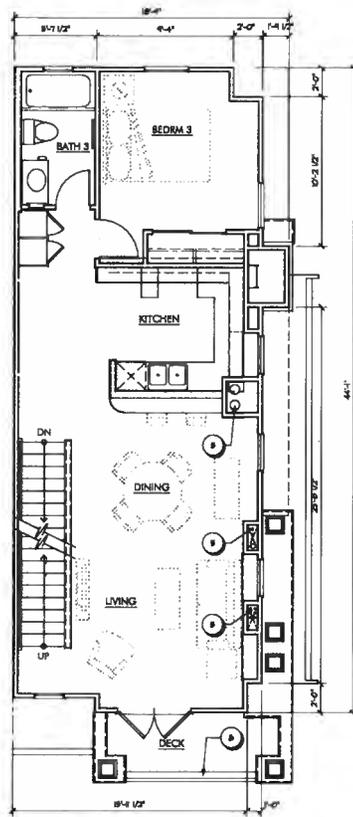


3885 OWENS DEVL.
 PLEASANTON, CA 94588
 925.751.7200
 925.751.7201 Fax

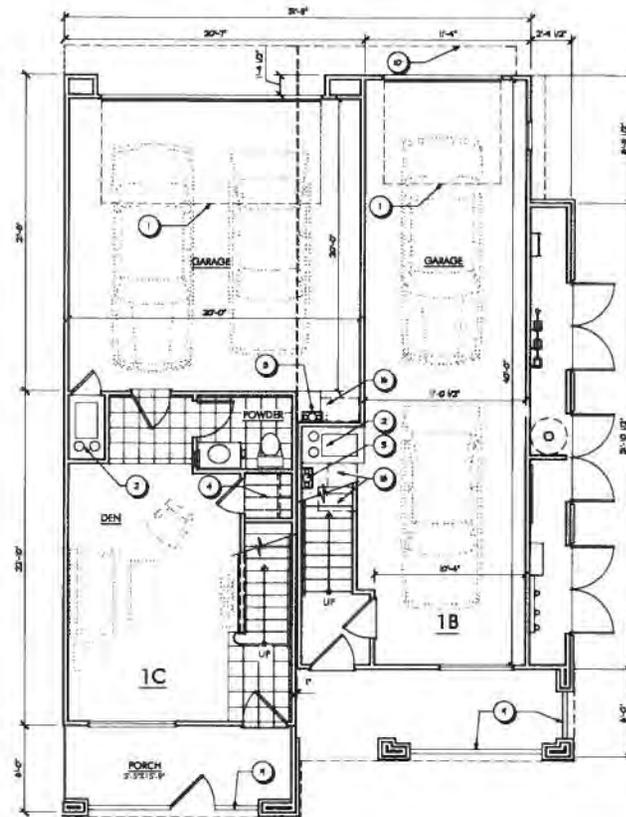
A3.1c2



PLAN 1B - UNITS 8, 12
THIRD FLOOR



PLAN 1B - UNITS 8, 12
SECOND FLOOR



PLAN 1C - 7, 11
FIRST FLOOR
SEE A3.1C2

PLAN 1B - UNITS 8, 12
FIRST FLOOR

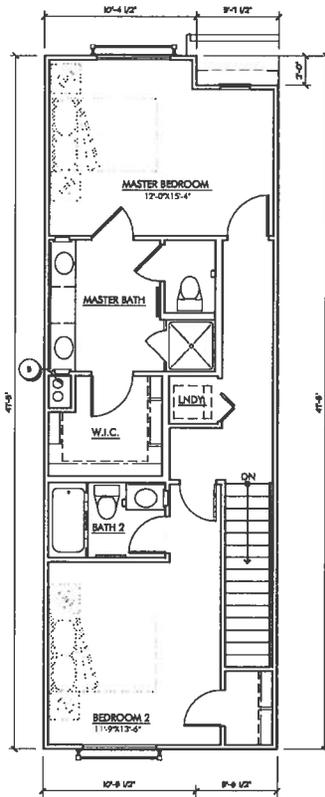
- FLOOR PLAN KEY NOTES:**
1. Overhead garage door above
 2. Furnace & ducts
 3. Tank-less water heater
 4. Line of stair above
 5. Duct space
 6. Roof access ladder
 7. Trellis- see elevation
 8. Guardrail
 9. Decorative railing
 10. Lines of floor above/ below
 11. Optional elevator/ storage
 12. Attic access
 13. Attic furnace
 14. Meter locations
 15. Fireplace
 16. Trash / recycling containers

389 EL CAMINO, MENLO PARK, CA
MATTESON DEVELOPMENT PARTNERS, INC.

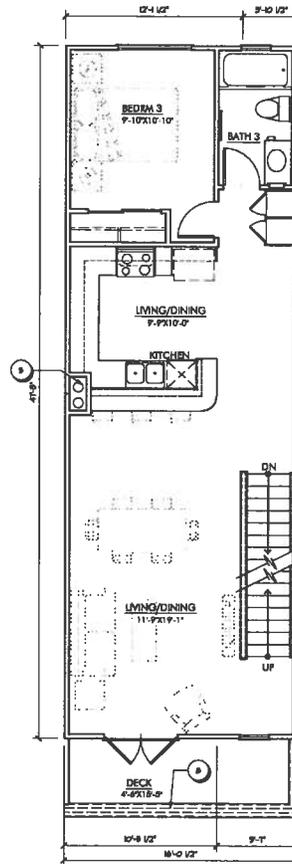
TOWNHOME - UNIT PLANS
PLAN 1B & 1C - FLOOR PLAN
SCALE: 1/4"=1'-0"

JUNE 8, 2012 PROJECT NO: 221.009

ARCHITECT



PLAN 1D - UNITS 6, 10
THIRD FLOOR



PLAN 1D - UNITS 6, 10
SECOND FLOOR

- FLOOR PLAN KEY NOTES: ①
1. Overhead garage door above
 2. Furnace & ducts
 3. Tank-less water heater
 4. Line of stair above
 5. Duct space
 6. Roof access ladder
 7. Trellis- see elevation
 8. Guardrail
 9. Decorative railing
 10. Lines of floor above/ below
 11. Optional elevator/ storage
 12. Attic access
 13. Attic furnace
 14. Meter locations
 15. Fireplace
 16. Trash / recycling containers

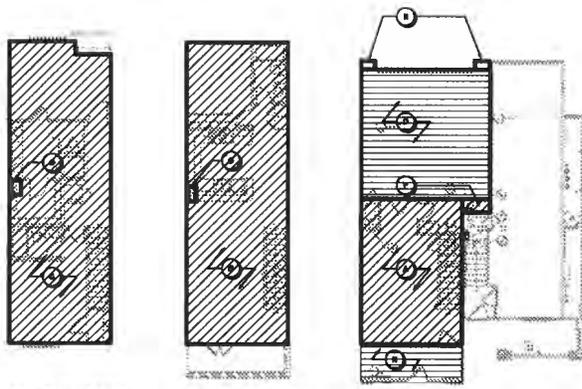
TOWNHOME - UNIT PLANS
PLAN 1D - FLOOR PLAN
SCALE: 1/4"=1'-0"

JUNE 8, 2012 PROJECT NO: 271.009



389 EL CAMINO, MENLO PARK, CA

MATTESON DEVELOPMENT PARTNERS, INC.



PLAN 1C - UNITS 7 & 11

FIRST FLOOR

A. FIRST FLOOR GROSS AREA: 442 SQFT

D. GARAGE AREA AREA: 412 SQFT

F. TRASH AREA, INC. IN GPA: 8 SQFT

H. EXCLUDED AREA: 8 SQFT

SECOND FLOOR

B. SECOND FLOOR GROSS AREA: 752 SQFT

G. VENT SHAFT AREA: 3 SQFT

THIRD FLOOR

C. THIRD FLOOR GROSS AREA: 740 SQFT

I. EXCLUDED AREA: 3 SQFT

UNIT TOTAL GROSS FLOOR AREA: 1,644 SQFT

BUILDING COVERAGE: PLAN 1C - UNITS 7 & 11

A. FIRST FLOOR GROSS AREA: 572 SQFT

D. GARAGE AREA AREA: 412 SQFT

H. EXCLUDED AREA: 8 SQFT

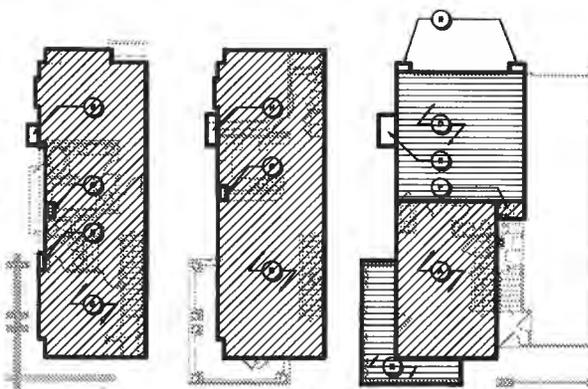
E. EXTERIOR BUILDING COVERAGE: 86 SQFT

BUILDING COVERAGE AREA: 858 SQFT

PLAN 1C, UNITS 7 & 11
THIRD FLOOR
AREA CALCULATION

PLAN 1C, UNITS 7 & 11
SECOND FLOOR
AREA CALCULATION

PLAN 1C, UNITS 7 & 11
FIRST FLOOR
AREA CALCULATION



PLAN 1C - UNITS 5 & 9

FIRST FLOOR

A. FIRST FLOOR GROSS AREA: 406 SQFT

D. GARAGE AREA AREA: 412 SQFT

F. TRASH AREA, INC. IN GPA: 30 SQFT

H. EXCLUDED AREA: 9 SQFT

SECOND FLOOR

B. SECOND FLOOR GROSS AREA: 814 SQFT

G. VENT SHAFT AREA: 3 SQFT

I. EXCLUDED AREA: 9 SQFT

THIRD FLOOR

C. THIRD FLOOR GROSS AREA: 754 SQFT

J. EXCLUDED AREA: 3 SQFT

UNIT TOTAL GROSS FLOOR AREA: 2,704 SQFT

BUILDING COVERAGE: PLAN 1C - UNITS 5 & 9

A. FIRST FLOOR GROSS AREA: 426 SQFT

D. GARAGE AREA AREA: 412 SQFT

H. EXCLUDED AREA: 20 SQFT

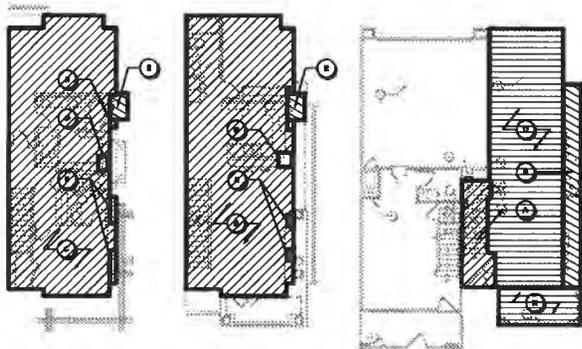
E. EXTERIOR BUILDING COVERAGE: 145 SQFT

BUILDING COVERAGE AREA: 803 SQFT

PLAN 1C, UNITS 5 & 9
THIRD FLOOR
AREA CALCULATION

PLAN 1C, UNITS 5 & 9
SECOND FLOOR
AREA CALCULATION

PLAN 1C, UNITS 5 & 9
FIRST FLOOR
AREA CALCULATION



PLAN 1B - UNITS 8 & 12

FIRST FLOOR

A. FIRST FLOOR GROSS AREA: 77 SQFT

D. GARAGE AREA AREA: 467 SQFT

E. UTILITY AREA: 69 SQFT

(ADDED TO BUILDING COV)

SECOND FLOOR

B. SECOND FLOOR GROSS AREA: 721 SQFT

G. VENT SHAFT AREA: 8 SQFT

H. EXCLUDED AREA: 12 SQFT

THIRD FLOOR

C. THIRD FLOOR GROSS AREA: 690 SQFT

I. EXCLUDED AREA: 10 SQFT

UNIT TOTAL GROSS FLOOR AREA: 1,488 SQFT

BUILDING COVERAGE: PLAN 1B - UNITS 8 & 12

A. FIRST FLOOR GROSS AREA: 77 SQFT

D. GARAGE AREA AREA: 467 SQFT

E. UTILITY AREA: 69 SQFT

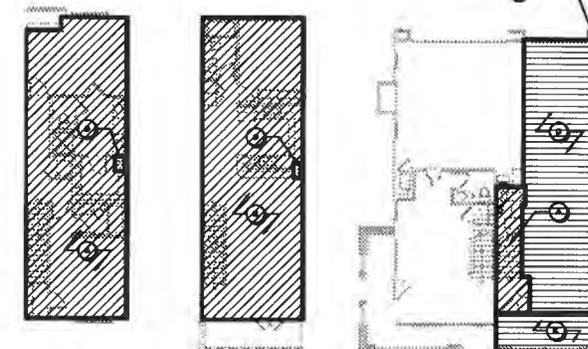
I. EXTERIOR BUILDING COVERAGE: 77 SQFT

BUILDING COVERAGE AREA: 790 SQFT

PLAN 1B, UNITS 8 & 12
THIRD FLOOR
AREA CALCULATION

PLAN 1B, UNITS 8 & 12
SECOND FLOOR
AREA CALCULATION

PLAN 1B, UNITS 8 & 12
FIRST FLOOR
AREA CALCULATION



PLAN 1D - UNITS 6 & 10

FIRST FLOOR

A. FIRST FLOOR GROSS AREA: 80 SQFT

D. GARAGE AREA AREA: 497 SQFT

H. EXCLUDED AREA: 2 SQFT

SECOND FLOOR

B. SECOND FLOOR GROSS AREA: 756 SQFT

G. VENT SHAFT AREA: 3 SQFT

THIRD FLOOR

C. THIRD FLOOR GROSS AREA: 740 SQFT

I. EXCLUDED AREA: 3 SQFT

UNIT TOTAL GROSS FLOOR AREA: 1,866 SQFT

BUILDING COVERAGE: PLAN 1D - UNITS 6 & 10

A. FIRST FLOOR GROSS AREA: 80 SQFT

D. GARAGE AREA AREA: 497 SQFT

H. EXCLUDED AREA: 2 SQFT

E. EXTERIOR BUILDING COVERAGE: 77 SQFT

BUILDING COVERAGE AREA: 756 SQFT

PLAN 1D, UNITS 6 & 10
THIRD FLOOR
AREA CALCULATION

PLAN 1D, UNITS 6 & 10
SECOND FLOOR
AREA CALCULATION

PLAN 1D, UNITS 6 & 10
FIRST FLOOR
AREA CALCULATION

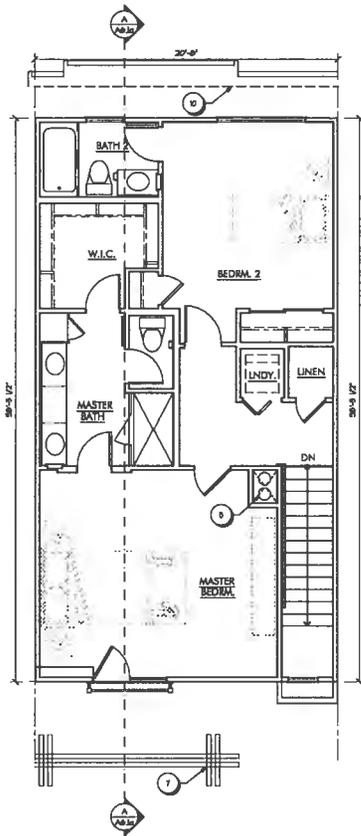
- LEGEND:**
- AREA COUNTED AS UNIT GROSS SQUARE FOOTAGE
 - AREA COUNTED AS BUILDING GROSS SQUARE FOOTAGE
 - AREA INCLUDED IN BUILDING COVERAGE BUT NOT COUNTED AS GROSS SQUARE FOOTAGE
- REVISION PLAN (R) NOTES:**
- A. FIRST FLOOR GROSS AREA
 - B. SECOND FLOOR GROSS AREA
 - C. THIRD FLOOR GROSS AREA
 - D. GARAGE AREA
 - E. UTILITIES CLOSET / ROOF ACCESS AREA
 - F. TRASH/RECYCLING RECEPTACLE AREA
 - G. VENT SHAFT AREA
 - H. EXCLUDED AREAS PER 14.04.325 (C) (1)
 - J. PAU AREA IN GARAGE (INCLUDED IN GPA)
 - K. EXTERIOR BUILDING COVERAGE

TOWNHOME - AREA CALCULATIONS
PLAN 1B, 1C & 1D
SCALE: 1/8" = 1'-0"
JUNE 9, 2012 PROJECT NO: 221.009

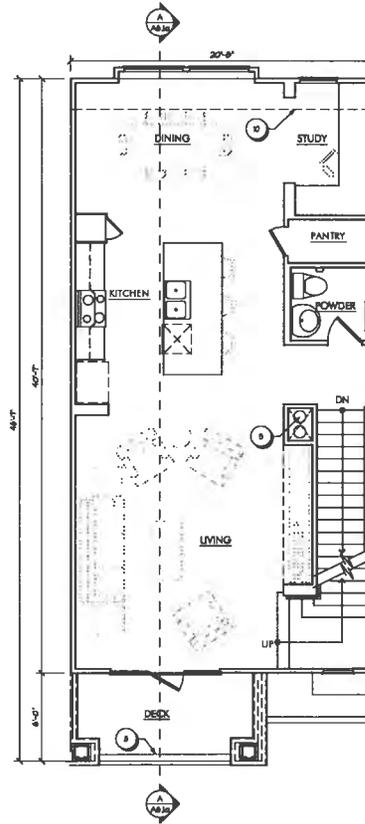
389 EL CAMINO, MENLO PARK, CA
MATTESON DEVELOPMENT PARTNERS, INC.

3865 OWENS DRIVE,
PULSANTON, CA 94388
925.251.7200
925.251.7201

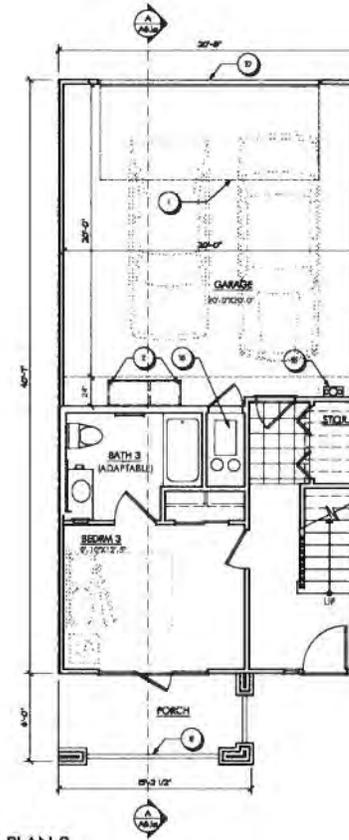
A3.1d



PLAN 2
THIRD FLOOR



PLAN 2
SECOND FLOOR



PLAN 2
FIRST FLOOR

- FLOOR PLAN KEY NOTES: ①
1. Overhead garage door above
 2. Furnace & ducts
 3. Tank-less water heater
 4. Line of stair above
 5. Duct space
 6. Roof access ladder
 7. Trellis- see elevation
 8. Guardrail
 9. Decorative railing
 10. Lines of floor above/ below
 11. Optional elevator/ storage
 12. Attic access
 13. Attic furnace
 14. Meter locations
 15. Fireplace
 16. Trash / recycling containers

389 EL CAMINO, MENLO PARK, CA

MATTESON DEVELOPMENT PARTNERS, INC.

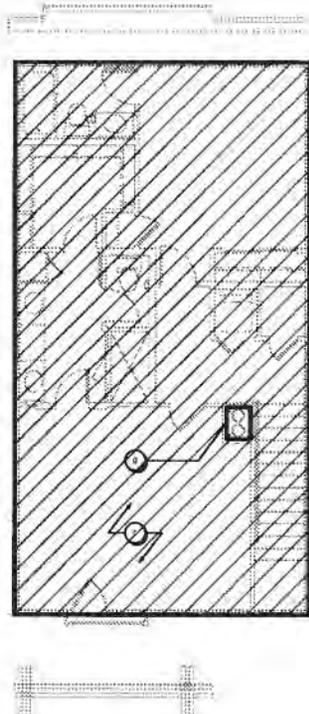
TOWNHOME - UNIT PLANS
PLAN 2 - FLOOR PLANS
SCALE: 1/4"=1'-0"

JUNE 8, 2012 PROJECT NO. 221 009

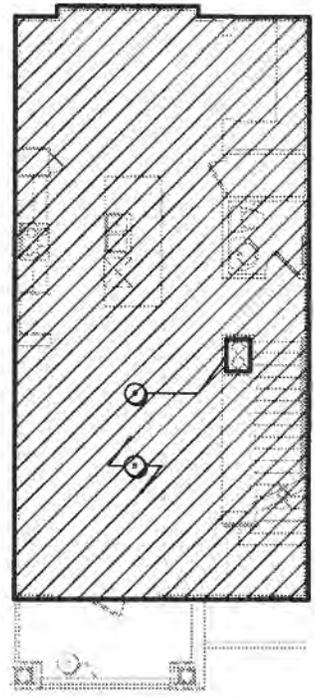


5065 OWENS DRIVE,
PLEASANTON, CA 94558
925.251.7200
925.251.7201 Fax

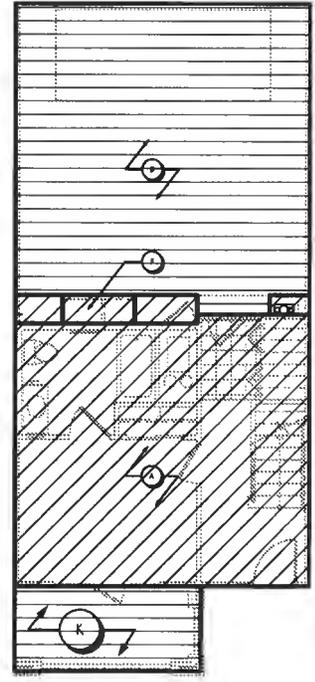
A3.2a



PLAN 2 - UNIT 15
THIRD FLOOR
AREA CALCULATION



PLAN 2 - UNIT 15
SECOND FLOOR
AREA CALCULATION



PLAN 2 - UNIT 15
FIRST FLOOR
AREA CALCULATION

PLAN 2 - UNIT 15

FIRST FLOOR		
A: FIRST FLOOR GROSS AREA:	425 SQFT	411 SQFT
D: GARAGE AREA AREA:		
F: TRASH AREA INC. IN GFA:		
SECOND FLOOR		
B: SECOND FLOOR GROSS AREA:	4 SQFT	808 SQFT
G: VENT SHAFT AREA:		
THIRD FLOOR		
C: THIRD FLOOR GROSS AREA:	4 SQFT	780 SQFT
H: VENT SHAFT AREA:		
UNIT TOTAL GROSS FLOOR AREA:		2,206 SQFT

BUILDING COVERAGE: PLAN 2 - UNIT 15		
A: FIRST FLOOR GROSS AREA:		411 SQFT
D: GARAGE AREA AREA:		425 SQFT
K: EXTERIOR BUILDING COVERAGE:		75 SQFT
BUILDING COVERAGE AREA:		911 SQFT

CALCULATION PLAN KEY NOTES:

- A. FIRST FLOOR GROSS AREA
- B. SECOND FLOOR GROSS AREA
- C. THIRD FLOOR GROSS AREA
- D. GARAGE AREA
- E. UTILITIES CLOSET/ ROOF ACCESS AREA
- F. TRASH/ RECYCLING RECEPTACLE AREA
- G. VENT SHAFT AREA
- H. EXCLUDED AREAS PER 16.04.325 (C) (1)
- J. FALL AREA IN GARAGE (INCLUDED IN GFA)
- K. EXTERIOR BUILDING COVERAGE

LEGEND:

- AREA COUNTED AS UNIT GROSS SQUARE FOOTAGE
- AREA COUNTED AS BUILDING GROSS SQUARE FOOTAGE
- AREA INCLUDED IN BUILDING COVERAGE BUT NOT COUNTED AS GROSS SQUARE FOOTAGE

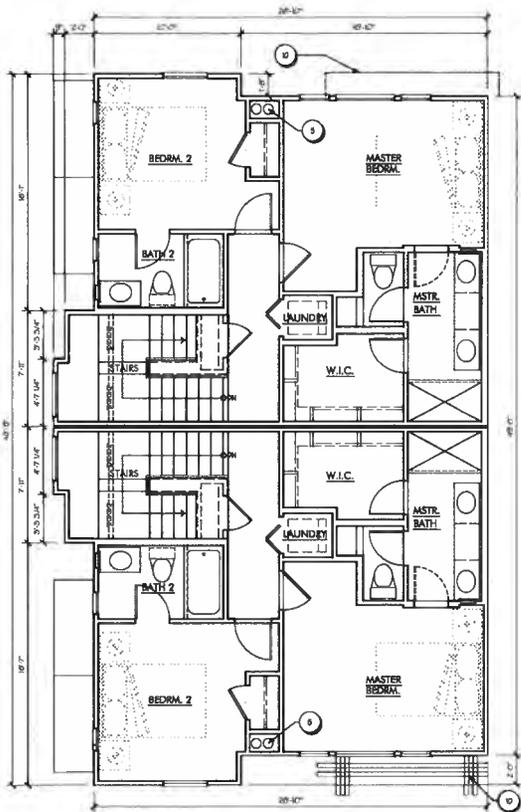
TOWNHOME - UNIT PLANS
PLAN 2- AREA CALCULATIONS
SCALE: 1/4"=1'-0"

JUNE 8, 2012 PROJECT NO.: 221.009

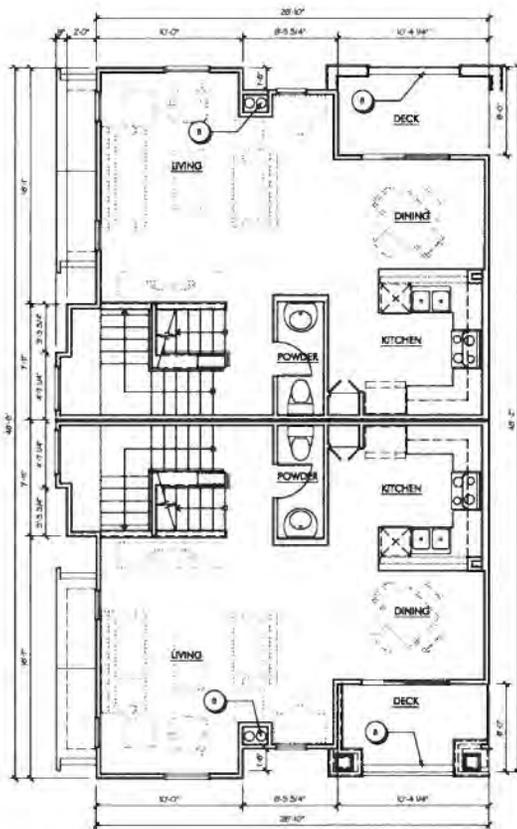
389 EL CAMINO, MENLO PARK, CA
MATTESON DEVELOPMENT PARTNERS, INC.

2863 COWAN DRIVE,
F. SASANTEL, CA 94068
925.251.7200
925.251.7201 FAX

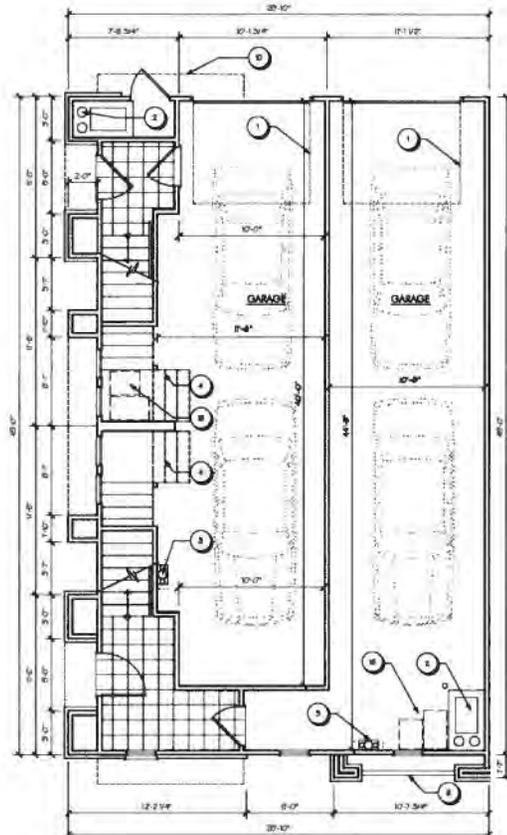
A3.2b



PLAN 3A & 3B
THIRD FLOOR



PLAN 3A & 3B
SECOND FLOOR



PLAN 3A
FIRST FLOOR

PLAN 3B
FIRST FLOOR

FLOOR PLAN KEY NOTES: ①

1. Overhead garage door above
2. Furnace & ducts
3. Tank-less water heater
4. Line of stair above
5. Duct space
6. Roof access ladder
7. Trellis- see elevation
8. Guardrail
9. Decorative railing
10. Lines of floor above/ below
11. Optional elevator/ storage
12. Attic access
13. Attic furnace
14. Meter locations
15. Fireplace
16. Trash / recycling containers

389 EL CAMINO, MENLO PARK, CA

MATTESON DEVELOPMENT PARTNERS, INC.

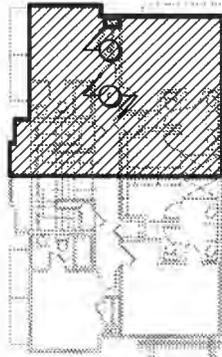
TOWNHOME - UNIT PLANS
PLAN 3A & 3B - FLOOR PLANS
SCALE: 1/4"=1'-0"

JUNE 8, 2012 PROJECT NO. 221-009

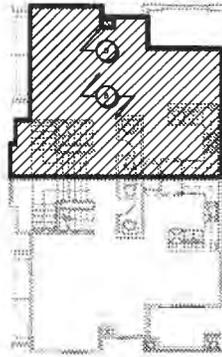


5865 OWENS DRIVE,
PLAZAANTON, CA 94589
925.251.7300
925.251.7301 Fax

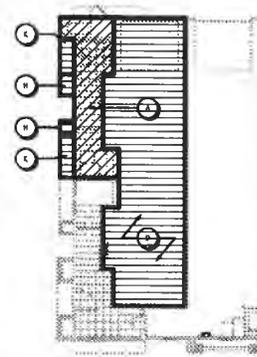
A3.3a



**PLAN 3A - UNIT 13
THIRD FLOOR
AREA CALCULATION**



**PLAN 3A - UNIT 13
SECOND FLOOR
AREA CALCULATION**



**PLAN 3A - UNIT 13
FIRST FLOOR
AREA CALCULATION**

PLAN 3A - UNIT 13

FIRST FLOOR

A: FIRST FLOOR GROSS AREA:	189 SQFT
D: GARAGE AREA AREA:	425 SQFT
H: EXCLUDED AREA:	10 SQFT

SECOND FLOOR

D: SECOND FLOOR GROSS AREA:	594 SQFT
G: VENT SHAFT AREA:	2 SQFT

THIRD FLOOR

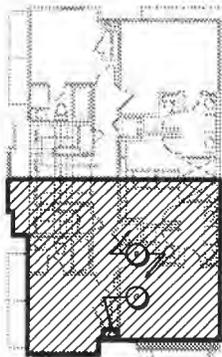
C: THIRD FLOOR GROSS AREA:	634 SQFT
G: VENT SHAFT AREA:	2 SQFT

UNIT TOTAL GROSS FLOOR AREA: 1342 SQFT

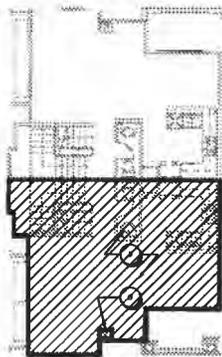
BUILDING COVERAGE: PLAN 3A - UNIT 13

A: FIRST FLOOR GROSS AREA:	189 SQFT
D: GARAGE AREA AREA:	425 SQFT
H: EXCLUDED AREA:	10 SQFT
K: EXTERIOR BUILDING COVERAGE:	22 SQFT

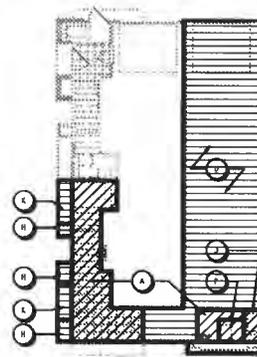
BUILDING COVERAGE AREA: 646 SQFT



**PLAN 3B - UNIT 14
THIRD FLOOR
AREA CALCULATION**



**PLAN 3B - UNIT 14
SECOND FLOOR
AREA CALCULATION**



**PLAN 3B - UNIT 14
FIRST FLOOR
AREA CALCULATION**

PLAN 3B - UNIT 14

FIRST FLOOR

A: FIRST FLOOR GROSS AREA:	189 SQFT
J: PAU AREA IN GARAGE (INCLUDED IN GFA):	12 SQFT
D: GARAGE AREA AREA:	492 SQFT
F: TRASH AREA INC. IN GFA:	16 SQFT
H: EXCLUDED AREA:	16 SQFT

SECOND FLOOR

D: SECOND FLOOR GROSS AREA:	594 SQFT
G: VENT SHAFT AREA:	2 SQFT

THIRD FLOOR

C: THIRD FLOOR GROSS AREA:	638 SQFT
G: VENT SHAFT AREA:	2 SQFT

UNIT TOTAL GROSS FLOOR AREA: 1490 SQFT

BUILDING COVERAGE: PLAN 3B - UNIT 14

A: FIRST FLOOR GROSS AREA:	189 SQFT
J: PAU IN GARAGE AREA:	12 SQFT
D: GARAGE AREA AREA:	492 SQFT
H: EXCLUDED AREA:	16 SQFT
K: EXTERIOR BUILDING COVERAGE:	39 SQFT

BUILDING COVERAGE AREA: 768 SQFT

CALCULATION PLAN KEY NOTES:

- A: FIRST FLOOR GROSS AREA
- B: SECOND FLOOR GROSS AREA
- C: THIRD FLOOR GROSS AREA
- D: GARAGE AREA
- E: UTILITY CLOSET/ WOOD ACCESS AREA
- F: TRASH/ RECYCLING RECEPTACLE AREA
- G: VENT SHAFT AREA
- H: EXCLUDED AREAS PER 14.04.325 (C) (1)
- J: PAU AREA IN GARAGE (INCLUDED IN GFA)
- K: EXTERIOR BUILDING COVERAGE

LEGEND:

- AREA COUNTED AS UNIT GROSS SQUARE FOOTAGE
- AREA COUNTED AS BUILDING GROSS SQUARE FOOTAGE
- AREA INCLUDED IN BUILDING COVERAGE BUT NOT COUNTED AS GROSS SQUARE FOOTAGE

TOWNHOME - UNIT PLANS
PLAN 3A & 3B - AREA CALCULATIONS
SCALE: 1/8" = 1'-0"

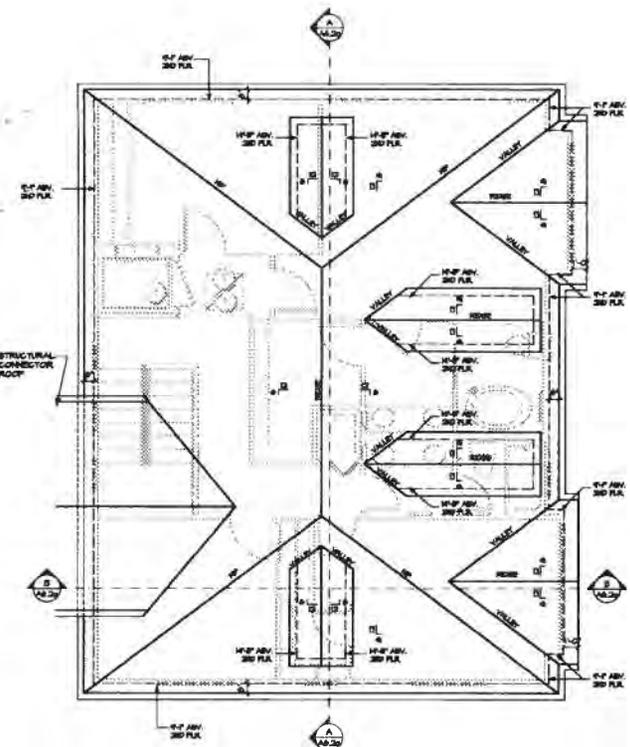


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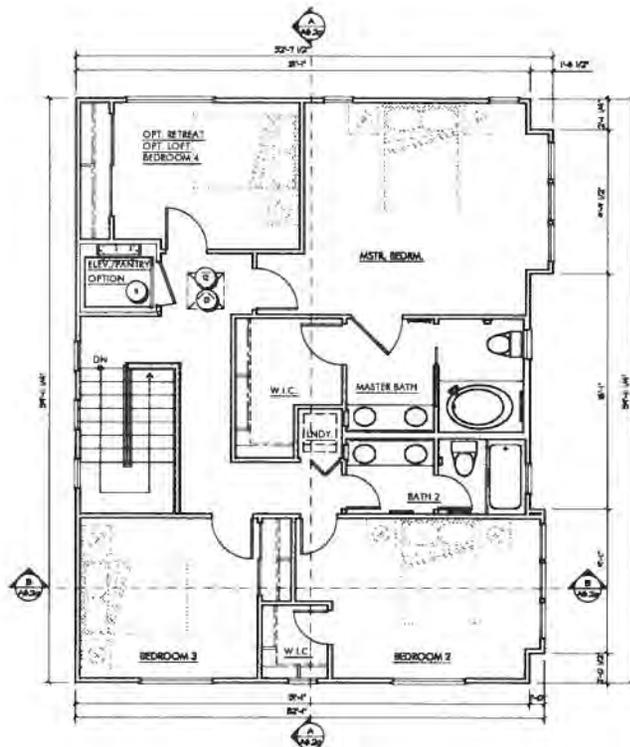
MATTESON DEVELOPMENT PARTNERS, INC.

3885 COWHIE DRIVE,
FISHERMAN, CA 94028
925.251.7300
925.251.7201 Fax

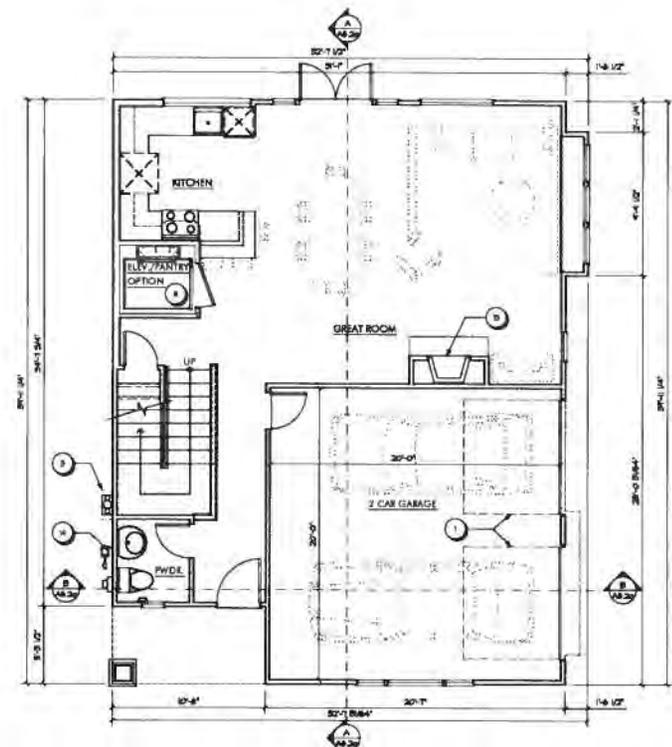
A3.3b



PLAN 4A
ROOF PLAN



PLAN 4A
SECOND FLOOR



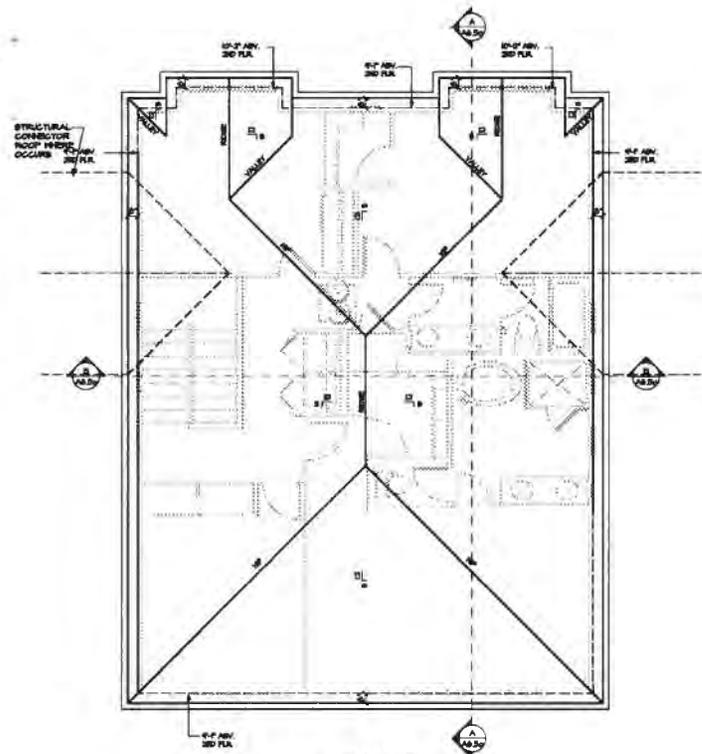
PLAN 4A
FIRST FLOOR

FLOOR PLAN KEY NOTES: ①

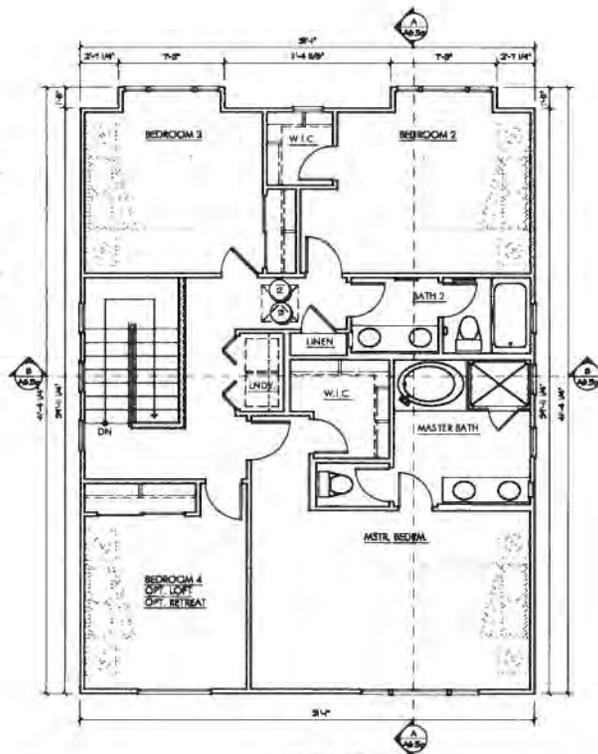
- | | |
|---------------------------------|----------------------------------|
| 1. Overhead garage door above | 13. Attic furnace |
| 2. Furnace & ducts | 14. Meter locations |
| 3. Tank-less water heater | 15. Fireplace |
| 4. Line of stair above | 16. Trash / recycling containers |
| 5. Duct space | |
| 6. Roof access ladder | |
| 7. Trellis- see elevation | |
| 8. Guardrail | |
| 9. Decorative railing | |
| 10. Lines of floor above/ below | |
| 11. Optional elevator/ storage | |
| 12. Attic access | |

BUILDING D1- UNIT PLANS
 PLAN 4A- FLOOR PLANS
 SCALE: 1/4"=1'-0"
 JUNE 8, 2012 PROJECT NO: 221.009

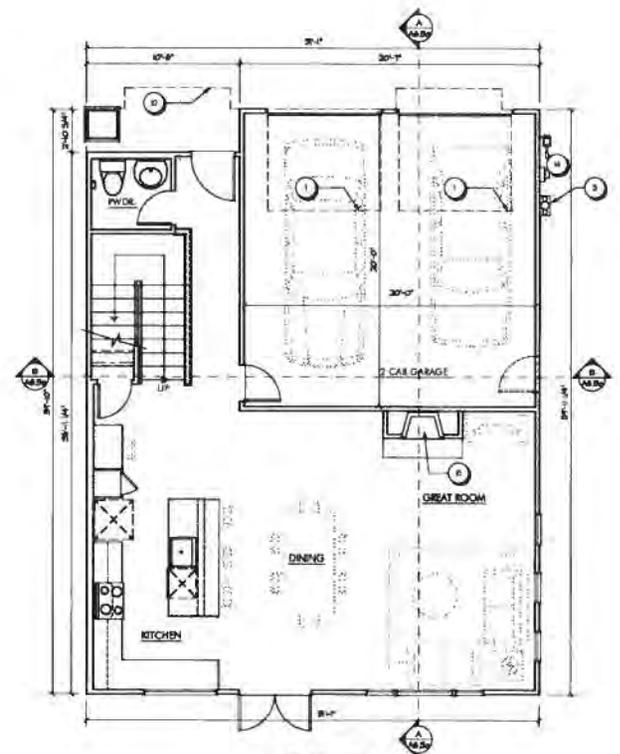
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PLAN 4B
ROOF PLAN



PLAN 4B
SECOND FLOOR



PLAN 4B
FIRST FLOOR

FLOOR PLAN KEY NOTES: ①

- | | |
|---------------------------------|----------------------------------|
| 1. Overhead garage door above | 13. Attic furnace |
| 2. Furnace & ducts | 14. Meter locations |
| 3. Tank-less water heater | 15. Fireplace |
| 4. Line of stair above | 16. Trash / recycling containers |
| 5. Duct space | |
| 6. Roof access ladder | |
| 7. Trellis- see elevation | |
| 8. Guardrail | |
| 9. Decorative railing | |
| 10. Lines of floor above/ below | |
| 11. Optional elevator/ storage | |
| 12. Attic access | |

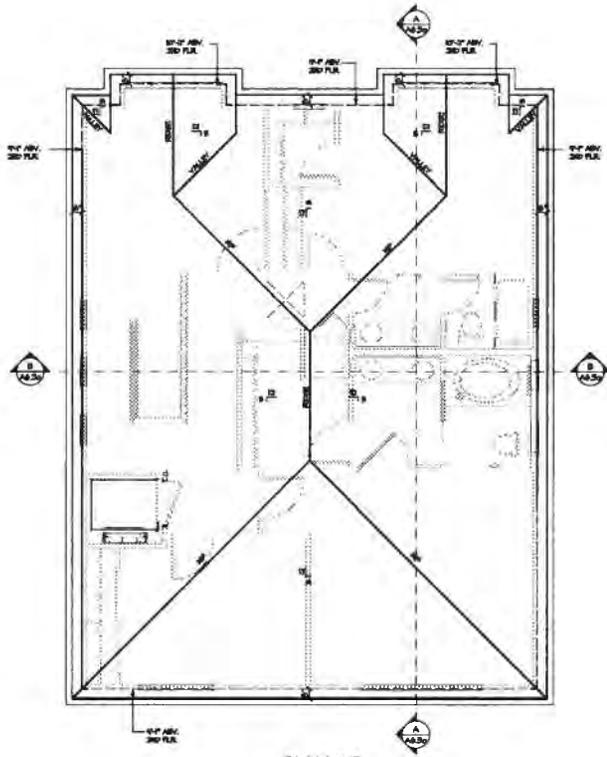
SINGLE FAMILY - UNIT D2, D3, & D4
PLAN 4B - FLOOR PLANS
SCALE: 1/4"=1'-0"

JUNE 8, 2012 PROJECT NO: 221 009

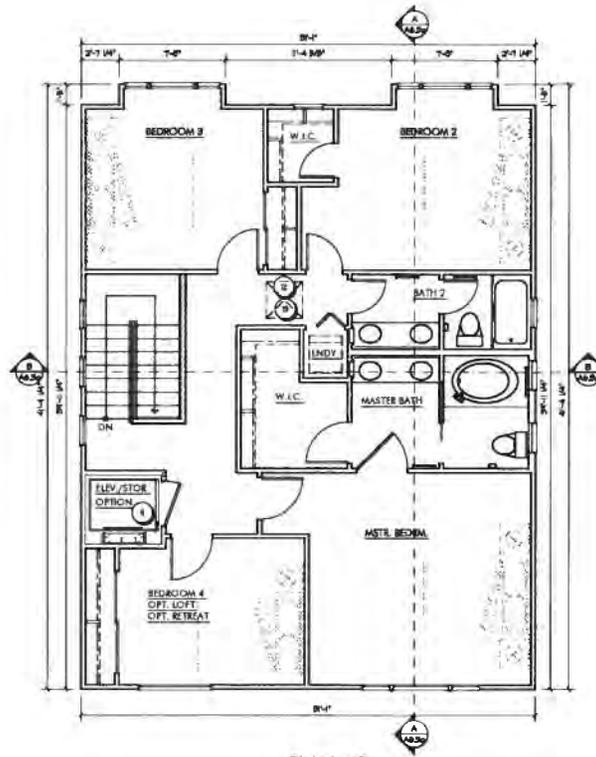


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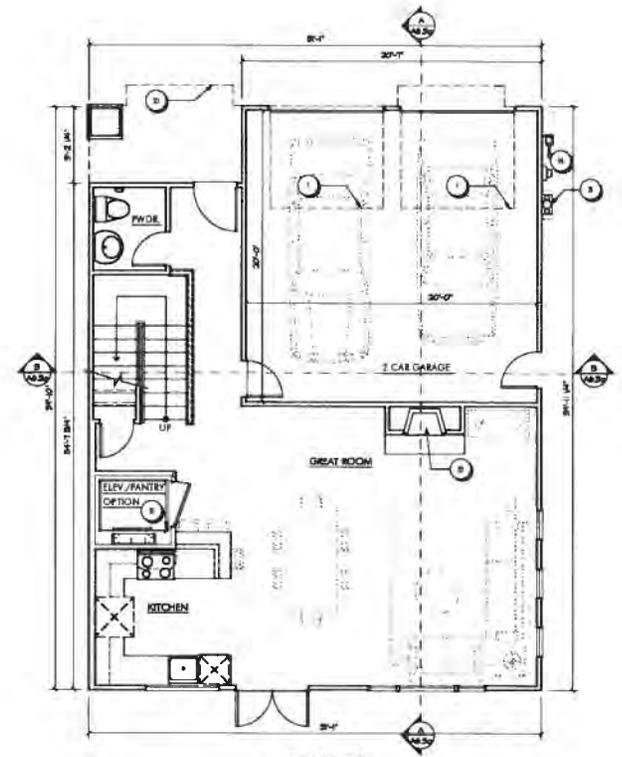
MATTESON DEVELOPMENT PARTNERS, INC.



PLAN 4C
ROOF PLAN



PLAN 4C
SECOND FLOOR



PLAN 4C
FIRST FLOOR

FLOOR PLAN KEY NOTES: ①

- | | |
|---------------------------------|----------------------------------|
| 1. Overhead garage door above | 13. Attic furnace |
| 2. Furnace & ducts | 14. Meter locations |
| 3. Tank-less water heater | 15. Fireplace |
| 4. Line of stair above | 16. Trash / recycling containers |
| 5. Duct space | |
| 6. Roof access ladder | |
| 7. Trellis- see elevation | |
| 8. Guardrail | |
| 9. Decorative railing | |
| 10. Lines of floor above/ below | |
| 11. Optional elevator/ storage | |
| 12. Attic access | |

SINGLE FAMILY - UNITS D5-DB
PLAN 4C - FLOOR PLANS
SCALE: 1/4"=1'-0"

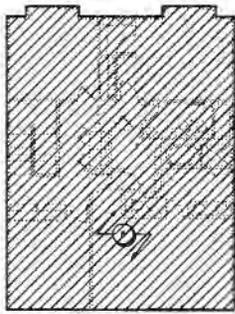


389 EL CAMINO, MENLO PARK, CA

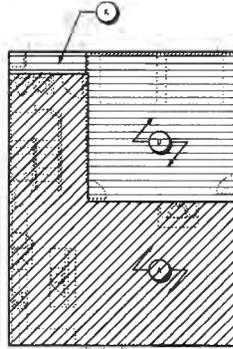
MATTESON DEVELOPMENT PARTNERS, INC.

5080 OWENS DRIVE,
PLEASANTON, CA 94533
925.251.7200
925.251.7251 Fax

A3.4c



PLAN 4B - UNITS 19-21
SECOND FLOOR
AREA CALCULATION

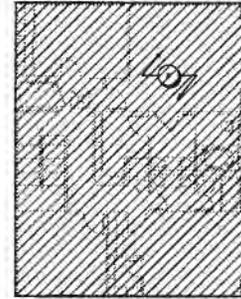


PLAN 4B - UNITS 19-21
FIRST FLOOR
AREA CALCULATION

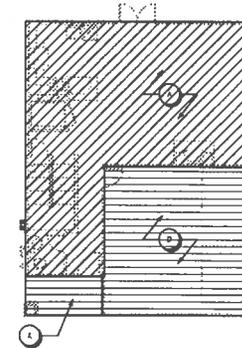
PLAN 4B - UNITS 19-21

FIRST FLOOR	
A: FIRST FLOOR GROSS AREA:	797 SQFT
D: GARAGE AREA AREA:	413 SQFT
SECOND FLOOR	
B: SECOND FLOOR GROSS AREA:	1,262 SQFT
UNIT TOTAL GROSS FLOOR AREA:	2,254 SQFT

BUILDING COVERAGE: PLAN 4B - UNITS 19-21	
A: FIRST FLOOR GROSS AREA:	797 SQFT
D: GARAGE AREA AREA:	413 SQFT
K: EXTERIOR BUILDING COVERAGE:	56 SQFT
BUILDING COVERAGE AREA:	1,241 SQFT



PLAN 4A - UNIT 18
SECOND FLOOR
AREA CALCULATION

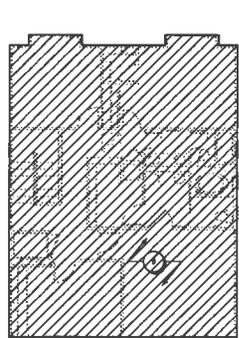


PLAN 4A - UNIT 18
FIRST FLOOR
AREA CALCULATION

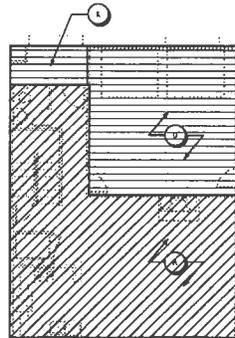
PLAN 4A - UNIT 18

FIRST FLOOR	
A: FIRST FLOOR GROSS AREA:	788 SQFT
D: GARAGE AREA AREA:	413 SQFT
SECOND FLOOR	
B: SECOND FLOOR GROSS AREA:	1,266 SQFT
UNIT TOTAL GROSS FLOOR AREA:	2,254 SQFT

BUILDING COVERAGE: PLAN 4A - UNIT 18	
A: FIRST FLOOR GROSS AREA:	788 SQFT
D: GARAGE AREA AREA:	413 SQFT
K: EXTERIOR BUILDING COVERAGE:	56 SQFT
BUILDING COVERAGE AREA:	1,257 SQFT



PLAN 4C - UNITS 22-25
SECOND FLOOR
AREA CALCULATION



PLAN 4C - UNITS 22-25
FIRST FLOOR
AREA CALCULATION

PLAN 4C - UNITS 22-25

FIRST FLOOR	
A: FIRST FLOOR GROSS AREA:	772 SQFT
D: GARAGE AREA AREA:	413 SQFT
SECOND FLOOR	
B: SECOND FLOOR GROSS AREA:	1,262 SQFT
UNIT TOTAL GROSS FLOOR AREA:	2,234 SQFT

BUILDING COVERAGE: PLAN 4B - UNITS 19-21	
A: FIRST FLOOR GROSS AREA:	772 SQFT
D: GARAGE AREA AREA:	413 SQFT
K: EXTERIOR BUILDING COVERAGE:	56 SQFT
BUILDING COVERAGE AREA:	1,241 SQFT

CALCULATION PLAN KEY NOTES: (A)

- A FIRST FLOOR GROSS AREA
- B SECOND FLOOR GROSS AREA
- C THIRD FLOOR GROSS AREA
- D GARAGE AREA
- E UTILITIES CLOSET/ ROOF ACCESS AREA
- F TRASH/ RECYCLING RECEPTACLE AREA
- G VENT SHAFT AREA
- H EXCLUDED AREAS PER 16.04.325 (C) (1)
- J PAU AREA IN GARAGE (INCLUDED IN GPA)
- K EXTERIOR BUILDING COVERAGE

LEGEND:

- AREA COUNTED AS UNIT GROSS SQUARE FOOTAGE
- AREA COUNTED AS BUILDING GROSS SQUARE FOOTAGE
- AREA INCLUDED IN BUILDING COVERAGE BUT NOT COUNTED AS GROSS SQUARE FOOTAGE

BUILDINGS D1 - D8 - UNIT PLANS
PLANS 4A-4C - AREA CALC'S
SCALE: 1/8"=1'-0"

JUNE 8, 2012 PROJECT NO: 221.009

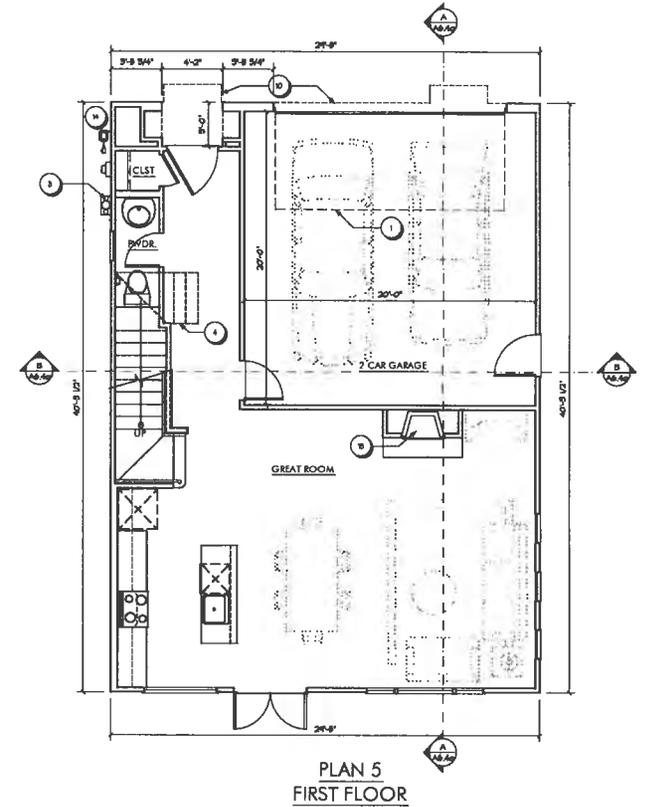
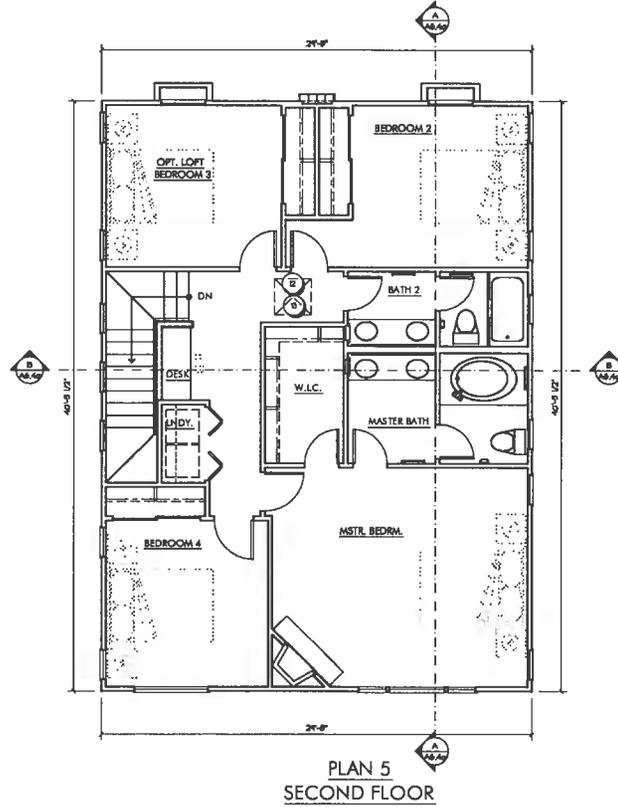
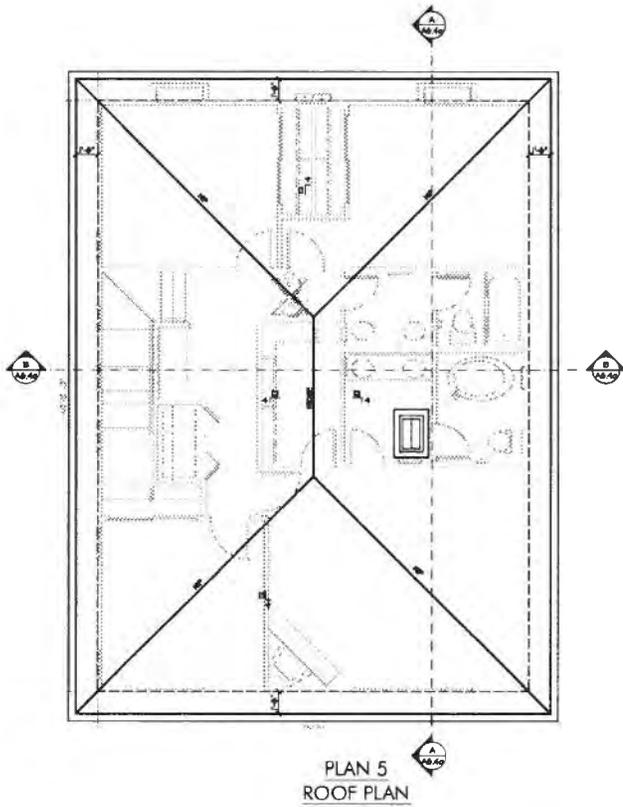


389 EL CAMINO, MENLO PARK, CA

MATTESON DEVELOPMENT PARTNERS, INC.

3855 OWENS DRIVE,
MENLO PARK, CA 94025
925.251.7000
925.251.7011 Fax

A3.4d



FLOOR PLAN KEY NOTES: ①

- | | |
|---------------------------------|----------------------------------|
| 1. Overhead garage door above | 13. Attic furnace |
| 2. Furnace & ducts | 14. Meter locations |
| 3. Tank-less water heater | 15. Fireplace |
| 4. Line of stair above | 16. Trash / recycling containers |
| 5. Duct space | |
| 6. Roof access ladder | |
| 7. Trellis- see elevation | |
| 8. Guardrail | |
| 9. Decorative railing | |
| 10. Lines of floor above/ below | |
| 11. Optional elevator/ storage | |
| 12. Attic access | |

BUILDING E - UNIT PLANS
PLAN 5 - FLOOR PLANS
SCALE: 1/4"=1'-0"

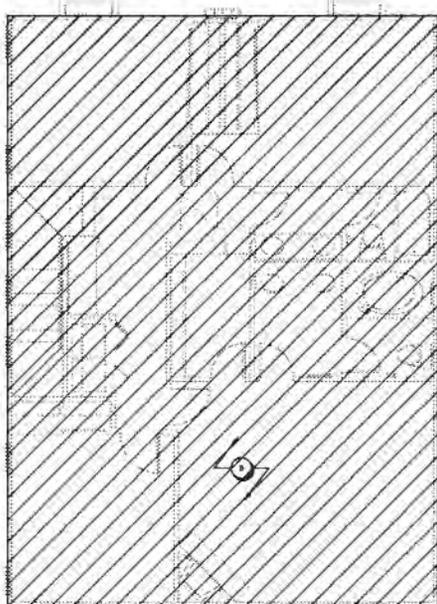
JUNE 8, 2012 PROJECT NO: 221.009

389 EL CAMINO, MENLO PARK, CA

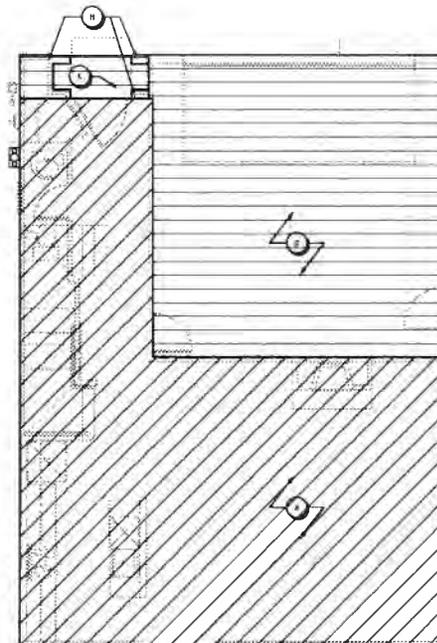
MATTESON DEVELOPMENT PARTNERS, INC.

5865 OWENS DRIVE,
PLEASANTON, CA 94588
925.251.7200
925.251.7201 Fax

A3.5a



PLAN 5 - UNIT 26
SECOND FLOOR
AREA CALCULATION



PLAN 5 - UNIT 26
FIRST FLOOR
AREA CALCULATION

PLAN 5 - UNIT 26

FIRST FLOOR

A. FIRST FLOOR GROSS AREA:	747 SQFT
D. GARAGE AREA:	419 SQFT
H. EXCLUDED AREA:	10 SQFT

SECOND FLOOR

B. SECOND FLOOR GROSS AREA:	1,035 SQFT
UNIT TOTAL GROSS FLOOR AREA:	1,782 SQFT

BUILDING COVERAGE: PLAN 5 - UNIT 26

A. FIRST FLOOR GROSS AREA:	747 SQFT
D. GARAGE AREA AREA:	419 SQFT
H. EXCLUDED AREA:	10 SQFT
E. EXTERIOR BUILDING COVERAGE:	17 SQFT
BUILDING COVERAGE AREA:	1,193 SQFT

CALCULATION PLAN KEY NOTES: (A)

- A. FIRST FLOOR GROSS AREA
- B. SECOND FLOOR GROSS AREA
- C. THIRD FLOOR GROSS AREA
- D. GARAGE AREA
- E. UTILITIES CLOSET/ ROOF ACCESS AREA
- F. TRASH/ RECYCLING RECEPTACLE AREA
- G. VENT SHAFT AREA
- H. EXCLUDED AREA PER 16.04.225 (C) (1)
- J. PAU AREA IN GARAGE (INCLUDED IN GFA)
- E. EXTERIOR BUILDING COVERAGE

LEGEND:

- AREA COUNTED AS UNIT GROSS SQUARE FOOTAGE
- AREA COUNTED AS BUILDING GROSS SQUARE FOOTAGE
- AREA INCLUDED IN BUILDING COVERAGE BUT NOT COUNTED AS GROSS SQUARE FOOTAGE

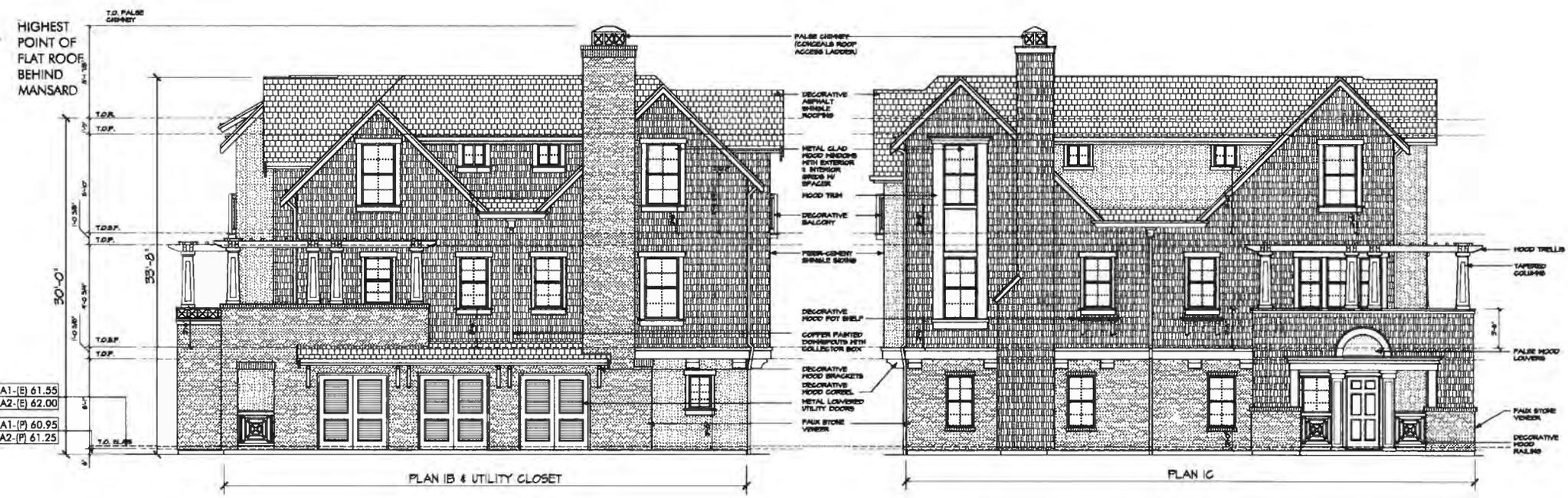
BUILDING E - UNIT PLANS
PLAN 5 - AREA CALCULATIONS
SCALE: 1/4"=1'-0"

JUNE 8, 2012 PROJECT NO: 221,009



389 EL CAMINO, MENLO PARK, CA

MATTESON DEVELOPMENT PARTNERS, INC.



A1 & A2 WEST (LEFT)

A1 & A2 EAST (RIGHT) EL CAMINO REAL

BUILDING A1, A2 SIM.
 REPRESENTIVE OF TOWNHOUSE
 BUILDINGS ON LOT ZONED C4 WITH
 MAXIMUM ROOF HEIGHT OF 30'

(E) = EXISTING AVERAGE NATURAL GRADE
 (P) = PROPOSED AVERAGE GRADE

SIDE ELEVATIONS
 BUILDING A1, A2 SIM.
 C4 ZONE
 SCALE: 1/4" = 1'-0"
 JUNE 8, 2012 PROJECT NO: 221.009

389 EL CAMINO, MENLO PARK, CA
 MATTESON DEVELOPMENT PARTNERS, INC.



A1-SOUTH (REAR) (E)-EXISTING AVERAGE NATURAL GRADE
 A2-NORTH (P)-PROPOSED AVERAGE GRADE

REAR ELEVATION
 BUILDING A1, A2 SIM.
 C4 ZONE
 SCALE: 1/4" = 1'-0"
 JUNE 8, 2012 PROJECT NO. 221.009
 DAHLIN GROUP

389 EL CAMINO, MENLO PARK, CA
 MATTESON DEVELOPMENT PARTNERS, INC.

5844 OWEN DRIVE,
 REDWOOD CITY, CA 94068
 925.251.7200
 925.251.7201 Fax

A5.1c

FALSE GIBNEY W/ METAL CAP
 (CONCEALS ROOF ACCESS LADDER)

DECORATIVE ASPHALT
 SHINGLE ROOFING

HOOD TRIM

METAL GLAZED HOOD
 HIDEOUTS WITH
 EXTERIOR & INTERIOR
 BRICK W/ SPACES

FRESH-CEMENT
 FINISH SIDING

COPPER PAINTED
 DOWNSPOUTS WITH
 COLLECTOR BOX

HOOD YELLS

TAPERED HOOD
 COLLARS

BRICK TRIM

DECORATIVE
 HOOD BRACKETS

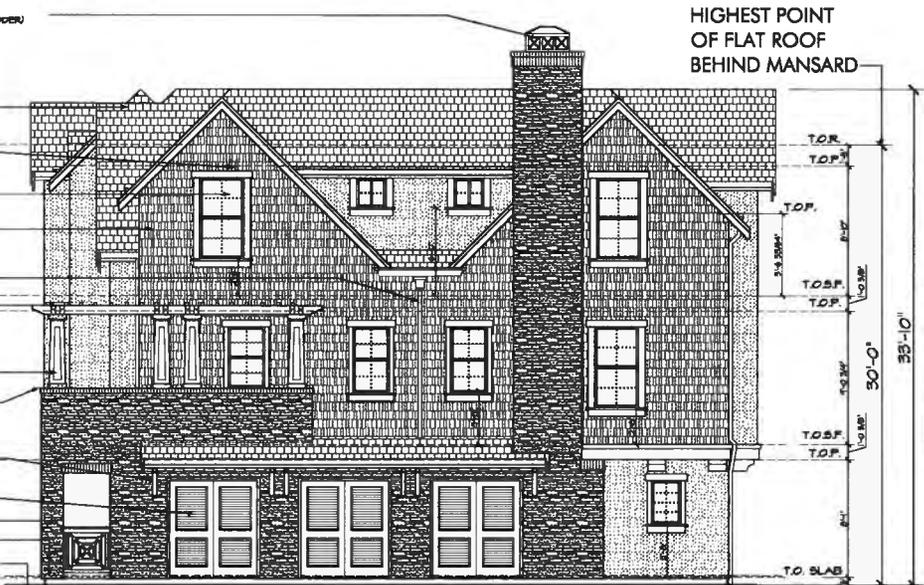
METAL LOUVERED
 UTILITY DOORS

PAVING STONE VENEER

(E) 61.80

(P) 61.20

HIGHEST POINT
 OF FLAT ROOF
 BEHIND MANSARD



PLAN 1A & UTILITY CLOSET

WEST RIGHT



PLAN B

EAST (LEFT-EL CAMINO REAL)

(E) - EXISTING AVERAGE NATURAL GRADE
 (P) - PROPOSED AVERAGE GRADE

SIDE ELEVATION
 BUILDING B
 C4 ZONE
 SCALE: 1/4" = 1'-0"

JUNE 8, 2012 PROJECT NO: 221.009

389 EL CAMINO, MENLO PARK, CA

MATTESSON DEVELOPMENT PARTNERS, INC.

5865 OWENS DRIVE,
 PLEASANTON, CA 94588
 925.251.7200
 925.251.7201 FAX

A5.2b

HIGHEST POINT OF FLAT ROOF BEHIND MANSARD



SOUTH (FRONT)

(E)=EXISTING AVERAGE NATURAL GRADE
(P)=PROPOSED AVERAGE GRADE

FRONT ELEVATION
BUILDING C
C4 ZONE
SCALE: 1/4"=1'-0"

JUNE 8, 2012 PROJECT NO: 221.009



389 EL CAMINO, MENLO PARK, CA

MATTESON DEVELOPMENT PARTNERS, INC.

220

5665 OWENS DRIVE,
PLEASANTON, CA 94588
925.251.7200
925.251.7201 Fax

A5.3a

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HIGHEST POINT OF FLAT ROOF BEHIND MANSARD

(E) 60.75
(P) 60.80

NORTH (REAR)

(E) - EXISTING AVERAGE NATURAL GRADE
(P) - PROPOSED AVERAGE GRADE

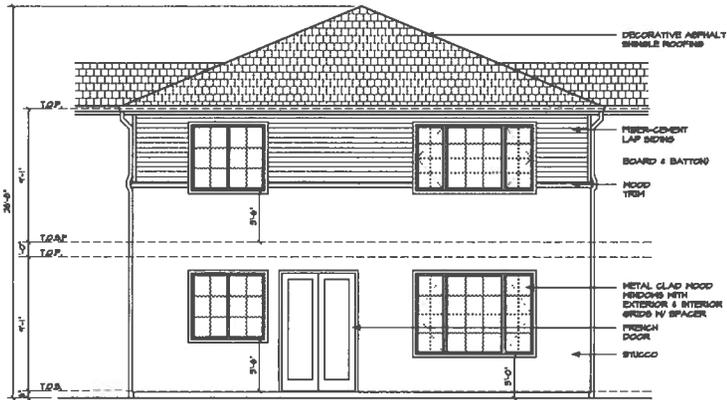
REAR ELEVATION
BUILDING C
C4 ZONE
SCALE: 1/4"=1'-0"
JUNE 8, 2012 PROJECT NO: 221.009



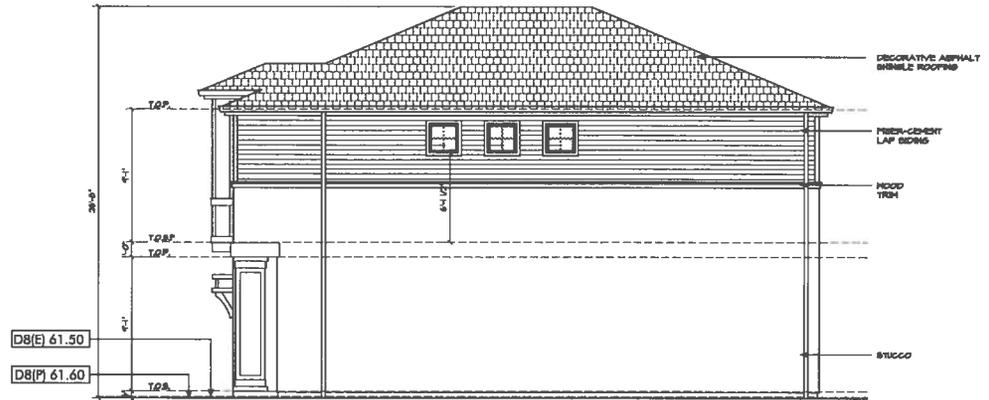
389 EL CAMINO, MENLO PARK, CA
MATTESON DEVELOPMENT PARTNERS, INC.

5865 OWENS DRIVE,
PLEASANTON, CA 94568
925.251.7200
925.251.7201 Fax

A5.3c

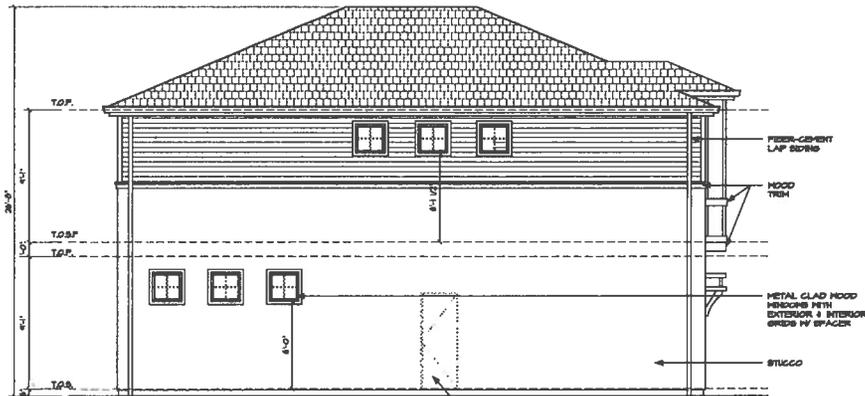


WEST (REAR)



NORTH (RIGHT)
D8-SOUTH

(E) = EXISTING AVERAGE NATURAL GRADE
(P) = PROPOSED AVERAGE GRADE



SOUTH (LEFT)
D8 NORTH

DOOR TO SIDE YARD AT ALL BUILDINGS EXCEPT D4 & D6



EAST (FRONT)

ELEVATION STYLE A, B SIM.
BUILDING D2 (D4, D6, & D8 SIM.)

SCALE: 1/4" = 1'-0"

JUNE 8, 2012 PROJECT NO: 221.009



389 EL CAMINO, MENLO PARK, CA

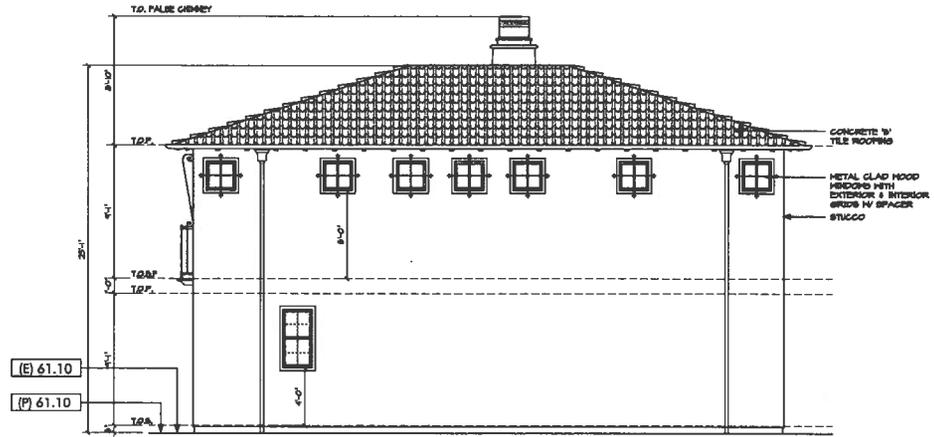
MATTESON DEVELOPMENT PARTNERS, INC.

5865 OWENS DRIVE,
PLEASANTON, CA 94588
925.251.7200
925.251.7201 Fax

A5.5a

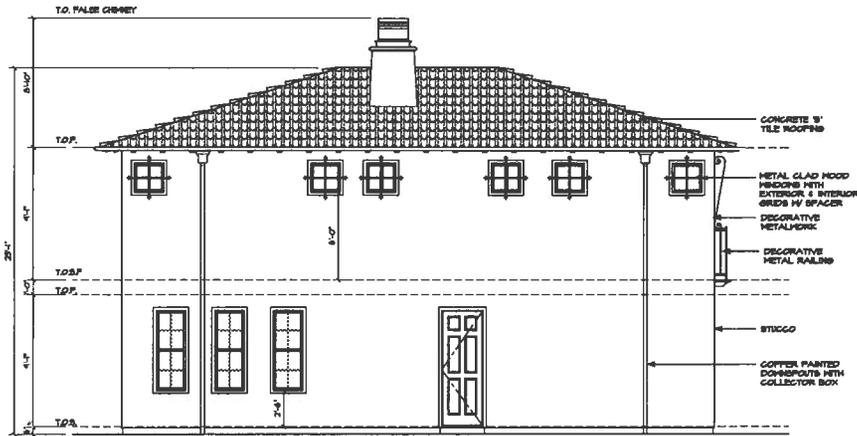


NORTH (REAR)



EAST (RIGHT)

(E)= EXISTING AVERAGE NATURAL GRADE
(P)= PROPOSED AVERAGE GRADE



WEST (LEFT)



SOUTH (FRONT) - PARTRIDGE AVENUE

ELEVATIONS
BUILDING E - PARTRIDGE AVENUE

SCALE: 1/4"=1'-0"

JUNE 8, 2012 PROJECT NO: 221.009



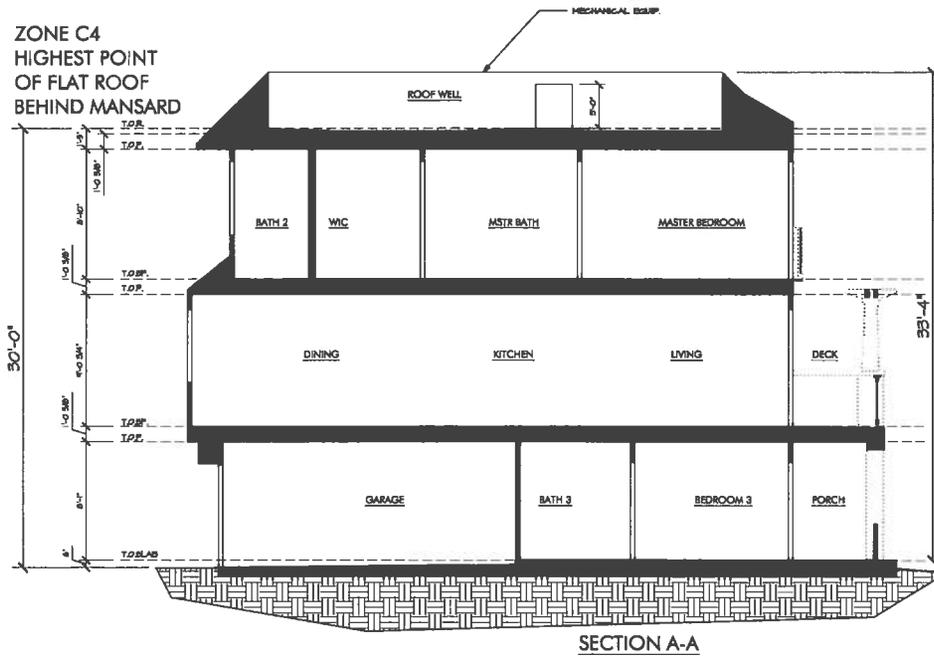
389 EL CAMINO, MENLO PARK, CA

MATTESON DEVELOPMENT PARTNERS, INC.

226

5865 OWENS DRIVE,
PLEASANTON, CA 94588
925.251.7100
925.251.7201 Fax

A5.6a



SECTION AA
BUILDING B - MANSARD
30' MAX. ROOF HEIGHT (C4)
SCALE: 1/4"=1'-0"

JUNE 8, 2012 PROJECT NO. 221.009

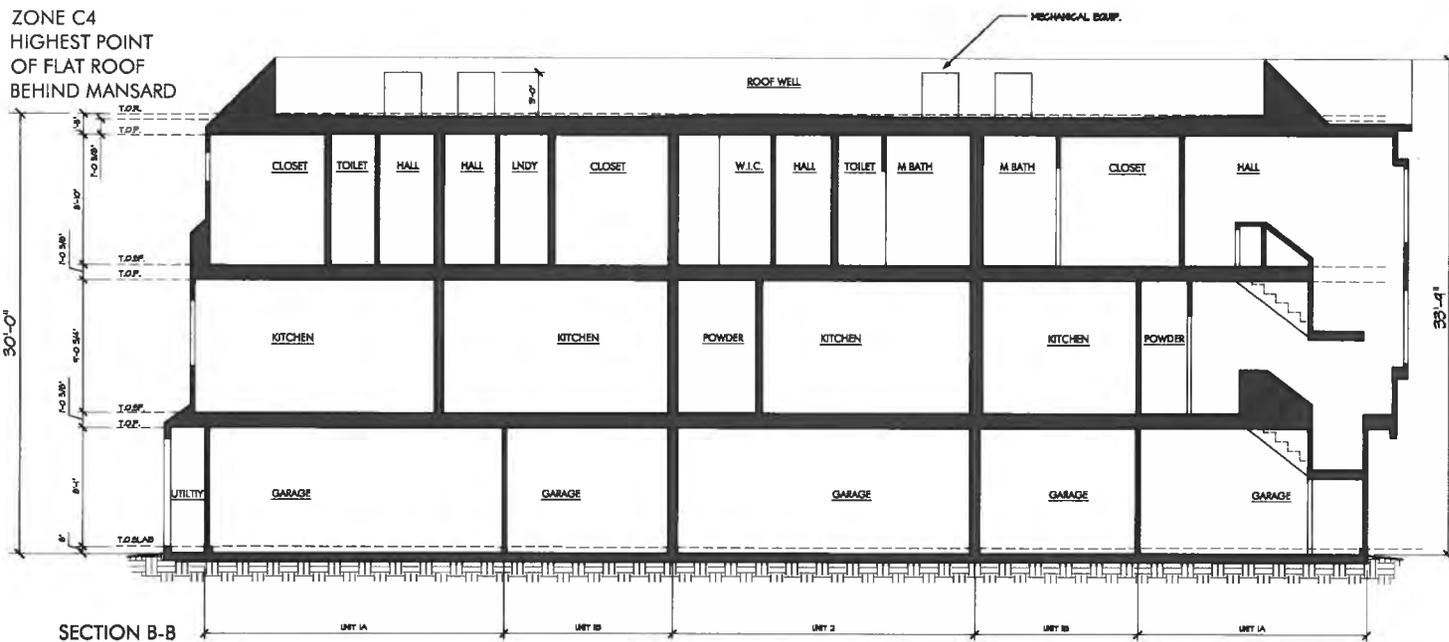


389 EL CAMINO, MENLO PARK, CA

MATTESON DEVELOPMENT PARTNERS, INC.

5865 COLUMBUS DRIVE,
DUBLIN, CA 94568
916.434.7200
916.251.7201 Fax

A6.1a



BUILDING B
 REPRESENTATIVE OF TOWNHOMES
 ON LOT ZONED C4 WITH
 MAXIMUM ROOF HEIGHT OF 30'

SECTION BB
 BUILDING B - MANSARD
 30' MAX. ROOF HEIGHT (C4)
 SCALE: 1/4"=1'-0"

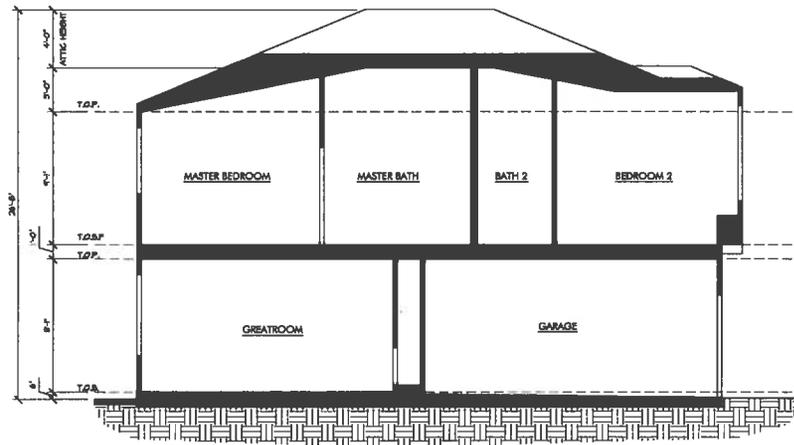
JUNE 8, 2012 PROJECT NO: 221.009

389 EL CAMINO, MENLO PARK, CA

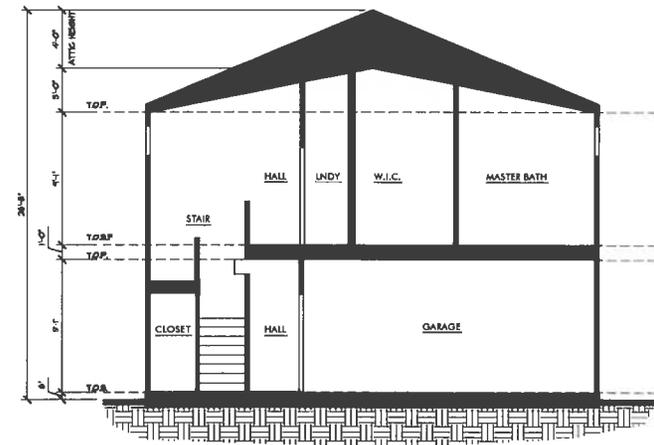
MATTESON DEVELOPMENT PARTNERS, INC.

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ZONE R3
 HIGHEST POINT OF
 ROOF IS BELOW 35'
 (30' FOR ZONE C4)



SECTION A-A



SECTION B-B

BUILDING D2
 REPRESENTATIVE OF BUILDING Ds

SECTIONS AA & BB
 BUILDING D2 (PLAN 4B)
 35' MAX. ROOF HEIGHT (R3)
 SCALE: 1/4" = 1'-0"

JUNE 8, 2012 PROJECT NO: 221.009



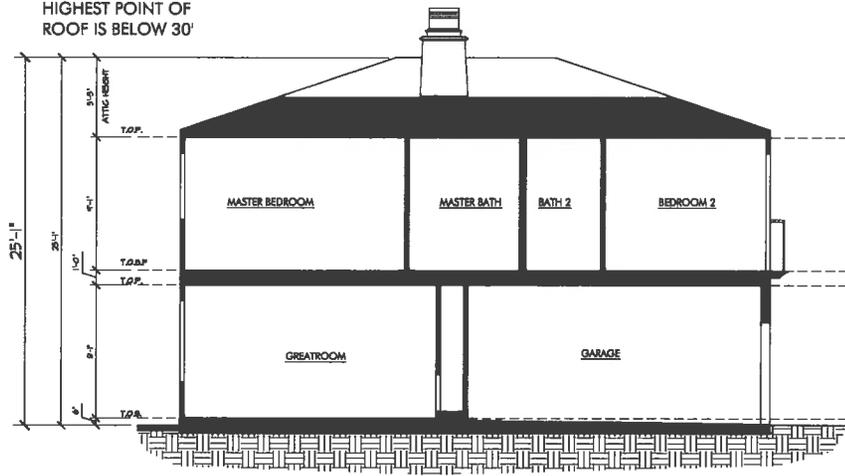
389 EL CAMINO, MENLO PARK, CA

MATTESON DEVELOPMENT PARTNERS, INC.

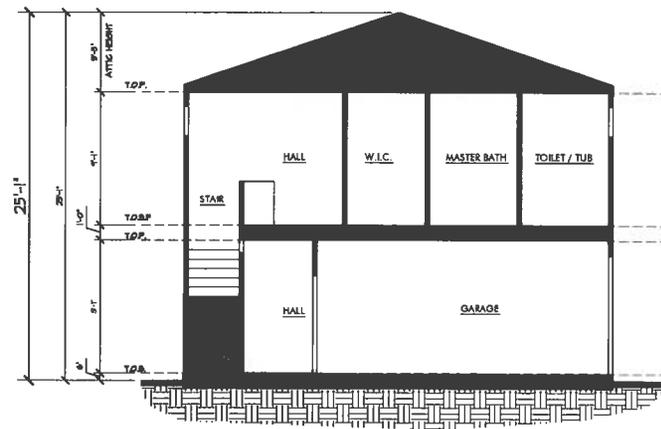
5865 OWENS DRIVE,
 PLEASANTON, CA 94568
 925.251.7200
 925.251.7201 Fax

A6.3a

ZONE C4
 HIGHEST POINT OF
 ROOF IS BELOW 30'



SECTION A-A



SECTION B-B

BUILDING E
 PARTRIDGE AVENUE

SECTIONS AA & BB
 BUILDING E (PLAN 5)
 35' MAX. ROOF HEIGHT (R3)
 SCALE: 1/4"=1'-0"

JUNE 8, 2012 PROJECT NO: 221.009



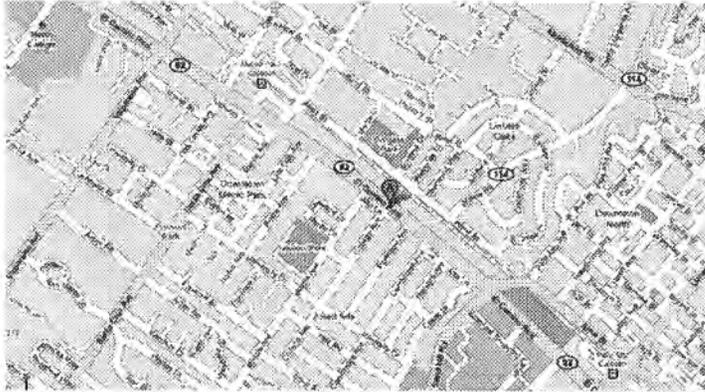
389 EL CAMINO, MENLO PARK, CA
 MATTESON DEVELOPMENT PARTNERS, INC.

5845 OWENS DRIVE
 MENLO PARK, CA 94508
 TEL: 650.720.0000
 FAX: 650.720.1201

231 A6.4a

SUSTAINABLE SITE:

- INFILL DEVELOPMENT**
- SITE IS LOCATED ON EXISTING URBANIZED SITE (INFILL) RATHER THAN OPEN SPACE AND FARMLAND (GREENFIELDS)
 - INFILL DEVELOPMENT REDUCES HARM TO THE ENVIRONMENT BY LIMITING SUBURBAN SPRAWL.
 - THIS REDUCES THE NEED FOR CAR TRAVEL, THEREBY REDUCING AIR POLLUTION

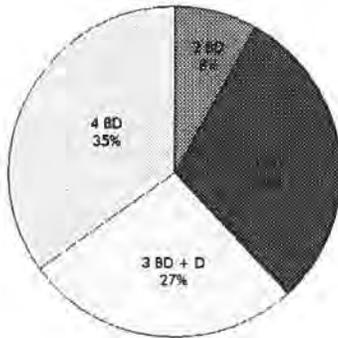


PROXIMITY TO JOBS:

LOCATED IN THE HEART OF DOWNTOWN MENLO PARK, CA.

DIVERSITY OF HOUSING TYPES AND COMMUNITY CONNECTIVITY

TYPE OF HOUSING



WATER USE REDUCTION:

- WATER SAVINGS 1.6 GALLONS PER FLUSH, LOW FLUSH TOILETS
- WATER SAVINGS 2.2 GALLONS PER MINUTE KITCHEN FAUCETS
- WATER SAVINGS 2.5 GALLONS PER MINUTE LAVATORY FAUCETS
- WATER SAVINGS 2.6 GALLONS PER MINUTE BATH, PRESSURE-BALANCING ANTI-SCALD VALVES
- USE OF WATER WISE DRAUGHT-TOLERANT NATIVE PLANT MATERIAL
- MOISTURE SENSING IRRIGATION-OVERRIDE SYSTEM

LIGHT POLLUTION REDUCTION:

- FULL CUTOFF-LUMINAIRES
- LOW REFLECTANCE SURFACES

ENVIRONMENTAL POLLUTION REDUCTION:

- LOW VOC ADHESIVES, SEALANTS, PAINTS, COATINGS, CARPETS & OTHER PRODUCTS
- STORAGE & COLLECTION OF RECYCLABLES. BUILDING'S RECYCLING BINS ARE CENTRALIZED & EASILY ACCESSIBLE.
- CONSTRUCTION ACTIVITY POLLUTION PREVENTION: DIVERSION OF MATERIALS FROM LANDFILL THROUGH REUSE & RECYCLING

NATIVE LANDSCAPING:

- USE OF NATIVE PLANTS FOSTERS SOIL HEALTH, REDUCES RUNOFF & POLLUTION, PREVENTS & REUSES PLANT WASTE, & CONSERVES WATER
- REQUIRES LITTLE OR NO IRRIGATION, REDUCING COST OF LANDSCAPING AS WELL AS WATER USE

Development Density and Community Connectivity Scores with a mile
1 SUPERMARKET
4 RESTAURANTS
1 PARKS
2 PHARMACIES
1 BUS STOP
Services with a mile
1.3 RESTAURANTS
1 THEATER
1 BOOK STORE
4 FITNESS
7 BANES
5 PUBLIC PARK
10 HAIR SALON
2 SUPERMARKET
1 PUBLIC LIBRARY
3 PHARMACIES
1 HARDWARE STORE
5 CLOTHING STORE
2 LAUNDRIES/CLEANERS
7 DENTIST
5 POST OFFICE/DELIVERY
1 CAL TRAIN

GREEN SHEET

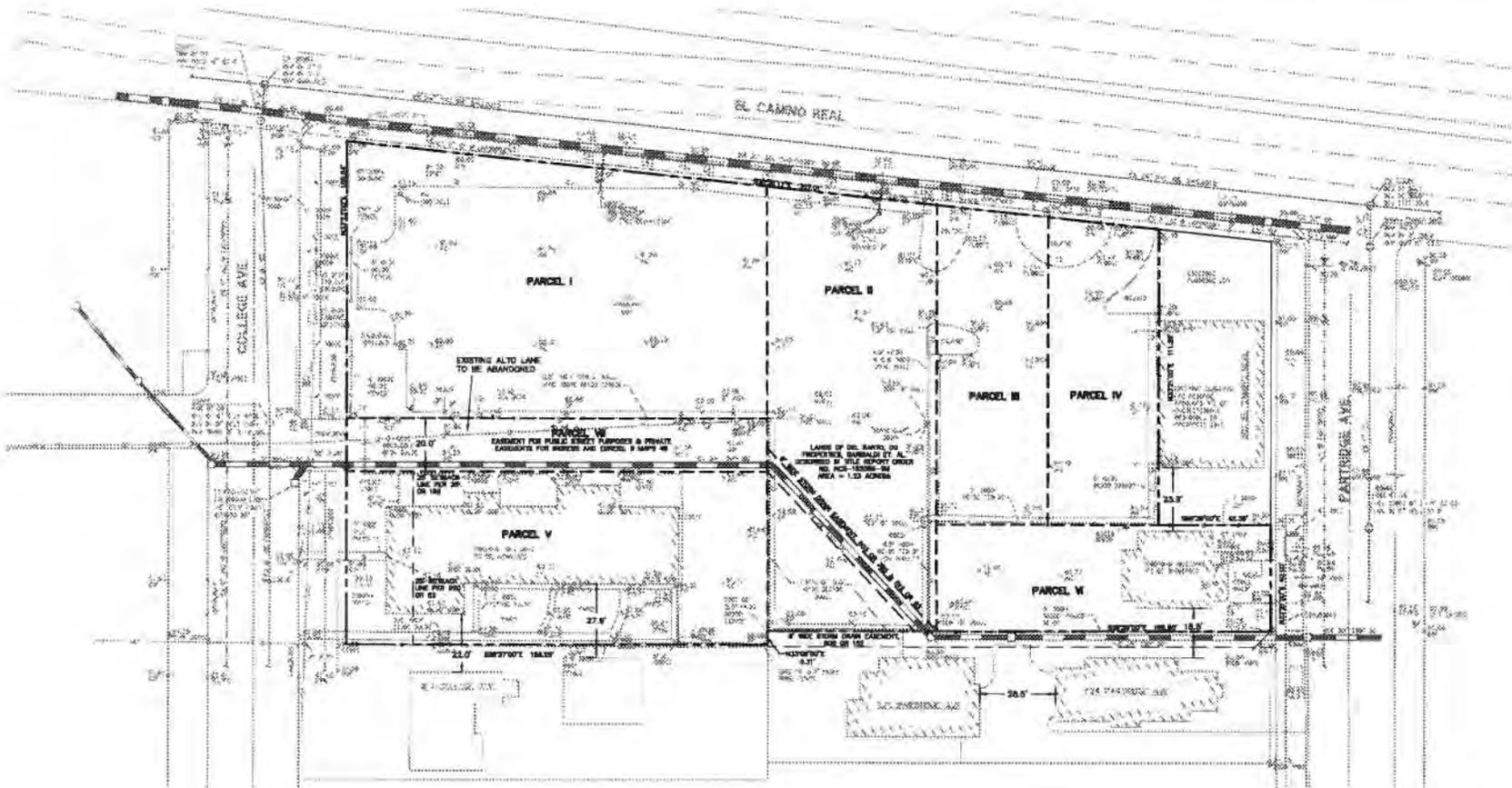
SEPTEMBER 21, 2011

PROJECT NO: 221.009



5845 CHIMES DRIVE
PLEASANTON, CA 94588
925.837.8776
925.837.2445 Fax

DRAWING NAME: 389 EL CAMINO REAL (MAY) 11/14/14 SHOWN AS EXISTING PLAN
 DATE: 08-20-12 PLOTTED BY: BCK

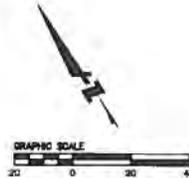


UTILITY STATEMENT

THE UNDERGROUND UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY INFORMATION AND/OR RECORD DATA AND ARE APPROXIMATE ONLY. THE SURVEYOR MAKES NO GUARANTEE THAT THE UNDERGROUND UTILITIES SHOWN COMPRISE ALL SUCH UTILITIES IN THE AREA EITHER IN SERVICE OR ABANDONED. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES. ACTUAL LOCATION AND SIZE, TOGETHER WITH THE PRESENCE OF ANY ADDITIONAL UTILITY LINES NOT SHOWN ON THIS PLAN, SHALL BE VERIFIED IN THE FIELD BY THE CONTRACTOR DURING CONSTRUCTION.

POSTING CONDITIONS NOTES

- 3" TREE'S IN 3.5' X 2.5' TREE WELLS (TYPICAL) ALONG EL CAMINO REAL.
- SEE COVENANTS, CONDITIONS, RESTRICTIONS AND EASEMENTS RECORDED 25 ON 188, AND 420 OR 32, FOR MORE SETBACK RESTRICTIONS.



389 EL CAMINO REAL
TENTATIVE MAP
EXISTING CONDITIONS PLAN

SAN MATEO COUNTY
MENLO PARK

CALIFORNIA

389 EL CAMINO REAL, SUITE 200
REDWOOD CITY, CA 94063
940/422-8300
940/422-8300 (FAX)

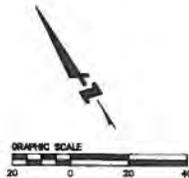
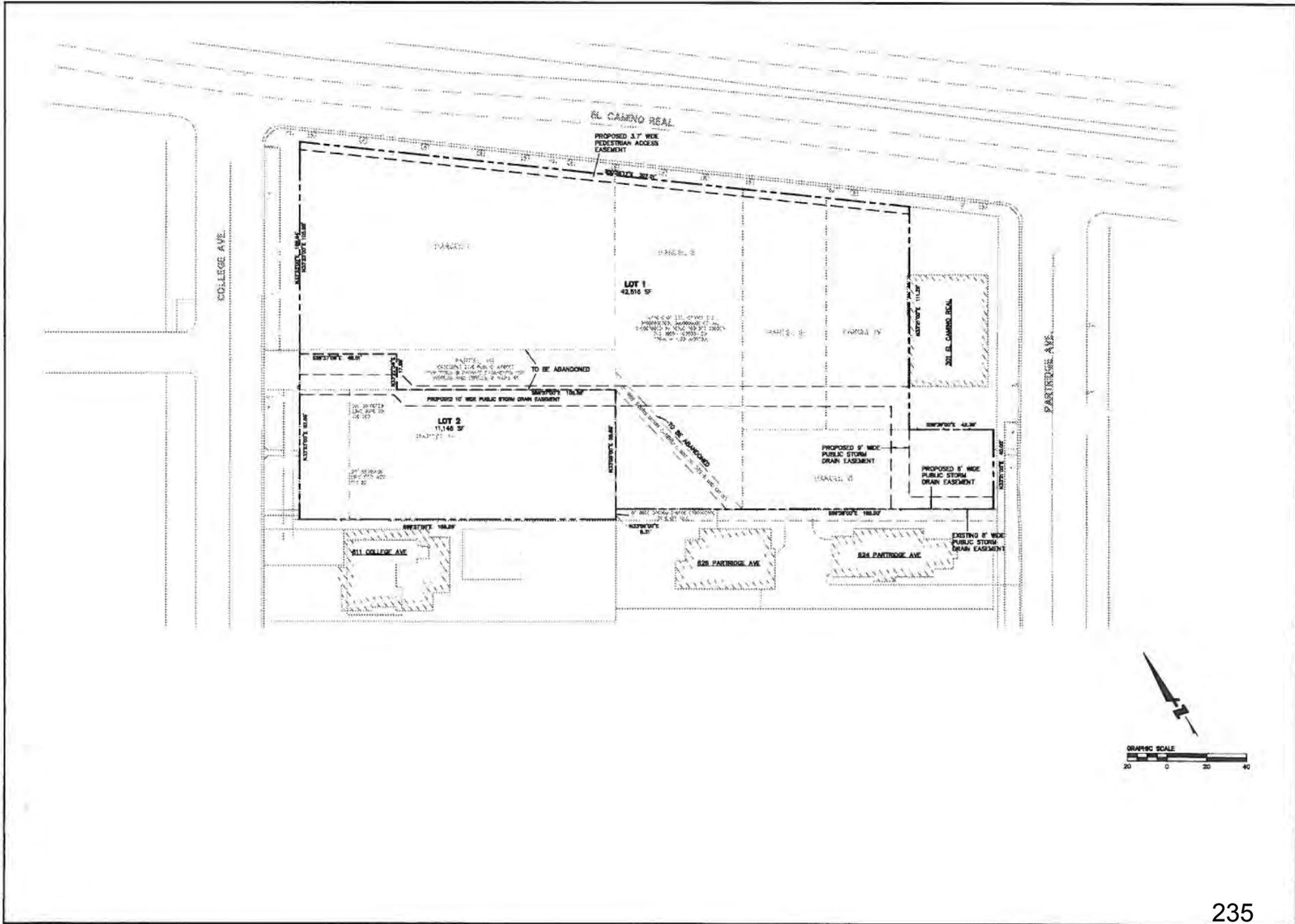
BKF
BUSINESS SURVEYORS ASSOCIATION

No.	Revisions

Date: 08/20/12	No.
Scale: AS SHOWN	
Design: CJA	
Drawn: JWP	
Approved: CJA	
Job No.: 2010008	

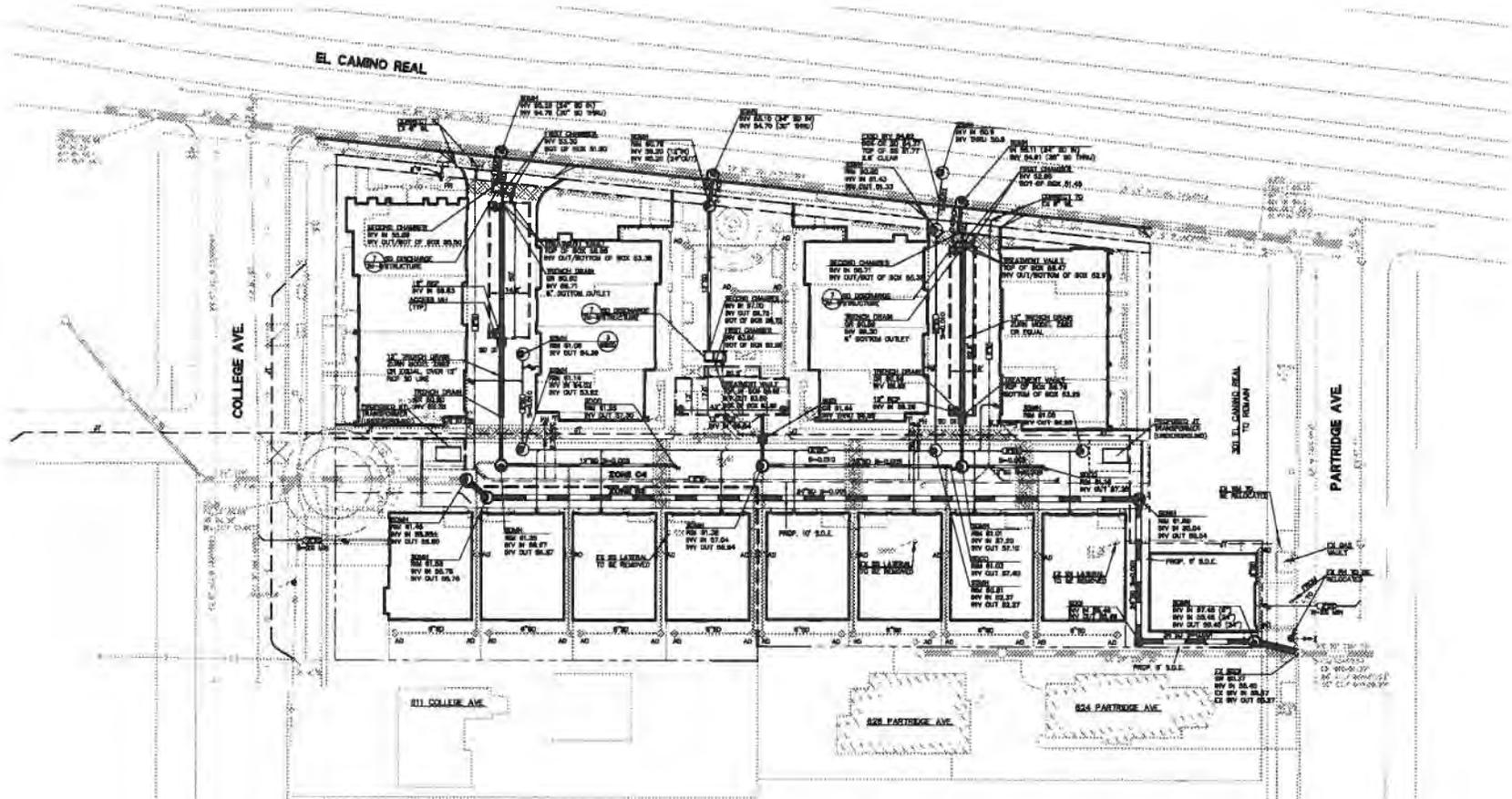
TM-2

DRAWING NAME: 389 EL CAMINO REAL TENTATIVE MAP - LOTS 1 & 2
 DATE: 08/09/12
 DRAWN BY: JAC



 BKF ENGINEERING SURVEYING & PLANNING	
200 SHORELINE DRIVE, SUITE 200 REDWOOD CITY, CA 94060 (650) 452-4300 (FAX)	
CALIFORNIA	
389 EL CAMINO REAL TENTATIVE MAP PROPOSED LOTS	
SAN MATEO COUNTY	
MENLO PARK	
Date: 08/09/12 Scale: AS SHOWN Design: JAC Drawn: JAC Approved: CAI Job No: 2010004	Revision:
No.	Description

Drawn by	Checked by	Reviewed by
Date	Scale	Sheet No.
Drawn	Checked	Reviewed
Approved	Scale	Sheet No.
Sheet Number		
TM-7		



NOTES

- 1) EXISTING UTILITIES SHOWN ARE BASED ON FIELD OBSERVATION AND RECORD DRAWINGS. THEY ARE NOT MEANT TO BE A FULL CATALOG OF ACTUAL CONDITIONS.
- 2) EXISTING UTILITIES SHOWN ARE TO REMAIN UNLESS OTHERWISE NOTED ON PLANS.
- 3) EXISTING FIRE HYDRANTS SHALL REMAIN. IF PROPOSED IMPROVEMENTS CONFLICT WITH EXISTING FIRE HYDRANTS, THE HYDRANT SHALL BE RELOCATED.
- 4) PROPOSED WATER AND SANITARY SEWER SYSTEMS SHALL CONFORM TO CALTRANS AND WEST BAY SANITARY DISTRICT STANDARD SPECIFICATIONS AND DETAILS, RESPECTIVELY.
- 5) REMOVE AND REPLACE ALL FRONTAGE IMPROVEMENTS ALONG THE ENTIRE PROPERTY FRONTAGE ON COLLEGE AVENUE AND PARTRIDGE AVENUE PER CITY STANDARDS.
- 6) REMOVE AND REPLACE ALL FRONTAGE IMPROVEMENTS ALONG THE ENTIRE PROPERTY FRONTAGE ON EL CAMINO REAL PER CALTRANS STANDARDS.



Drawn by: J. M. G. / Checked by: J. M. G. / Reviewed by: J. M. G. / Date: 08-08-12 / Scale: AS SHOWN / Sheet No.: 001

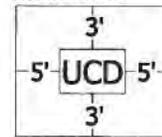
UTILITIES
PLEASE CONFIRM
TIE IN LOCATIONS

INTENT DRAWING
NOT FOR CONSTRUCTION

LEGEND:

-  PROPOSED JOINT TRENCH
-  PROPOSED SERVICE TRENCH
-  EXISTING OVERHEAD TO BE REMOVED
-  4'-6" x 8'-6" x 7'-6" UCD TRANSFORMER (PG&E)
-  EXISTING POLE TO REMAIN
-  EXISTING POLE TO BE REMOVED

Working Space & easement requirements for #7 box:



Maintain a clear, level working space as shown, per PG&E drawing 051122.

Electric Conduit
Minimum Bend Radius for New Construction Service

Conduit Diameter	Vertical Radius	Horizontal Radius
3"	24"	36"
4"	36"	54"
6"	48"	72"

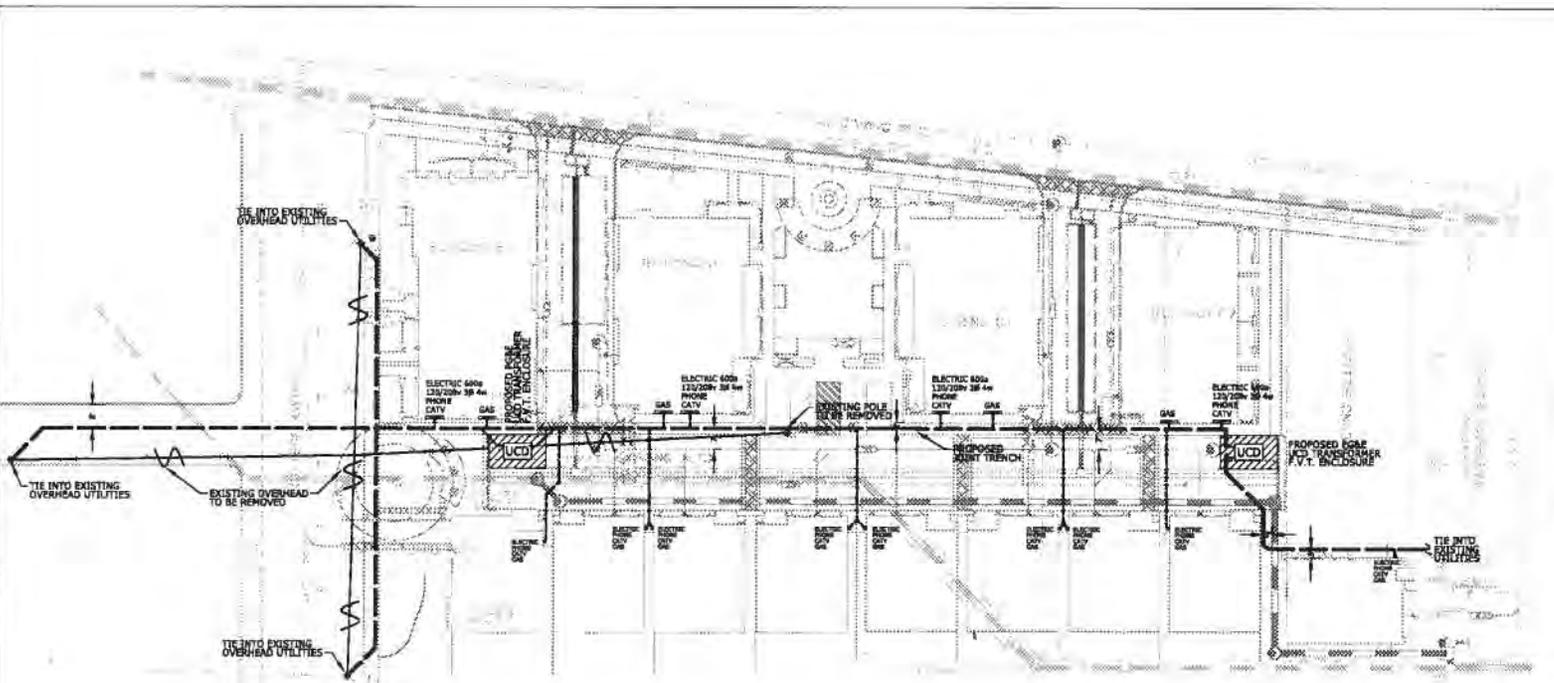
Electric Conduit
Minimum Bend Radius for New Construction Primary

Conduit Diameter	Vertical Radius	Horizontal Radius
3"	48"	72"
4"	60"	90"
6"	84"	126"
8"	112"	168"
10"	140"	210"

NOTE:
330° DEGREES MAX BENDS IN ANY CONDUIT RUN



Sheet Index
JT-1 JOINT TRENCH TITLE SHEET
JT-2 JOINT TRENCH INTENT



REGISTERED PROFESSIONAL ENGINEER
 CALIFORNIA
 JOINT TRENCH INTENT
 389 EL CAMINO REAL
 BKF ENGINEERS
 Menlo Park
 RGA
 RGA DESIGN, LLC.
 UTILITY CONSULTANTS & ENGINEERS - ARCHITECTURAL DESIGN
 1400 CALIFORNIA AVENUE, SUITE 200, MENLO PARK, CA 94025
 TEL: (650) 321-9000 FAX: (650) 321-9007
 PROJECT NO. 11-306
 DATE 11-20
 BY S. HANDESTER
 CHECKED BY B. CASTILLO
 DESIGNED BY D. VOORHIES
 DATE PLOTTED 9-21-2011
 SHEET NO. JT-2
 OF 2

PLANT LIST:

SYMBOL	QUANTITY	SYMBOL	SCIENTIFIC NAME	COMMON NAME	SIZE	UC	WATER	SHADE	CA NATIVE/
						SPACING	REQ.		IND.
T	1	1	<i>Abies Fraseri</i>	Fraser Fir	24' Dia	Ab 8' x 8'	MOD	SHDL	---
	1	1	<i>Cercis canadensis Varied Form</i>	Redbud	8' Gal	Ab 8' x 8'	MOD	SHDL	---
	1	1	<i>Cornus amomum</i>	Spicebush	8' Gal	Ab 8' x 8'	MOD	SHDL	---
	1	1	<i>Hydrangea arborescens</i>	Hydrangea	8' Gal	Ab 8' x 8'	MOD	SHDL	---
	1	1	<i>Philadelphus floridus</i>	Flora	8' Gal	Ab 8' x 8'	MOD	SHDL	---
S	1	1	<i>Amelanchier alnifolia</i>	Shadbush	8' Gal	Ab 8' x 8'	MOD	SHDL	---
	1	1	<i>Amelanchier canadensis</i>	Shadbush	8' Gal	Ab 8' x 8'	MOD	SHDL	---
	1	1	<i>Amelanchier florida</i>	Shadbush	8' Gal	Ab 8' x 8'	MOD	SHDL	---
	1	1	<i>Amelanchier spicata</i>	Shadbush	8' Gal	Ab 8' x 8'	MOD	SHDL	---
	1	1	<i>Amelanchier spicata</i>	Shadbush	8' Gal	Ab 8' x 8'	MOD	SHDL	---
T	1	1	<i>Amelanchier florida</i>	Shadbush	8' Gal	Ab 8' x 8'	MOD	SHDL	---
	1	1	<i>Amelanchier florida</i>	Shadbush	8' Gal	Ab 8' x 8'	MOD	SHDL	---
	1	1	<i>Amelanchier florida</i>	Shadbush	8' Gal	Ab 8' x 8'	MOD	SHDL	---
	1	1	<i>Amelanchier florida</i>	Shadbush	8' Gal	Ab 8' x 8'	MOD	SHDL	---
	1	1	<i>Amelanchier florida</i>	Shadbush	8' Gal	Ab 8' x 8'	MOD	SHDL	---
	1	1	<i>Amelanchier florida</i>	Shadbush	8' Gal	Ab 8' x 8'	MOD	SHDL	---
	1	1	<i>Amelanchier florida</i>	Shadbush	8' Gal	Ab 8' x 8'	MOD	SHDL	---
	1	1	<i>Amelanchier florida</i>	Shadbush	8' Gal	Ab 8' x 8'	MOD	SHDL	---
	1	1	<i>Amelanchier florida</i>	Shadbush	8' Gal	Ab 8' x 8'	MOD	SHDL	---
	1	1	<i>Amelanchier florida</i>	Shadbush	8' Gal	Ab 8' x 8'	MOD	SHDL	---
	1	1	<i>Amelanchier florida</i>	Shadbush	8' Gal	Ab 8' x 8'	MOD	SHDL	---
	1	1	<i>Amelanchier florida</i>	Shadbush	8' Gal	Ab 8' x 8'	MOD	SHDL	---
	1	1	<i>Amelanchier florida</i>	Shadbush	8' Gal	Ab 8' x 8'	MOD	SHDL	---
	1	1	<i>Amelanchier florida</i>	Shadbush	8' Gal	Ab 8' x 8'	MOD	SHDL	---
	1	1	<i>Amelanchier florida</i>	Shadbush	8' Gal	Ab 8' x 8'	MOD	SHDL	---



ARBUTUS MENZIESII



ACER PALMATUM 'SANGO KAKU'



CERCIS CANADENSIS 'FOREST PANSY'



RAPPOLEPIS MAJESTIC BEAUTY STANDARD



ARCTOSTAPHYLOS SUNSET



BUXUS M. JAPONICA 'GREEN BEAUTY'



WATER USE RATING LEGEND

IRrigation water use categories are based on the amount of water needed to grow plants in a landscape. The amount of water needed is determined by the plant's water requirements, the soil type, and the climate. The amount of water needed is determined by the plant's water requirements, the soil type, and the climate.

MODERATE WATER: Plants that require moderate water. These plants are suitable for most landscapes and require regular watering.

OCcAsIONAL WATER: Plants that require occasional water. These plants are suitable for drought-tolerant landscapes and require minimal watering.

NO ADDITIONAL WATER: Plants that require no additional water. These plants are suitable for xeriscaping and require no watering.

PERCENTAGE OF PLANTS MEETING LOW WATER REQUIREMENTS: 100% MEETS OR BETTER, 75% MEETS OR BETTER, 50% MEETS OR BETTER, 25% MEETS OR BETTER, 0% MEETS OR BETTER.

ON-SITE TOTAL PLANTS: 100% MEETS OR BETTER, 75% MEETS OR BETTER, 50% MEETS OR BETTER, 25% MEETS OR BETTER, 0% MEETS OR BETTER.

PERCENTAGE OF PLANTS MEETING LOW WATER REQUIREMENTS: 100% MEETS OR BETTER, 75% MEETS OR BETTER, 50% MEETS OR BETTER, 25% MEETS OR BETTER, 0% MEETS OR BETTER.

ON-SITE TOTAL PLANTS: 100% MEETS OR BETTER, 75% MEETS OR BETTER, 50% MEETS OR BETTER, 25% MEETS OR BETTER, 0% MEETS OR BETTER.



ERIGERON KARVINKORHICUS



OPHIOPOGON PLANTSCAPUS



PHORMIUM TENAX MAUI



SALVIA CLEAVLANDII



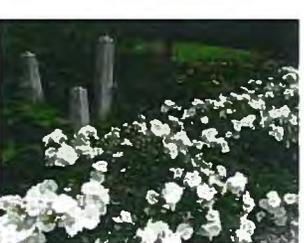
SALVIA SPYTHACEA



HARDENBERGIA VIOLACEA

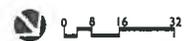
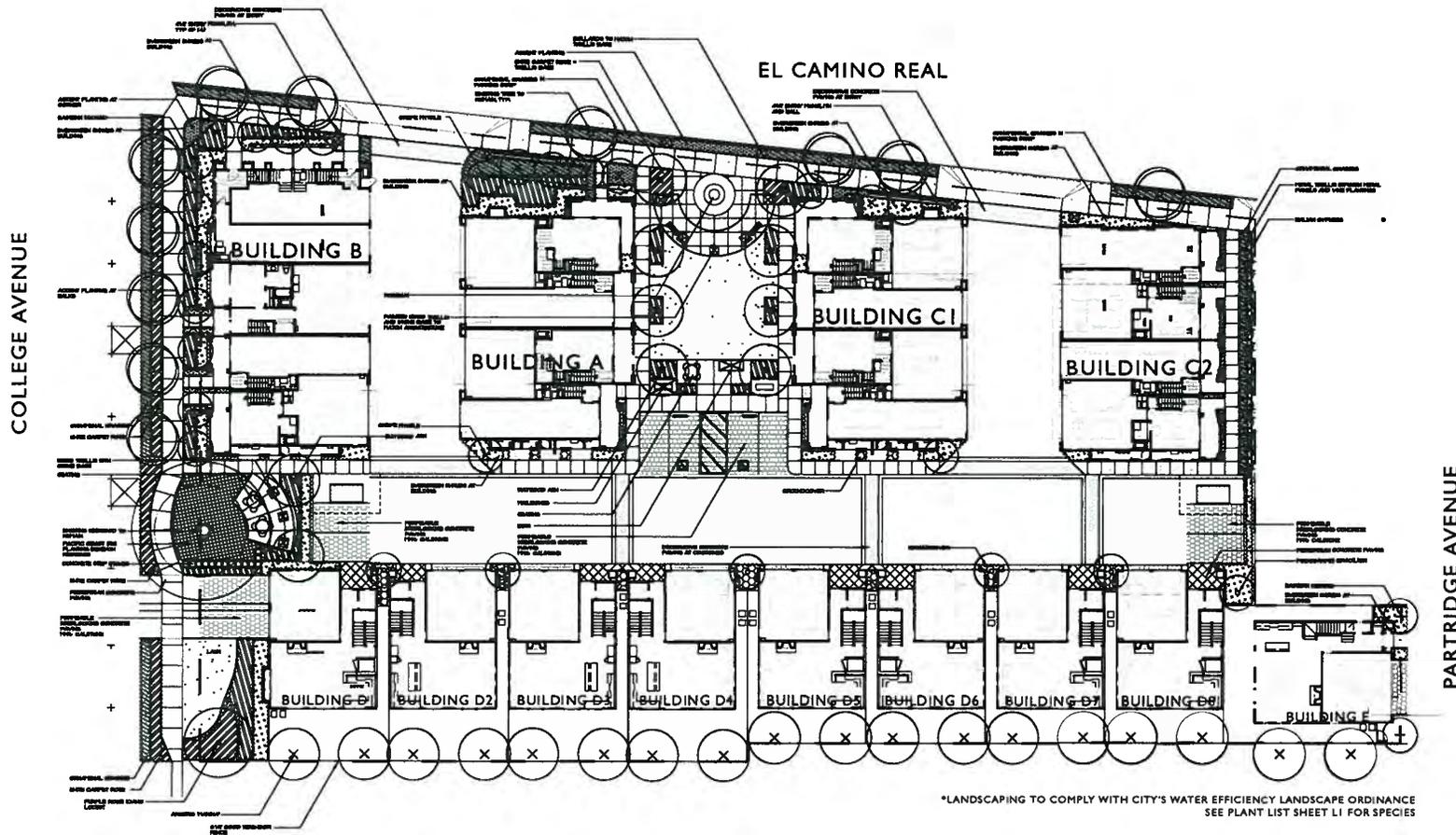


ROSA FLOWER CARPET YELLOW



ROSA FLOWER CARPET WHITE

MATTESON DEVELOPMENT PARTNERS, INC



MATTESON DEVELOPMENT PARTNERS, INC

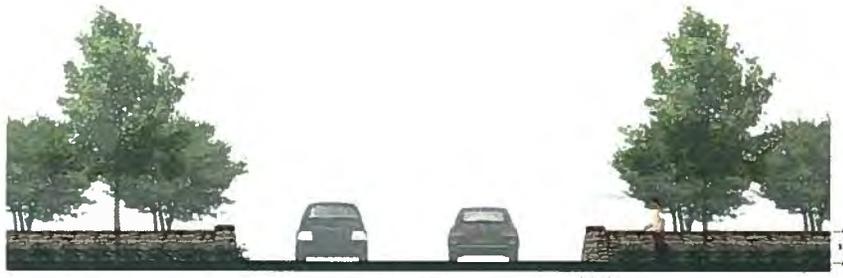
JUNE 8, 2012



GATES
+ ASSOCIATES

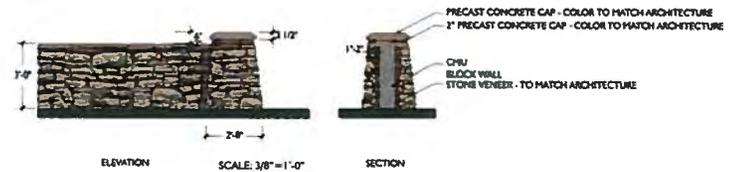
389 EL CAMINO REAL MENLO PARK, CALIFORNIA

CONCEPTUAL LANDSCAPE PLAN **L2**

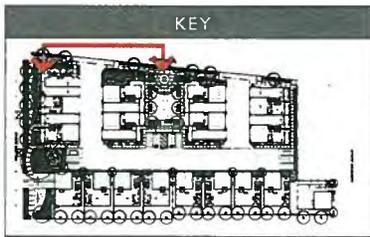


SCALE: 3/16" = 1'-0"

ENTRY WALLS ALONG EL CAMINO REAL



ENLARGEMENT
ENTRY WALLS ALONG EL CAMINO REAL



COMMON GREEN AT EL CAMINO REAL

MATTESON DEVELOPMENT PARTNERS, INC

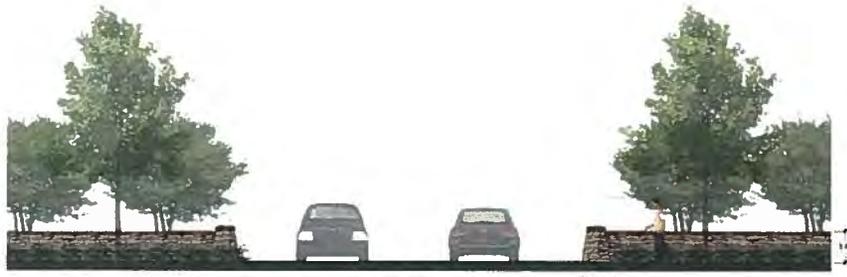
JUNE 2012



GATES
+ ASSOCIATES

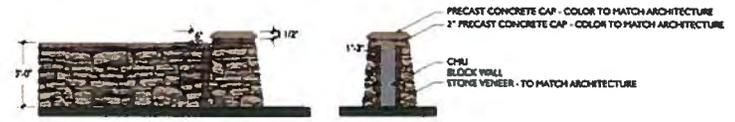
389 EL CAMINO REAL MENLO PARK, CALIFORNIA

ELEVATIONS L3



SCALE: 3/16"=1'-0"

ENTRY WALLS ALONG EL CAMINO REAL

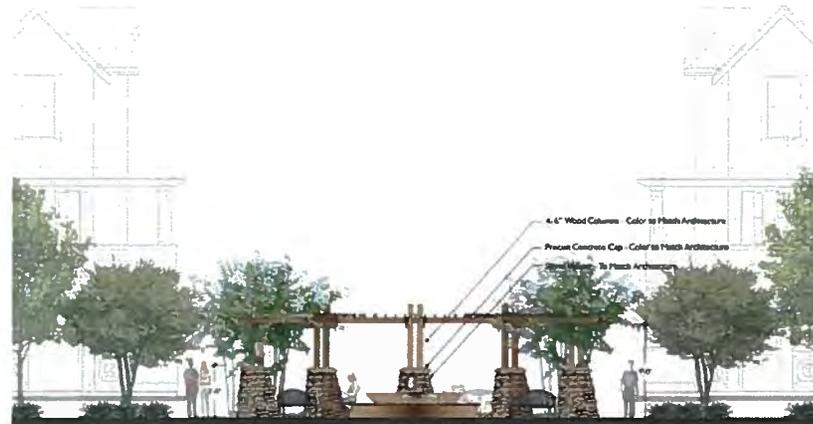
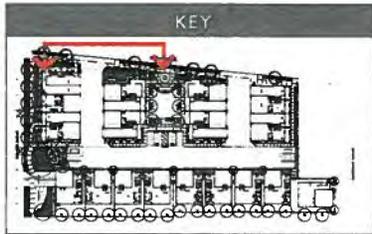


ELEVATION

SCALE: 3/8"=1'-0"

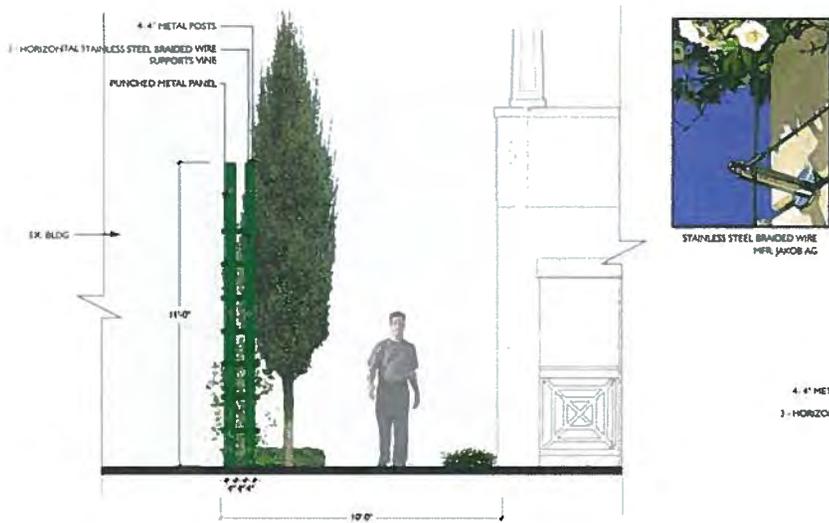
SECTION

ENLARGEMENT
ENTRY WALLS ALONG EL CAMINO REAL

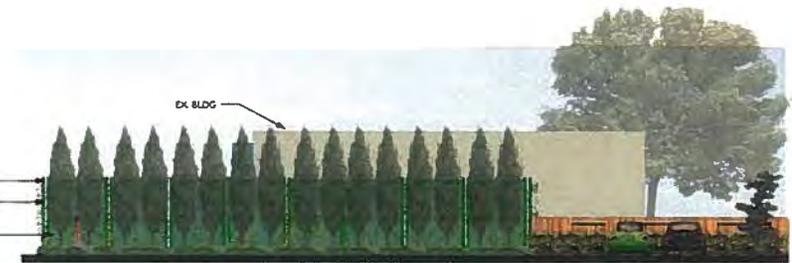


COMMON GREEN AT EL CAMINO REAL

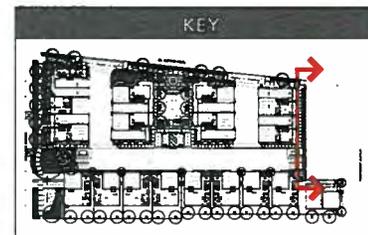
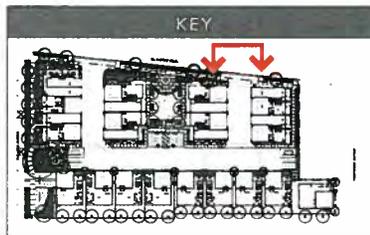




TRELLIS AT BUILDING C FACING EL CAMINO - VIEW FROM EL CAMINO
SCALE: 1/2"=1'-0"



TRELLIS AT BUILDING C FACING EL CAMINO - ELEVATION
SCALE: 1/4"=1'-0"



EXISTING TREES

Number	Scientific Name	dbh HT (ft)	Notes/Action
1	Calocedrus	14" 39'-40'	
2	Cupressus sempervirens	36" 10'	To Be Removed
3	Crataegus	4" 29'-30'	
4	Crataegus	4" 29'-30'	
5	Crataegus	4" 29'-30'	
6	Crataegus	4" 29'-30'	
7	Platanus acerifolia	4" 10'-29'	
8	Platanus acerifolia	4" 10'-29'	To Be Removed
9	Platanus acerifolia	4" 10'-29'	To Be Removed
10	Platanus acerifolia	4" 10'-29'	To Be Removed
11	Platanus acerifolia	4" 10'-29'	To Be Removed
12	Platanus acerifolia	4" 10'-29'	To Be Removed
13	Platanus acerifolia	4" 10'-29'	To Be Removed
14	Platanus acerifolia	4" 10'-29'	To Be Removed
15	Platanus acerifolia	4" 10'-29'	To Be Removed
16	Platanus acerifolia	4" 10'-29'	To Be Removed
17	Platanus acerifolia	4" 10'-29'	To Be Removed
18	Platanus acerifolia	4" 10'-29'	To Be Removed
19	Platanus acerifolia	14" 30'	
20	Platanus acerifolia	14" 30'	
21	Platanus acerifolia	14" 30'	Diseased
22	Pithecoporus unclatus	2" 10'	Multi-trunked

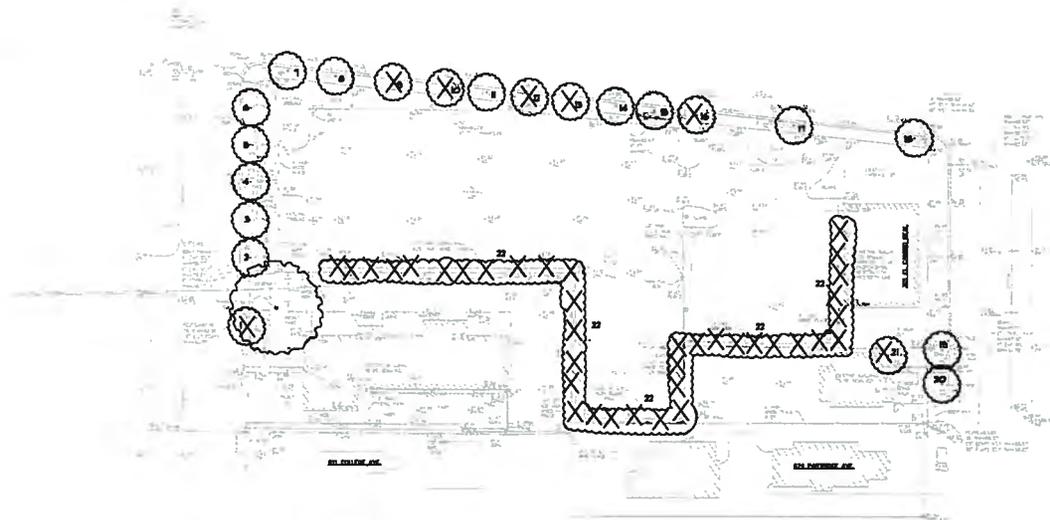
TREE COUNT

TREES TO BE REMOVED: 8
 PROPOSED TREES: 14

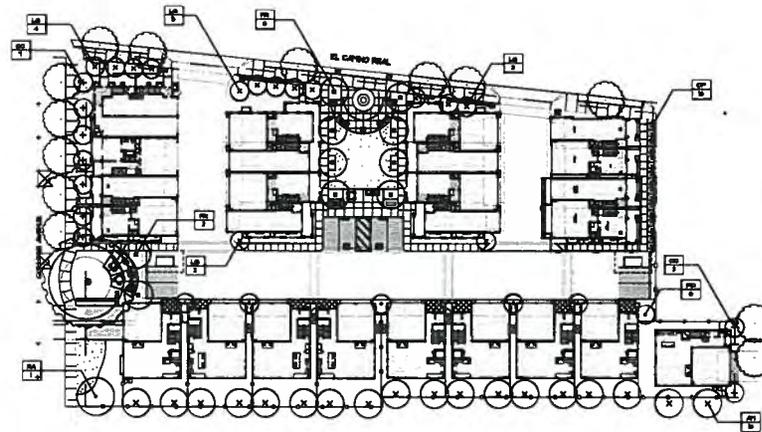
TREE LEGEND

-  EXISTING TREE TO BE REMOVED
-  EXISTING TREE TO REMAIN
-  AM AREBUTUS THURNA
-  CC CERCIS CANADENSIS 'FORREST FANBY'
-  CP CUPRESSUS SEMPERVIRENS
-  LO LAENOSTROEPA INDICA 'HUSKODER'
-  PD PODOCARPUS GRACILIOR
-  PR PRUNUS 'RAYBROOK'
-  RA ROBINIA X APEKUSA 'PURPLE ROBE'

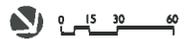
SEE SHEET L1 FOR COMPLETE PLANT LIST



EXISTING SITE PLAN



PROPOSED SITE PLAN



MATTESON DEVELOPMENT PARTNERS, INC

JUNE 8, 2012



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TREE PLAN L5



MATTESON DEVELOPEMENT PARTNERS, INC

JUNE 8, 2012



GATES
+ ASSOCIATES

389 EL CAMINO REAL MENLO PARK, CALIFORNIA

SITE FURNITURE L6



Effective: January 1, 2009

West's Annotated California Codes Currentness

Government Code (Refs & Annos)

Title 7. Planning and Land Use (Refs & Annos)

Division 1. Planning and Zoning (Refs & Annos)

Chapter 4.3. Density Bonuses and Other Incentives (Refs & Annos)

→ § 65915. Applicants seeking density bonus; incentives or concessions for lower income housing units and child care facilities; conditions, agreements and submission requirements; duties of local officials

(a) When an applicant seeks a density bonus for a housing development within, or for the donation of land for housing within, the jurisdiction of a city, county, or city and county, that local government shall provide the applicant with incentives or concessions for the production of housing units and child care facilities as prescribed in this section. All cities, counties, or cities and counties shall adopt an ordinance that specifies how compliance with this section will be implemented. Failure to adopt an ordinance shall not relieve a city, county, or city and county from complying with this section.

(b)(1) A city, county, or city and county shall grant one density bonus, the amount of which shall be as specified in subdivision (f), and incentives or concessions, as described in subdivision (d), when an applicant for a housing development seeks and agrees to construct a housing development, excluding any units permitted by the density bonus awarded pursuant to this section, that will contain at least any one of the following:

(A) Ten percent of the total units of a housing development for lower income households, as defined in Section 50079.5 of the Health and Safety Code.

(B) Five percent of the total units of a housing development for very low income households, as defined in Section 50105 of the Health and Safety Code.

(C) A senior citizen housing development, as defined in Sections 51.3 and 51.12 of the Civil Code, or mobilehome park that limits residency based on age requirements for housing for older persons pursuant to Section 798.76 or 799.5 of the Civil Code.

(D) Ten percent of the total dwelling units in a common interest development as defined in Section 1351 of the Civil Code for persons and families of moderate income, as defined in Section 50093 of the Health and Safety Code, provided that all units in the development are offered to the public for purchase.

(2) For purposes of calculating the amount of the density bonus pursuant to subdivision (f), the applicant who requests a density bonus pursuant to this subdivision shall elect whether the bonus shall be awarded on the basis of subparagraph (A), (B), (C), or (D) of paragraph (1).

(3) For the purposes of this section, "total units" or "total dwelling units" does not include units added by a density bonus awarded pursuant to this section or any local law granting a greater density bonus.

(c)(1) An applicant shall agree to, and the city, county, or city and county shall ensure, continued affordability of all low- and very low income units that qualified the applicant for the award of the density bonus for 30 years or a longer period of time if

required by the construction or mortgage financing assistance program, mortgage insurance program, or rental subsidy program. Rents for the lower income density bonus units shall be set at an affordable rent as defined in Section 50053 of the Health and Safety Code. Owner-occupied units shall be available at an affordable housing cost as defined in Section 50052.5 of the Health and Safety Code.

(2) An applicant shall agree to, and the city, county, or city and county shall ensure that, the initial occupant of the moderate-income units that are directly related to the receipt of the density bonus in the common interest development, as defined in Section 1351 of the Civil Code, are persons and families of moderate income, as defined in Section 50093 of the Health and Safety Code, and that the units are offered at an affordable housing cost, as that cost is defined in Section 50052.5 of the Health and Safety Code. The local government shall enforce an equity sharing agreement, unless it is in conflict with the requirements of another public funding source or law. The following apply to the equity sharing agreement:

(A) Upon resale, the seller of the unit shall retain the value of any improvements, the downpayment, and the seller's proportionate share of appreciation. The local government shall recapture any initial subsidy, as defined in subparagraph (B), and its proportionate share of appreciation, as defined in subparagraph (C), which amount shall be used within five years for any of the purposes described in subdivision (e) of Section 33334.2 of the Health and Safety Code that promote home ownership.

(B) For purposes of this subdivision, the local government's initial subsidy shall be equal to the fair market value of the home at the time of initial sale minus the initial sale price to the moderate-income household, plus the amount of any downpayment assistance or mortgage assistance. If upon resale the market value is lower than the initial market value, then the value at the time of the resale shall be used as the initial market value.

(C) For purposes of this subdivision, the local government's proportionate share of appreciation shall be equal to the ratio of the local government's initial subsidy to the fair market value of the home at the time of initial sale.

(d)(1) An applicant for a density bonus pursuant to subdivision (b) may submit to a city, county, or city and county a proposal for the specific incentives or concessions that the applicant requests pursuant to this section, and may request a meeting with the city, county, or city and county. The city, county, or city and county shall grant the concession or incentive requested by the applicant unless the city, county, or city and county makes a written finding, based upon substantial evidence, of any of the following:

(A) The concession or incentive is not required in order to provide for affordable housing costs, as defined in Section 50052.5 of the Health and Safety Code, or for rents for the targeted units to be set as specified in subdivision (c).

(B) The concession or incentive would have a specific adverse impact, as defined in paragraph (2) of subdivision (d) of Section 65589.5, upon public health and safety or the physical environment or on any real property that is listed in the California Register of Historical Resources and for which there is no feasible method to satisfactorily mitigate or avoid the specific adverse impact without rendering the development unaffordable to low- and moderate-income households.

(C) The concession or incentive would be contrary to state or federal law.

(2) The applicant shall receive the following number of incentives or concessions:

(A) One incentive or concession for projects that include at least 10 percent of the total units for lower income households, at least 5 percent for very low income households, or at least 10 percent for persons and families of moderate income in a common interest development.

(B) Two incentives or concessions for projects that include at least 20 percent of the total units for lower income households, at least 10 percent for very low income households, or at least 20 percent for persons and families of moderate income in a common interest development.

(C) Three incentives or concessions for projects that include at least 30 percent of the total units for lower income households, at least 15 percent for very low income households, or at least 30 percent for persons and families of moderate income in a common interest development.

(3) The applicant may initiate judicial proceedings if the city, county, or city and county refuses to grant a requested density bonus, incentive, or concession. If a court finds that the refusal to grant a requested density bonus, incentive, or concession is in violation of this section, the court shall award the plaintiff reasonable attorney's fees and costs of suit. Nothing in this subdivision shall be interpreted to require a local government to grant an incentive or concession that has a specific, adverse impact, as defined in paragraph (2) of subdivision (d) of Section 65589.5, upon health, safety, or the physical environment, and for which there is no feasible method to satisfactorily mitigate or avoid the specific adverse impact. Nothing in this subdivision shall be interpreted to require a local government to grant an incentive or concession that would have an adverse impact on any real property that is listed in the California Register of Historical Resources. The city, county, or city and county shall establish procedures for carrying out this section, that shall include legislative body approval of the means of compliance with this section.

(e)(1) In no case may a city, county, or city and county apply any development standard that will have the effect of physically precluding the construction of a development meeting the criteria of subdivision (b) at the densities or with the concessions or incentives permitted by this section. An applicant may submit to a city, county, or city and county a proposal for the waiver or reduction of development standards that will have the effect of physically precluding the construction of a development meeting the criteria of subdivision (b) at the densities or with the concessions or incentives permitted under this section, and may request a meeting with the city, county, or city and county. If a court finds that the refusal to grant a waiver or reduction of development standards is in violation of this section, the court shall award the plaintiff reasonable attorney's fees and costs of suit. Nothing in this subdivision shall be interpreted to require a local government to waive or reduce development standards if the waiver or reduction would have a specific, adverse impact, as defined in paragraph (2) of subdivision (d) of Section 65589.5, upon health, safety, or the physical environment, and for which there is no feasible method to satisfactorily mitigate or avoid the specific adverse impact. Nothing in this subdivision shall be interpreted to require a local government to waive or reduce development standards that would have an adverse impact on any real property that is listed in the California Register of Historical Resources, or to grant any waiver or reduction that would be contrary to state or federal law.

(2) A proposal for the waiver or reduction of development standards pursuant to this subdivision shall neither reduce nor increase the number of incentives or concessions to which the applicant is entitled pursuant to subdivision (d).

(f) For the purposes of this chapter, "density bonus" means a density increase over the otherwise maximum allowable residential density as of the date of application by the applicant to the city, county, or city and county. The applicant may elect to accept a lesser percentage of density bonus. The amount of density bonus to which the applicant is entitled shall vary according to the amount by which the percentage of affordable housing units exceeds the percentage established in subdivision (b).

(1) For housing developments meeting the criteria of subparagraph (A) of paragraph (1) of subdivision (b), the density bonus shall be calculated as follows:

Percentage Low-Income Units	Percentage Density Bonus
10	20
11	21.5
12	23
13	24.5
14	26

15	27.5
17	30.5
18	32
19	33.5
20	35

(2) For housing developments meeting the criteria of subparagraph (B) of paragraph (1) of subdivision (b), the density bonus shall be calculated as follows:

Percentage Very Low Income Units	Percentage Density Bonus
5	20
6	22.5
7	25
8	27.5
9	30
10	32.5
11	35

(3) For housing developments meeting the criteria of subparagraph (C) of paragraph (1) of subdivision (b), the density bonus shall be 20 percent of the number of senior housing units.

(4) For housing developments meeting the criteria of subparagraph (D) of paragraph (1) of subdivision (b), the density bonus shall be calculated as follows:

Percentage Moderate-Income Units	Percentage Density Bonus
10	5
11	6
12	7
13	8
14	9
15	10
16	11
17	12
18	13
19	14
20	15
21	16
22	17
23	18
24	19



25	20
26	21
27	22
28	23
29	24
30	25
31	26
32	27
33	28
34	29
35	30
36	31
37	32
38	33
39	34
40	35

(5) All density calculations resulting in fractional units shall be rounded up to the next whole number. The granting of a density bonus shall not be interpreted, in and of itself, to require a general plan amendment, local coastal plan amendment, zoning change, or other discretionary approval.

(g)(1) When an applicant for a tentative subdivision map, parcel map, or other residential development approval donates land to a city, county, or city and county in accordance with this subdivision, the applicant shall be entitled to a 15-percent increase above the otherwise maximum allowable residential density for the entire development, as follows:

Percentage Very Low Income	Percentage Density Bonus
10	15
11	16
12	17
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(2) This increase shall be in addition to any increase in density mandated by subdivision (b), up to a maximum combined mandated density increase of 35 percent if an applicant seeks an increase pursuant to both this subdivision and subdivision (b). All density calculations resulting in fractional units shall be rounded up to the next whole number. Nothing in this subdivision shall be construed to enlarge or diminish the authority of a city, county, or city and county to require a developer to donate land as a condition of development. An applicant shall be eligible for the increased density bonus described in this subdivision if all of the following conditions are met:

(A) The applicant donates and transfers the land no later than the date of approval of the final subdivision map, parcel map, or residential development application.

(B) The developable acreage and zoning classification of the land being transferred are sufficient to permit construction of units affordable to very low income households in an amount not less than 10 percent of the number of residential units of the proposed development.

(C) The transferred land is at least one acre in size or of sufficient size to permit development of at least 40 units, has the appropriate general plan designation, is appropriately zoned with appropriate development standards for development at the density described in paragraph (3) of subdivision (c) of Section 65583.2, and is or will be served by adequate public facilities and infrastructure.

(D) The transferred land shall have all of the permits and approvals, other than building permits, necessary for the development of the very low income housing units on the transferred land, not later than the date of approval of the final subdivision map, parcel map, or residential development application, except that the local government may subject the proposed development to subsequent design review to the extent authorized by subdivision (i) of Section 65583.2 if the design is not reviewed by the local government prior to the time of transfer.

(E) The transferred land and the affordable units shall be subject to a deed restriction ensuring continued affordability of the units consistent with paragraphs (1) and (2) of subdivision (c), which shall be recorded on the property at the time of the transfer.

(F) The land is transferred to the local agency or to a housing developer approved by the local agency. The local agency may require the applicant to identify and transfer the land to the developer.

(G) The transferred land shall be within the boundary of the proposed development or, if the local agency agrees, within one-quarter mile of the boundary of the proposed development.

(H) A proposed source of funding for the very low income units shall be identified not later than the date of approval of the final subdivision map, parcel map, or residential development application.



(h)(1) When an applicant proposes to construct a housing development that conforms to the requirements of subdivision (b) and includes a child care facility that will be located on the premises of, as part of, or adjacent to, the project, the city, county, or city and county shall grant either of the following:

(A) An additional density bonus that is an amount of square feet of residential space that is equal to or greater than the amount of square feet in the child care facility.

(B) An additional concession or incentive that contributes significantly to the economic feasibility of the construction of the child care facility.

(2) The city, county, or city and county shall require, as a condition of approving the housing development, that the following occur:

(A) The child care facility shall remain in operation for a period of time that is as long as or longer than the period of time during which the density bonus units are required to remain affordable pursuant to subdivision (c).

(B) Of the children who attend the child care facility, the children of very low income households, lower income households, or families of moderate income shall equal a percentage that is equal to or greater than the percentage of dwelling units that are required for very low income households, lower income households, or families of moderate income pursuant to subdivision (b).

(3) Notwithstanding any requirement of this subdivision, a city, county, or a city and county shall not be required to provide a density bonus or concession for a child care facility if it finds, based upon substantial evidence, that the community has adequate child care facilities.

(4) "Child care facility," as used in this section, means a child day care facility other than a family day care home, including, but not limited to, infant centers, preschools, extended day care facilities, and schoolage child care centers.

(i) "Housing development," as used in this section, means a development project for five or more residential units. For the purposes of this section, "housing development" also includes a subdivision or common interest development, as defined in Section 1351 of the Civil Code, approved by a city, county, or city and county and consists of residential units or unimproved residential lots and either a project to substantially rehabilitate and convert an existing commercial building to residential use or the substantial rehabilitation of an existing multifamily dwelling, as defined in subdivision (d) of Section 65863.4, where the result of the rehabilitation would be a net increase in available residential units. For the purpose of calculating a density bonus, the residential units shall be on contiguous sites that are the subject of one development application, but do not have to be based upon individual subdivision maps or parcels. [FN1] The density bonus shall be permitted in geographic areas of the housing development other than the areas where the units for the lower income households are located.

(j) The granting of a concession or incentive shall not be interpreted, in and of itself, to require a general plan amendment, local coastal plan amendment, zoning change, or other discretionary approval. This provision is declaratory of existing law.

(k) For the purposes of this chapter, concession or incentive means any of the following:

(1) A reduction in site development standards or a modification of zoning code requirements or architectural design requirements that exceed the minimum building standards approved by the California Building Standards Commission as provided in Part 2.5 (commencing with Section 18901) of Division 13 of the Health and Safety Code, including, but not limited to, a reduction in setback and square footage requirements and in the ratio of vehicular parking spaces that would otherwise be required that results in identifiable, financially sufficient, and actual cost reductions.

(2) Approval of mixed use zoning in conjunction with the housing project if commercial, office, industrial, or other land uses will reduce the cost of the housing development and if the commercial, office, industrial, or other land uses are compatible with the housing project and the existing or planned development in the area where the proposed housing project will be located.

(3) Other regulatory incentives or concessions proposed by the developer or the city, county, or city and county that result in identifiable, financially sufficient, and actual cost reductions.

(l) Subdivision (k) does not limit or require the provision of direct financial incentives for the housing development, including the provision of publicly owned land, by the city, county, or city and county, or the waiver of fees or dedication requirements.

(m) Nothing in this section shall be construed to supersede or in any way alter or lessen the effect or application of the California Coastal Act (Division 20 (commencing with Section 30000) of the Public Resources Code).

(n) If permitted by local ordinance, nothing in this section shall be construed to prohibit a city, county, or city and county from granting a density bonus greater than what is described in this section for a development that meets the requirements of this section or from granting a proportionately lower density bonus than what is required by this section for developments that do not meet the requirements of this section.

(o) For purposes of this section, the following definitions shall apply:

(1) "Development standard" includes a site or construction condition, including, but not limited to, a height limitation, a setback requirement, a floor area ratio, an onsite open-space requirement, or a parking ratio that applies to a residential development pursuant to any ordinance, general plan element, specific plan, charter, or other local condition, law, policy, resolution, or regulation.

(2) "Maximum allowable residential density" means the density allowed under the zoning ordinance and land use element of the general plan, or if a range of density is permitted, means the maximum allowable density for the specific zoning range and land use element of the general plan applicable to the project. Where the density allowed under the zoning ordinance is inconsistent with the density allowed under the land use element of the general plan, the general plan density shall prevail.

(p)(1) Upon the request of the developer, no city, county, or city and county shall require a vehicular parking ratio, inclusive of handicapped and guest parking, of a development meeting the criteria of subdivision (b), that exceeds the following ratios:

(A) Zero to one bedroom: one onsite parking space.

(B) Two to three bedrooms: two onsite parking spaces.

(C) Four and more bedrooms: two and one-half parking spaces.

(2) If the total number of parking spaces required for a development is other than a whole number, the number shall be rounded up to the next whole number. For purposes of this subdivision, a development may provide "onsite parking" through tandem parking or uncovered parking, but not through onstreet parking.

(3) This subdivision shall apply to a development that meets the requirements of subdivision (b) but only at the request of the applicant. An applicant may request parking incentives or concessions beyond those provided in this subdivision pursuant to subdivision (d).



CREDIT(S)

(Added by Stats.1979, c. 1207, p. 4748, § 10, eff. Oct. 2, 1979. Amended by Stats.1982, c. 1263, § 2, eff. Sept. 22, 1982; Stats.1983, c. 634, § 1; Stats.1984, c. 1333, § 2; Stats.1989, c. 842, § 3; Stats.1990, c. 31 (A.B.1259), § 3, eff. March 26, 1990; Stats.1991, c. 1091 (A.B.1487), § 64; Stats.1998, c. 689 (S.B.1362), § 6; Stats.1999, c. 968 (S.B.948), § 7; Stats.2000, c. 556 (A.B.2755), § 1; Stats.2002, c. 1062 (A.B.1866), § 3; Stats.2003, c. 430 (A.B.305), § 1; Stats.2004, c. 724 (A.B.2348), § 5; Stats.2004, c. 928 (S.B.1818), § 1; Stats.2005, c. 496 (S.B.435), § 2; Stats.2008, c. 454 (A.B.2280), § 1.)

[FN1] So in enrolled bill.

Current with urgency legislation through Ch. 14, and 16-17 of 2011 Reg.Sess.

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PLANNING COMMISSION EXCERPT MINUTES

Regular Meeting
June 25, 2012 at 7:00 p.m.
City Council Chambers
701 Laurel Street, Menlo Park, CA 94025

CALL TO ORDER – 7:01 p.m.

ROLL CALL – Bressler, Eiref, Ferrick (Chair), Kadvany (Vice Chair), O'Malley, Riggs, Yu – All present

INTRODUCTION OF STAFF – Momoko Ishijima, Planner; Jean Lin, Associate Planner; Kyle Perata, Assistant Planner; Thomas Rogers, Associate Planner, Leigh Prince, City Attorney

D. PUBLIC HEARING

- 3. Use Permit, Architectural Control, Major Subdivision, Below Market Rate Housing Agreement, and Environmental Review/389 El Camino Real, LLC/389 El Camino Real:** Request to demolish the existing single-family house at 612 Partridge Avenue and residential triplex at 603-607 College Avenue and construct 26 residential units and associated site improvements on the subject parcels in the C-4(ECR) (General Commercial Applicable to El Camino Real) and R-3 (Apartment) zoning districts. The application includes the following requests:

Staff Comment: Planner Lin said the project request was to demolish an existing single-family residence and residential triplex and construct 26 residential units including 17 attached townhouses and nine 9 detached single family residences on a 1.23 acre site in the R-3 (Apartment) and C-4 El Camino Real and General Commercial Applicable to El Camino Real. She said the Planning Commission was the reviewing and recommending body to the City Council as the final decision making body on the project. She outlined the six areas of review and recommendation. She said the City Council would consider the project at its July 31, 2012 meeting. She said staff had received four additional pieces of correspondence from Rochelle Hutter, Hobart Street, Rico and Ann Rosales, August Circle, Sam Sinnott, architect, and Sohala Khalily, owner of Yogurt Stoppe, El Camino Real. She said all four letters expressed the need to redevelop the project site and supported the proposed project. She said Matt Matteson, the applicant, Glenn Simmons, the project architect and Ethan McAllister, the project engineer were present to address any questions on the proposed project. She said Adam Weinstein, David Clure, and Carolyn Parks from LSA Associates and Paul Stannis, traffic consultant from BKS, were available to answer any questions on the

EIR. She said Leigh Prince, City Attorney and staff were also available to answer questions.

Public Comment: Mr. Matteson, applicant, said the original objective was to develop an economically feasible project that would contribute to Menlo Park's housing stock and within that to redevelop the vacant site with a mixture of attached and detached residences that would be compatible with both El Camino Real and the surrounding neighborhood, to develop a project sensitive to the Allied Arts neighborhood, encourage infill development in a way that would create a more vibrant mix of housing on El Camino Real and areas nearby, provide housing and particularly affordable housing, enhance the visual character of El Camino Real, build a project that everyone was proud of, take advantage of El Camino Real as a transit corridor and design a project in such a way that it would encourage residents to use it and the project's proximity for transit services as well as local retail shopping and downtown Menlo Park. He said in summary the project included 26 residences, 17 of which were attached townhomes and nine were detached single-family residences along the rear property line that adjoin other single-family residential neighbors, two of which were located on corners with access for one from Partridge Avenue and the other from College Avenue. He said the latter were designed to blend with neighborhood and not look like the rest of the development. He said each residence has a two-car garage and guest parking spaces to screen vehicles.

Mr. Matteson presented a visual presentation on the project features. He said revised plans had moved the sidewalk and trellis away from the heritage redwood tree, and had greatly increased the amount of landscaping. He said in working with the neighborhood task force that there would be more extensive landscaping on the College Avenue side of the project.

Mr. Matteson said the project had three below market rate homes for lower income households and would be spread out in three different buildings and would be indistinguishable from other units. He reviewed the cost of the three below market rate units to build and subtracted the allowable purchase price. He said in total the subsidies provided equaled \$1,452,000 for the three units. He said the provision of three below market rate housing units triggered the state density bonus law and qualified the building of 27 residential units. He said their application was for 26 units. He said the traffic studies were done on 27 units as were some of the other environmental studies. He said the application of the state density bonus law allowed the request of development standard waivers. He said they had had 13 requests which had now been reduced to six requests for waivers. He said also they were also by statute eligible for one incentive and their request was to increase the base FAR from 55% to 75%. He said that would bring the project to an overall FAR of 87%.

Mr. Matteson said they had a complete application and plans before the Downtown Specific Plan was finalized so they were exempt but he thought a comparison was helpful. He said the project was consistent with the Plan but it was a little bit less dense and impactful. He said the base FAR in the new zone under the Specific Plan was

110% compared to their plan's 87%. He said under the Plan density was allowed at 25 residences per acre and their proposal was at 21 residences per acre. He said the Plan would allow for 38 feet in height and their project averaged at 30 feet in height or less. He said minimum parking under the Plan was 42 spaces and their project has 69 spaces. He said the Plan required a minimum of 30% open space and their project had a combined 34.1% when common ground and yards were counted.

Mr. Matteson said there had been a few changes to their plans since the last study session. He said they went to the Menlo Park Fire District to get their approval on their plans. He said the District requested they modify the juncture of the sidewalks and driveways to accommodate the weight and turning radiuses of their longest truck. He said that was done in an aesthetically pleasing way and the District had approved. He said they have moved the sidewalk on College Avenue away from the heritage Redwood tree roots. He said the housing units with dens on the first floor had been modified to allow for a half-bath that reduced the garage size, which were larger than they needed to be. He said sidewalks on El Camino Real and College Avenue went from five to six feet. He said Partridge Avenue has four foot wide sidewalks and that would be maintained. He said the project was a transition from lower density to what would probably be much higher density on the east side of El Camino Real. He said the mix of styles would attract a mix of property owners including young couples, small families, and empty nesters. He said five of the units had the capacity for elevators. He said they were pleased to increase the housing supply near local merchants. He said they have worked on the project for two years with City staff and neighbors. He said the Financial Impact Study showed that they would be paying \$1.1 million in fees to the City and other local agencies and they were providing \$1.45 million in BMR subsidies.

Commissioner Bressler asked how soon construction would begin. Mr. Matteson said it would take six to eight months to do detailed construction plans and he suspected by next spring.

Commissioner O'Malley asked if they had financing for the project. Mr. Matteson said that was no problem.

Mr. John Boyle, former City Council member, said that there was a blight problem along El Camino Real. He said the project developers had worked extensively with neighbors, and he thought there was a good outcome. He said the solution was attractive and something he would be proud of for Menlo Park. He said it was good for the City and local merchants. He said he and others initially wanted some retail but that did not really work at this site. He said having another 100 people to shop locally was a benefit that would add to vibrancy downtown, increase sales tax revenue, add to the housing stock and provide BMR housing.

Mr. Karl Hutter, Menlo Park, said he thought the developer's presentation was excellent. He said the closed car dealerships along El Camino Real did not reflect well on the City. He encouraged the Commission to recommend the project.

Ms. Wendy McPerson, Menlo Park, said she lived about six houses away from the project, and she supported it. She said she spent a good part of the 1990s on the Housing Commission and they had worked hard to get residential zoning along El Camino Real. She said there were many young people and young families who want to live along transportation corridors. She said she thought this would be a great project.

Mr. Howie Dallmar, Menlo Park, said he was a long time friend of the Matteson family, and he knew they would build a quality project. He said he supported the project and noted that it was a thoughtful and responsible project. He said the developer had met with the neighbors, listened to their concerns, made changes and earned the support of the majority of the neighbors. He said the project would add to the housing stock and provide BMR housing. He said he thought everyone would be proud of this project.

Ms. Kimberly Glenn, Menlo Park, said she deliberately does not take visitors down El Camino Real because of the vacant lots. She said they moved from Marin 22 years ago specifically to Menlo Park, which they considered the jewel between Atherton and Palo Alto. She said the City had disappointingly degraded over the years. She said she loved Menlo Park and would like to see this project move forward.

Ms. Deborah Fitz, Menlo Park, said she completely supported the project and asked the Commission to recommend the approval to the City Council.

Chair Ferrick closed the public hearing.

Commission Comment: Commissioner Bressler asked if this density bonus would apply to development under the Specific Plan. Planner Rogers said that as a state law it would apply to all development. Commissioner Bressler said the City computes building costs and asked what the construction number per square foot was. Planner Rogers said the City used a spreadsheet for information that has a \$200 per square foot construction cost but that was not as important as the comparative ratio as to how they look at remodels. He said the number was not an exact replica of construction costs.

Commissioner Riggs said if one was building a home where there was an existing home you might budget \$300 per foot. He said if you were building a home where there had been a used car lot there would be a need to bring in services and connection fees which would significantly exceed \$300 per square foot.

Commissioner Bressler said the developer was indicating that it would cost about \$530 per square foot to build these units based on the number offered for the BMR units.

Chair Ferrick said there were six items to vote upon and asked if the Commission wanted to structure the discussion.

Commissioner O'Malley said he would like to take action on all of the items listed noting the project had been discussed ad infinitum. He said it would be hard to find shortcomings with the project as there was considerable support.

Commissioner Eiref asked if any of the Commissioners had any objections.

Commissioner Kadvany said the presentation was excellent and the project had been a long time coming. He said he wanted to discuss each item as he was not comfortable on how the density bonus law was implemented in Menlo Park or at least he wanted to discuss that process.

1. **Use Permit.** A use permit to construct three or more residential units in the R-3 zoning district, and to construct residential units in the C-4(ECR) zoning district.

Chair Ferrick moved to recommend the approval of the use permit to the City Council. Commissioner Yu seconded the motion.

Chair Ferrick said she was pleased to see this project and liked that it fit within what the City has approved in its Specific Plan. Commissioner Bressler said the project fit under what was proposed under the Specific Plan and it would be hard to object to the project in that regard. He said it was important that the project get built quickly so people had an opportunity to see a slightly smaller development on El Camino Real than what the Plan would permit as that was an important part of accessing the Specific Plan.

Commission Action: M/S Ferrick/Yu to recommend approval to the City Council as recommended in the staff report.

1. Make findings, as per Section 16.82.030 of the Zoning Ordinance pertaining to the granting of use permits, that the proposed use will not be detrimental to the health, safety, morals, comfort and general welfare of the persons residing or working in the neighborhood of such proposed use, and will not be detrimental to property and improvements in the neighborhood or the general welfare of the City.
2. Approve the Use Permit for construction of three or more units in the R-3 zoning district and new construction of residential units in the C-4(ECR) zoning district.

Motion carried 7-0.

2. **Architectural Control.** Design review for the proposed residential buildings and site improvements.

Commissioner Riggs moved to recommend approval to the City Council of the architectural control. He said he had been less than pleased with the initial proposals' scale and aesthetics. He said the turnaround since then in terms of the project scale and aesthetics was an obvious credit to the developer and neighbors and behind the scene work from staff. He said the buildings on College and Partridge Avenues were like anchor buildings in retail terminology as they set a wonderful stage. He said this

project would set the bar pretty high in terms of internal site planning, mixes of architectural treatment, details, and materials. He said this was an excellent project and it was wonderful to have an example to refer to in the future. Commissioner O'Malley seconded the motion.

Commissioner Bressler said putting condominiums on El Camino Real was not his preference but he supported the project moving ahead so people could see what this would look like as opposed to what development could occur under the Specific Plan.

Commissioner Kadvany said he agreed with the quality and that this was an exemplary project but returning to the beginning of the project he did not agree with the assumption of bringing housing to El Camino Real. He said speakers had asked that the project not look like Redwood City or Mountain View along El Camino Real and he thought they were talking in part about housing coming all the way out on the ground level along El Camino Real. He said El Camino Real was a state highway and not designed for residential. He said the number of parking spaces was somewhat higher than it could be but it was like a suburban cul de sac with separated two-car garages. He said the setbacks were generous near College Avenue but going toward Planet Auto narrowed, and that the City was not getting the setback needed on El Camino Real. He said he agreed that there would be a lot of high perceived value of the project that would motivate other builders and other projects.

Commissioner Eiref said it was unfair to encumber one project with the vision for what was 10 acres of vacant space. He said hopefully they would see different approaches to using this land. He said with the Specific Plan in place and this project kicking off there was an opportunity to think about where they should go with the rest of the land. He said it was an excellent opportunity to change the momentum and perception of what was happening on El Camino Real.

Commissioner Yu said she was supportive of the project and that it was not meant to summarize every ideal for El Camino Real. She said the housing was setting a nice aesthetic bar. She said there had been a great process and the proponents cared about the community. She said it set a nice tone for being the first project on El Camino Real since the adoption of the Specific Plan, but it did not have to encapsulate all of the City's hopes and dreams.

Commission Action: M/S Riggs/O'Malley to recommend to the City Council to approve the architectural control.

3. Adopt the following findings, as per Section 16.68.020 of the Zoning Ordinance, pertaining to architectural control approval:
 - a. The general appearance of the structures is in keeping with the character of the neighborhood;

- b. The development will not be detrimental to the harmonious and orderly growth of the City;
 - c. The development will not impair the desirability of investment or occupation in the neighborhood; and,
 - d. The development provides adequate parking as required in all applicable City Ordinances and has made adequate provisions for access to such parking.
4. Approve the proposed design of the new buildings and site improvements.

Motion carried 7-0.

3. **Major Subdivision.** Tentative Map for seven existing legal lots to be merged into two lots; the public street easement for Alto Lane would be abandoned; and 26 residential condominium units would be created.

Commissioner Riggs moved to recommend approval of the major subdivision to the City Council. Chair Ferrick seconded the motion.

Commissioner Kadvany wished he had understood earlier the significance that the City was giving the developer Alto Lane. Recognized by the Chair, Planner Lin said that the City was abandoning Alto Lane but it was important to recognize that the lane only served the triplexes currently on the property. She said when those triplexes were demolished the lane would serve no purpose. Commissioner Kadvany said that while it was the right thing for the City to do, he would have liked the City's beneficence to have been more apparent at the beginning of the process and that might have helped with some of the issues. Commissioner Riggs said his context was the alleys of the Willows which the City did not seem to want to own. He said for that reason he did not see Alto Lane as having any intrinsic value although its abandonment provided land to the developer.

Commission Action: M/S Riggs/Ferrick to recommend the City Council approve the Major Subdivision.

5. Make findings that the proposed major subdivision is technically correct and in compliance with all applicable State regulations, City General Plan, Zoning and Subdivision Ordinances, and the State Subdivision Map Act.
6. Approve the request for a Tentative Map to merge seven lots into two lots, abandon the public street easement for Alto Lane, and create 26 residential condominium units.

Motion carried 7-0.

4. **Below Market Rate Housing Agreement.** A Below Market Rate (BMR) Housing Agreement to provide for the development of three on-site low-income BMR units in accordance with the City's BMR Program and the provisions of Government Code Section 65915, the State Density Bonus Law.

Commissioner O'Malley moved to recommend to the City Council approval of the Below Market Rate Housing Agreement as recommended in the staff report. Chair Ferrick seconded the motion. She noted that the Housing Commission had analyzed the BMR Housing Agreement and supported.

Commissioner Riggs said the Planning Commission had about a two-hour session about a year ago on the state density bonus law and was something they were made aware of and subsequently that knowledge was useful for the consideration of this project.

Commissioner Kadvany asked about density and intensity of units per acre. Planner Rogers said that some cities in addition to maximum standards have minimum density standards. He said in the absence of that the BMR state density bonus was based on a percentage of the units the developer was opting for so there was no mechanism by which the City could require minimum density.

Chair Ferrick suggested that the motion for the BMR be combined with a motion for the state density bonus law which was listed next on the approval. Commissioner O'Malley agreed as the maker of the motion to include also a recommendation to the City Council to approve the incentive and six development standard waivers requested under the state density bonus law. Chair Ferrick seconded.

Commissioner Kadvany said that the state density bonus law was the item he wanted to address.

Commissioner O'Malley retracted the modification to the motion.

Commission Action: M/S O'Malley/Ferrick to recommend that the City Council approve the Below Market Rate Housing Agreement.

7. Approve the Below Market Rate Housing Agreement to provide three on-site BMR units in accordance with the City's Below Market Rate Housing Program and State Density Bonus Law (Attachment E).

Motion carried 7-0.

5. **State Density Bonus Law.** The application is being submitted subject to the State Density Bonus Law, which permits exceptions to the City's Zoning Ordinance requirement, to allow one incentive and six development standard waivers.

Commissioner Kadvany said he had mentioned, earlier this evening the origins of the project with driveways off Partridge and College Avenue as he believed that project had struck great fear into neighbors that there would be considerable traffic increase on those streets. He said neighbors, rightly so, began mobilizing. He said as originally proposed having a retail use on El Camino Real with ingress/egress from College and Partridge Avenues, he could understand neighbors' concern. He said however that the processes bifurcated with the neighborhood group working with the developer, contrasted with what was happening in the public meetings. He said neighbors were very concerned about traffic and the project went from a project with 3,000 square feet of retail to zero retail. He said that made sense for the neighbors and from then on out that group was setting the premises for the developer. He said in the meantime at the Planning Commission the next phase of the project seen was under the state density bonus law. He said that seemed to remove any decision making power or design influence the Commission had. He said the Commission spent a lot of time with the City Attorney trying to understand what the law implied and what influence the Commission could have on this project. He said the project changed through the persistence of the neighbors. He agreed with one of the letters received that the project was organized around cars, garages and was suburban. He said there was never really an opportunity to discuss including some portion as retail. He said in terms of process that the process disappeared. He said because residents were worried about cars then the focus was on parking. He said he was pleased there were real below market rate homes through this project but he did not like the state density bonus law hijacking the process so the Commission could not focus on the project in a meaningful way.

Chair Ferrick said the state density bonus law was a mechanism and it happened to apply to this project.

Commissioner Riggs moved to recommend to the City Council approval of the one incentive and six development standard waivers allowable under the state density bonus law. Commissioner O'Malley seconded the motion.

Commissioner Kadvany said this project could have probably been built under the Specific Plan.

Commission Action: M/S Riggs/O'Malley to recommend the following action to the City Council.

8. Apply the State Density Bonus Law to allow one incentive and six development standard waivers.

Motion carried 5-2 with Commissioners Bressler and Kadvany dissenting.

6. **Environmental Review.** The project is analyzed for potential environmental impacts in the focused EIR.

Commissioner Kadvany said there was a letter from the Department of Transportation stating they thought the project was overparked, suggesting more facilities for bicycles, and decoupling spaces. He said the reply was on page 16 and it indicated that residents could utilize on street parking along El Camino Real, College Avenue and Partridge Avenue but failed to point out there was no overnight street parking. He thanked LSA for a well organized and thorough environmental document.

Commission Action: M/S Riggs/Yu to recommend the following action to the City Council.

9. Adopt a Resolution of the City Council of the City of Menlo Park, State of California, Certifying the Final Environmental Impact Report (EIR), Adopting the Statement of Overriding Considerations and Adopting the Mitigation Monitoring and Reporting Program for the 389 El Camino Real Project for the 389 El Camino Real Project (Attachment I).

Motion carried 7-0.

Commissioner Bressler said he thought the overhead and process cost for this project had been huge. He said he had an expectation that with some of that cost not being there for the Specific Plan that this would result in projects for Menlo Park to enjoy. Commissioner Riggs said he agreed with that comment. He said he wanted to thank staff for the staff reports that made this process very functional for the Commission and City. Chair Ferrick said she applauded the developer and neighbors for bringing divergent viewpoints to a good compromise.

ADJOURNMENT

The meeting adjourned at 10:28 p.m.

Staff Liaison: Thomas Rogers, Associate Planner

Recording Secretary: Brenda Bennett

Approved by Planning Commission on July 23, 2012